

PUBLIC STREET & ALLEY STANDARD DETAILS

**PUBLIC STREETS AND ALLEYS STANDARD DETAILS
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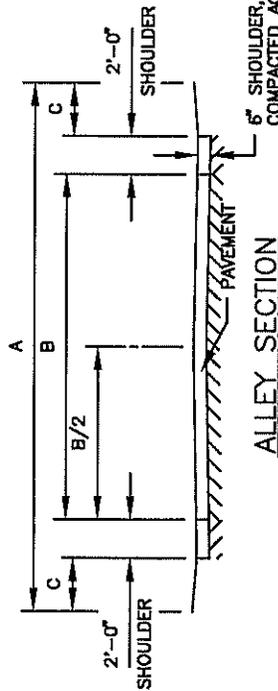
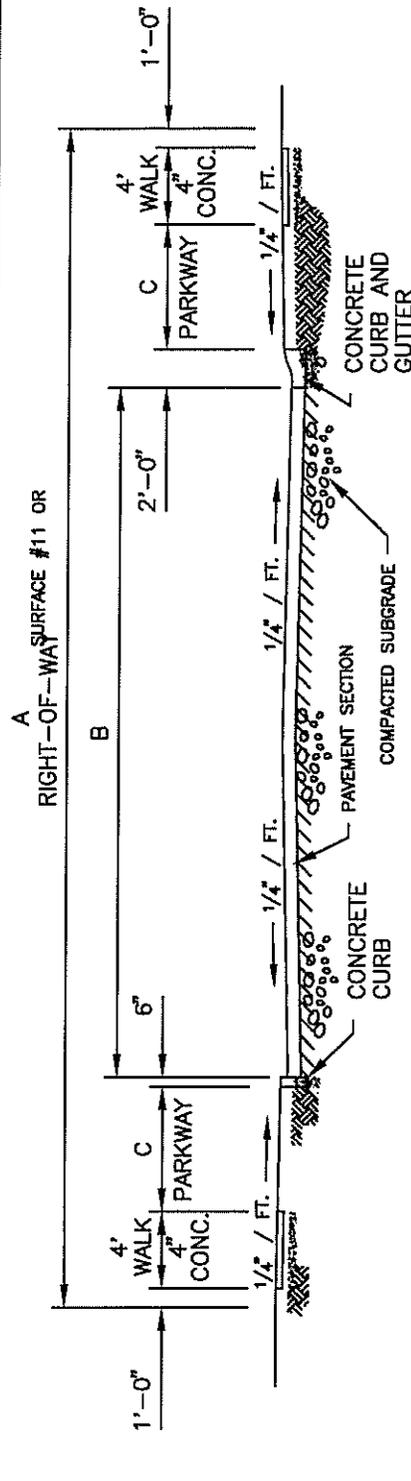
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CITY of MONTICELLO, INDIANA
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 PUBLIC STREETS AND ALLEYS

FIGURE
 PS-1

PAVEMENT DETAILS



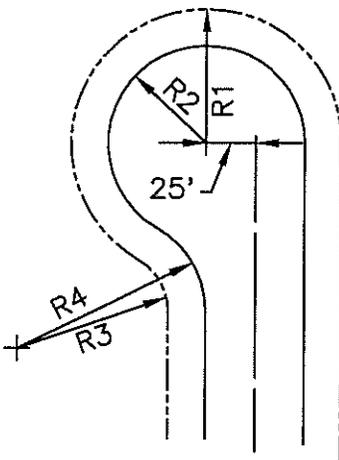
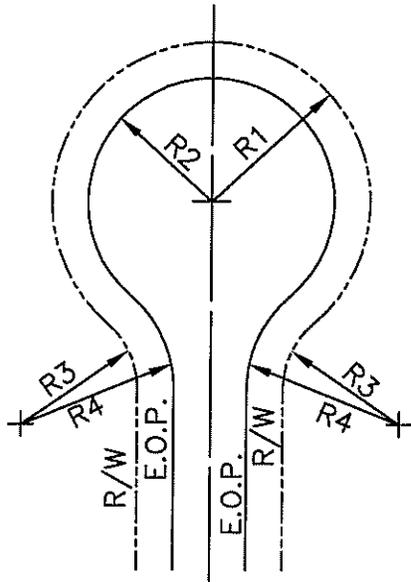
NOTE: OTHER PAVEMENT SECTIONS WILL BE CONSIDERED PROVIDING THEY ARE DESIGNED BY A REGISTERED PROFESSIONAL ENGINEER, AND ARE BASED ON TRAFFIC NEEDS AND EXISTING SOIL CONDITIONS.

ALL RIGHT-OF-WAY IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE INDIANA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, LATEST EDITION.

CLASSIFICATION	BITUMINOUS					CONCRETE		COMMENTS
	H.A.C. SURFACE #11 OR #12 (INCHES)	H.A.C. BINDER #9 OR #11 (INCHES)	H.A.C. BASE #5 (INCHES)	COMPACTED AGG. BASE (INCHES)	COMPACTED AGG. BASE (INCHES)	PCC (INCHES)	CURB TYPE	
PRIMARY, ARTERIAL	1	2.5	6	8	8	8	6	
SECONDARY, COLLECTOR	1	2.5	4	8	8	7	6	
LOCAL, MINOR	1	3	N/A	10	10	6	6	
SERVICE DRIVE	1	3	N/A	10	10	6	6	
ALLEY	1	3	N/A	10	10	6	6	
CLASSIFICATION	NO. OF LANES	A (FEET)	B (FEET)	C (FEET)				
PRIMARY, ARTERIAL	3	60	36	6.5	STRAIGHT			
SECONDARY, COLLECTOR	2	60	30	8	ROLL/STRAIGHT			
LOCAL, MINOR	2	50	24	6	ROLL/STRAIGHT			
SERVICE DRIVE	2	50	24	5	STRAIGHT			
ALLEY	1	30	18	4	N/A			

MIN. DIMENSIONS (IN FEET)

	R1	R2	R3	R4
RESIDENTIAL	50'	38'	48'	60'
COMMERICAL	60'	50'	88'	100'



NOTES:

1. PAVEMENT SECTIONS AND STREET WIDTHS (PAVEMENTS AND RIGHT OF WAY) SHALL BE IN ACCORDANCE W/ THE REQUIREMENTS OF TABLE 4-1.
2. MAX. CUL-DE-SAC STREET LENGTH ALLOWED IS 500 FT., DEFINED TO THE CENTER OF THE CUL-DE-SAC.
3. OFFSET TYPE CUL-DE-SAC ALLOWED, W/ SAME MIN. DIMENSIONS.
4. CUL-DE-SAC W/ MIN. DIMENSIONS WILL NOT BE ALLOWED TO CONTAIN ISLANDS, LANDSCAPED, CURBED OR OTHERWISE.
5. TRAFFIC CONTROL SIGNING, AS NECESSARY, WILL BE IN ACCORDANCE W/ THE INDIANA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
6. CUL-DE-SAC WILL NOT BE ALLOWED AT INTERSECTIONS (CIRCULAR TRAFFIC PATTERNS).
7. PAVEMENT MATERIALS (CONC. OR ASPH.) SHALL BE THE SAME AS THE REMAINDER OF THE STREET.

CUL-DE-SAC DETAIL

CALDSAC.DWG

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FIGURE

PS-2

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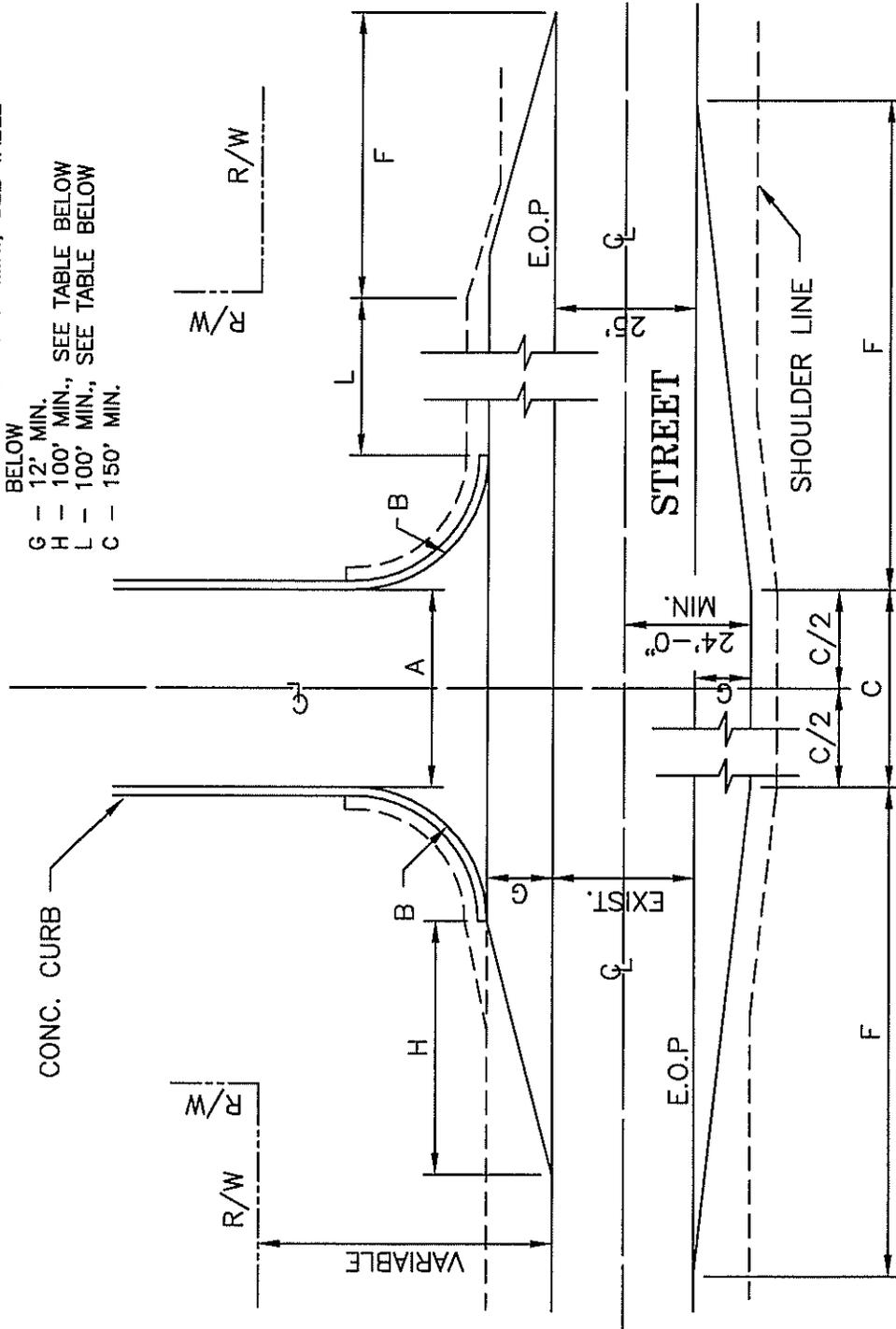
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FIGURE
 PS-3

PUBLIC STREET OR ROAD APPROACH DETAIL

LEGEND

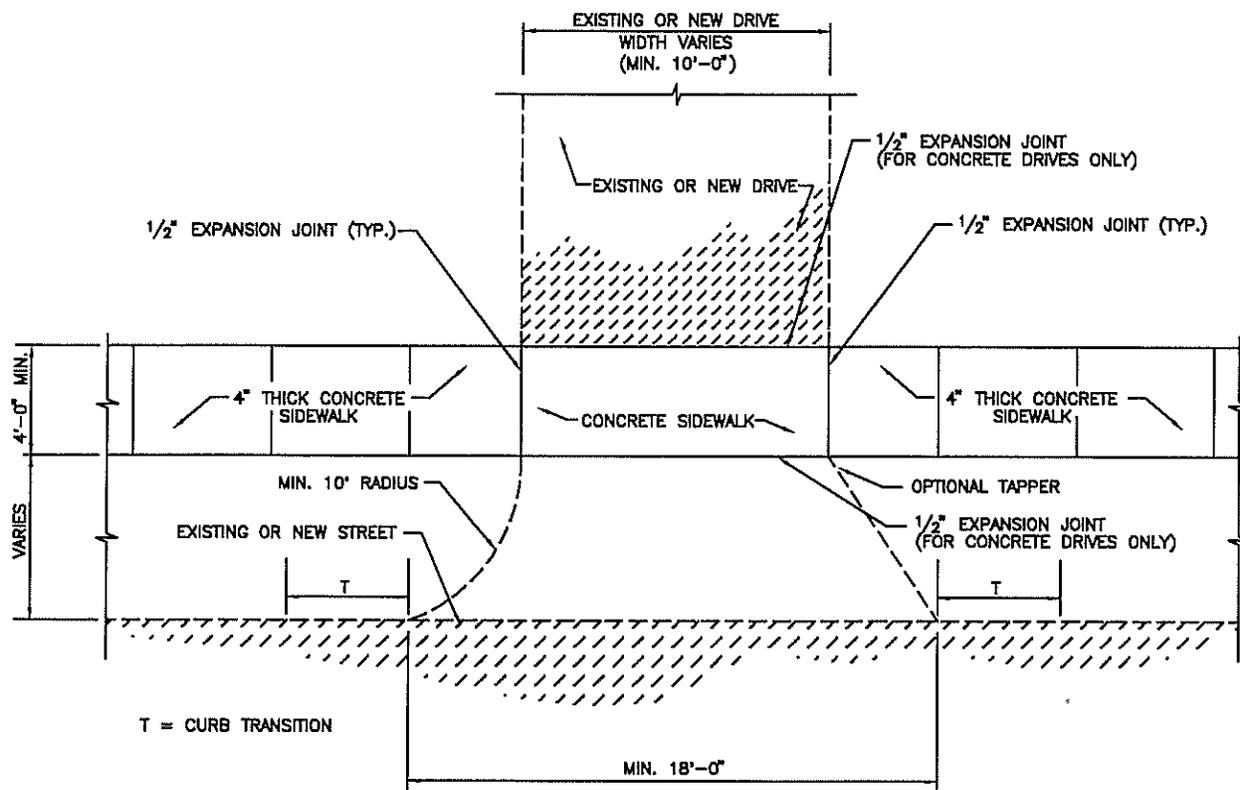
- A - TOWN PUBLIC WORKS STANDARDS, TABLE 4-1
- B - 25' STD., 10' MIN. RADIUS
- F - 1:12 TAPER-150' MIN., SEE TABLE BELOW
- G - 12' MIN.
- H - 100' MIN., SEE TABLE BELOW
- L - 100' MIN., SEE TABLE BELOW
- C - 150' MIN.



NOTE:

1. PROVIDE LARGER RADIUS, "B", IF DESIGN VEHICLE REQUIRES IT.
2. WHEN "F" IS OTHER THAN 90° "B" (EDGE RADI) IS TO BE BY APPROVAL OF TOWN ENGINEER.

DESIGN SPEED	F + L
30	250 FEET
40	370 FEET
50	500 FEET



NOTES FOR SIDEWALK RAMP:

1. CURB CUT RAMPS ARE TO BE LOCATED AS SHOWN ON THE PLANS OR AS DIRECTED.
2. SURFACE TEXTURE OF THE RAMP SHALL BE NO LESS THAN A BROOM FINISH, BUT SHALL MEET AMERICAN DISABILITIES ACT REQUIREMENTS
3. CARE SHALL BE TAKEN TO ASSURE A UNIFORM GRADE ON ALL RAMPS WITH NO BREAKS IN GRADE.
4. THE NORMAL GUTTER LINE PROFILE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP.
5. EXPANSION JOINT FOR THE RAMP SHALL BE A MAXIMUM 1/2" WIDE. THE TOP OF THE JOINT FILLER FOR ALL RAMP TYPES SHALL BE FLUSH WITH ADJACENT CONCRETE.

TYPICAL RESIDENTIAL DRIVE APPROACH DETAIL

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FIGURE

PS-4

SIDE4.DWG

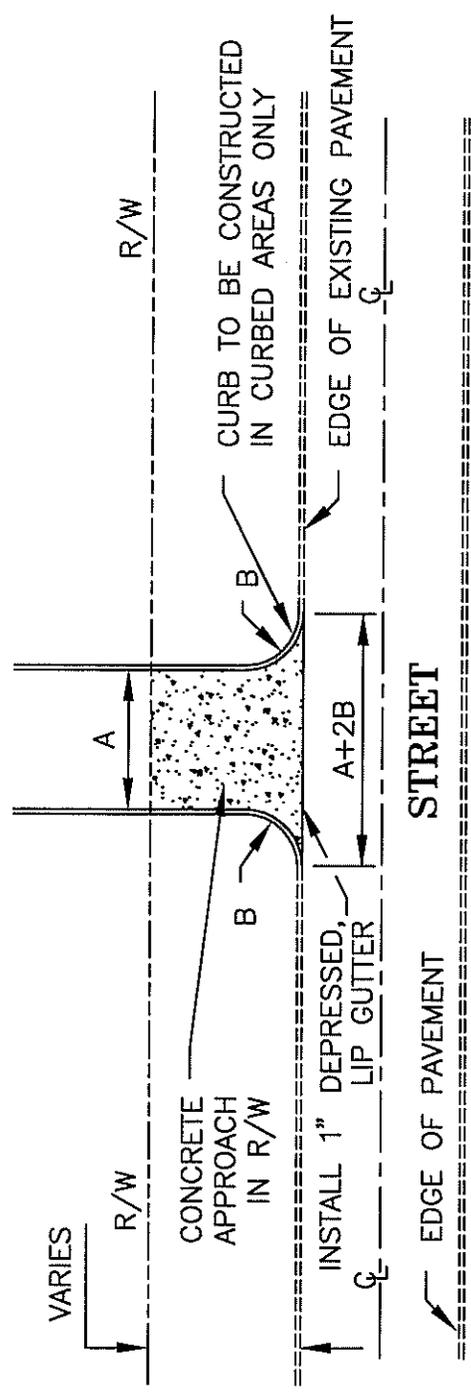
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FIGURE
PS-5

COMMERCIAL DRIVE APPROACH DETAIL TYPE I



LEGEND

- A = 24'
- B = 25' STANDARD, 10' MINIMUM RADIUS.
LARGER RADIUS IF DESIGN VEHICLE REQUIRES IT.

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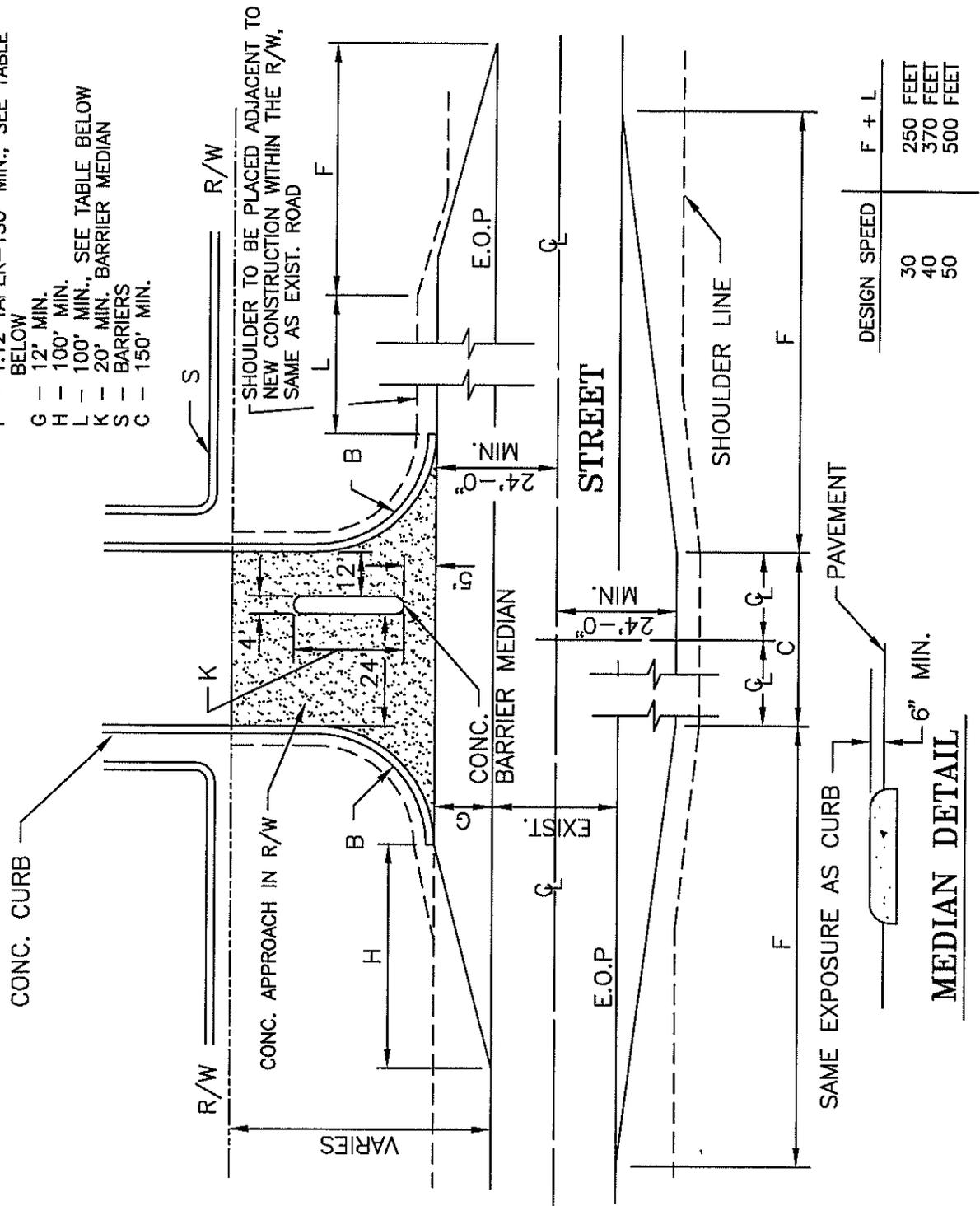
FIGURE
PS-6

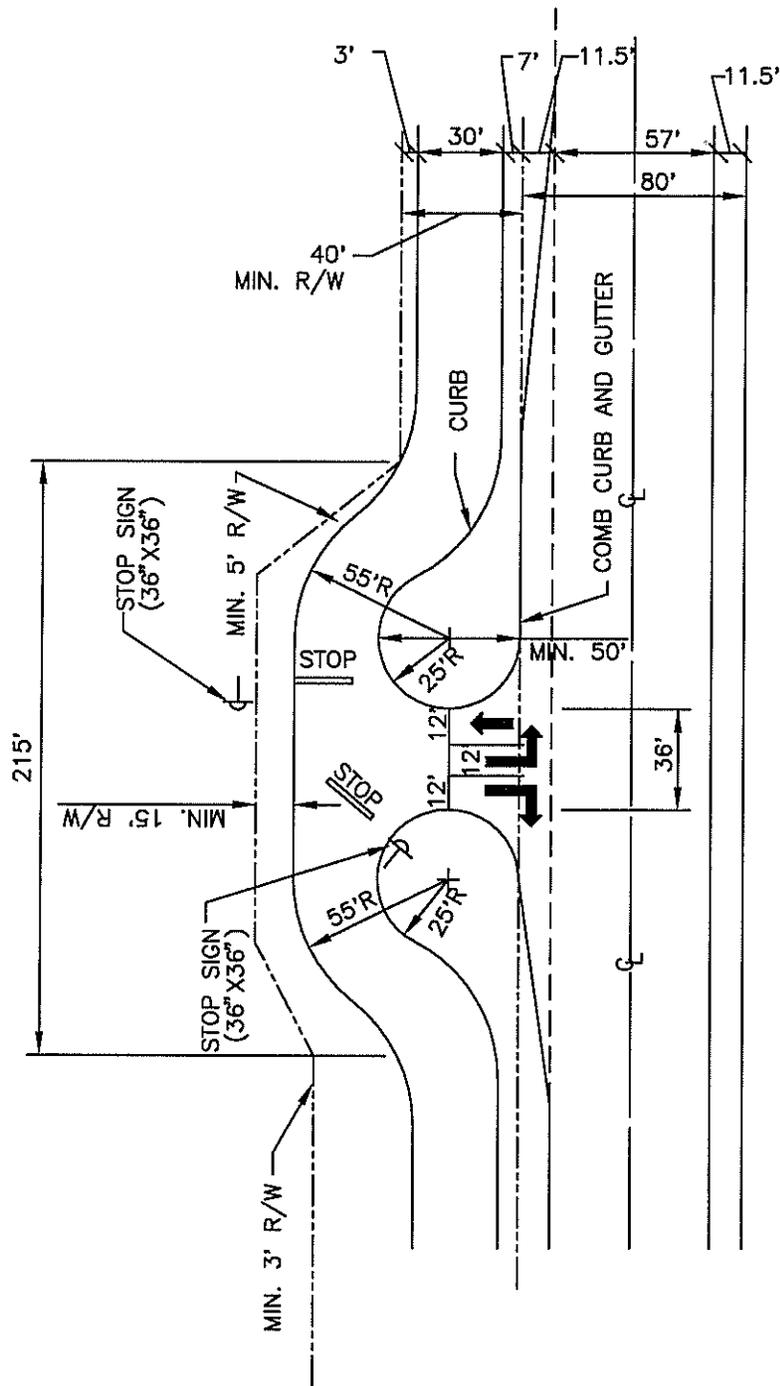
COMMERCIAL DRIVE APPROACH DETAIL - TYPE II

NOTE: PROVIDE LARGER RADIUS, "B" IF DESIGN VEHICLE REQUIRES IT.

LEGEND

- A - 40'
- B - 25' MAX., 10' MIN. RADIUS
- F - 1:12 TAPER-150' MIN., SEE TABLE BELOW
- G - 12' MIN.
- H - 100' MIN.
- L - 20' MIN., SEE TABLE BELOW
- K - 20' MIN. BARRIER MEDIUM
- S - BARRIERS
- C - 150' MIN.





TYPICAL SERVICE ROAD ENTRANCE

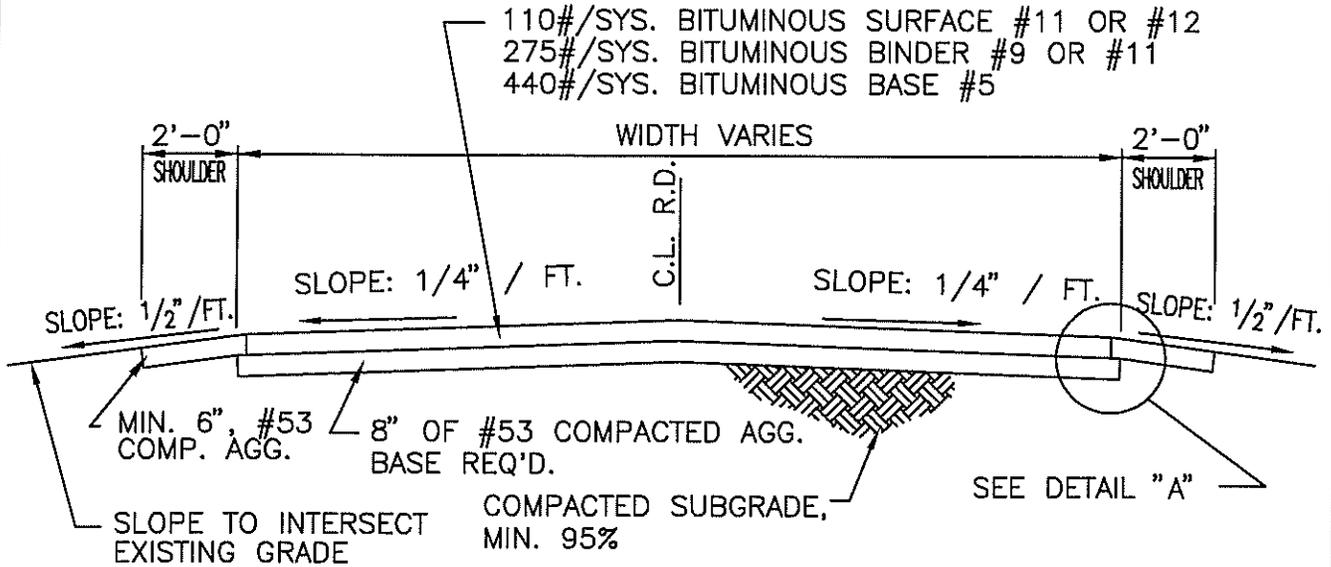
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FIGURE
 PS-7

SERRDENT.DWG



110#/SYS. BITUMINOUS SURFACE #11 OR #12
 275#/SYS. BITUMINOUS BINDER #9 OR #11
 440#/SYS. BITUMINOUS BASE #5

MIN. 6", #53
 COMP. AGG.

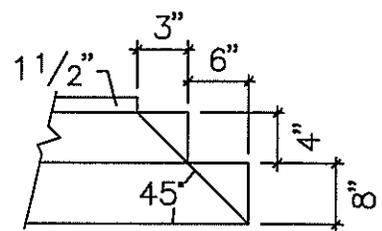
8" OF #53 COMPACTED AGG.
 BASE REQ'D.

COMPACTED SUBGRADE,
 MIN. 95%

SLOPE TO INTERSECT
 EXISTING GRADE

SEE DETAIL "A"

NOTE: ALL BITUMINOUS TO BE TYPE "LV"
 OR BETTER.



DETAIL "A"

TYPICAL SECONDARY ROAD REPLACEMENT-
 OPEN SECTION

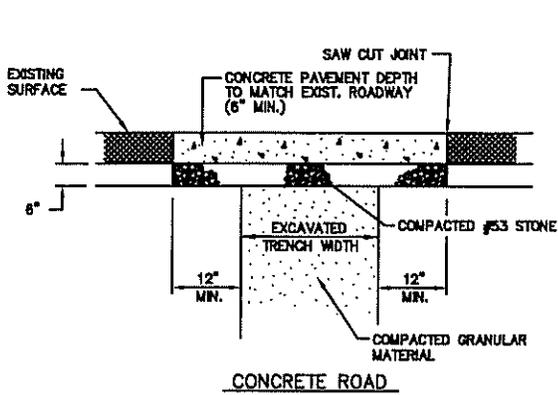
ROAD2.DWG

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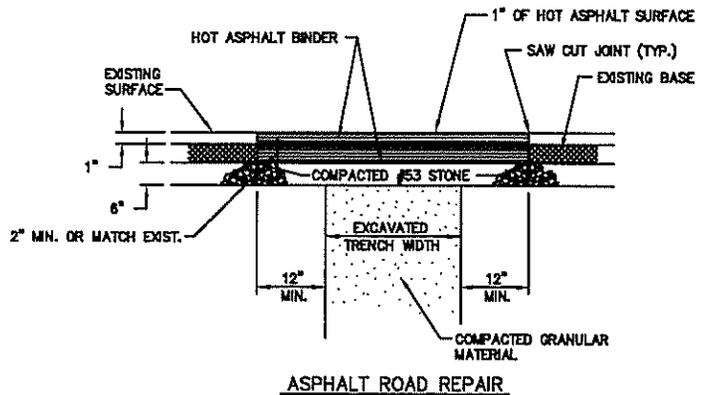
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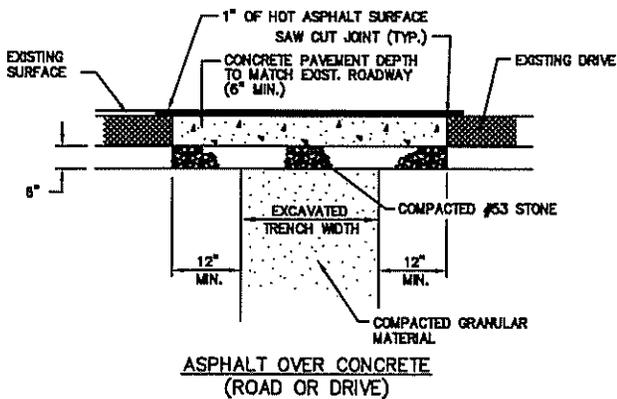
FIGURE
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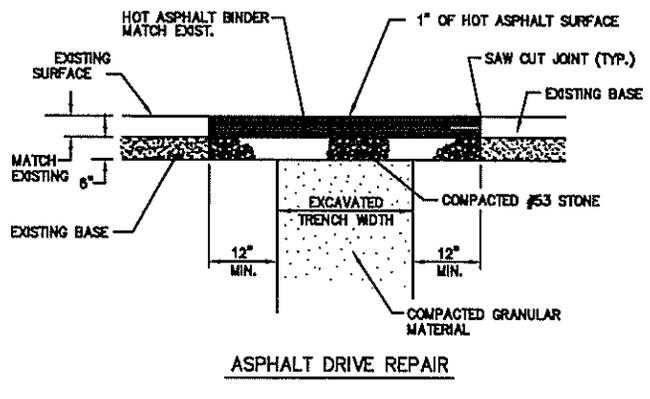
CONCRETE ROAD



ASPHALT ROAD REPAIR



ASPHALT OVER CONCRETE
(ROAD OR DRIVE)



ASPHALT DRIVE REPAIR

STREET AND DRIVE REPAIR DETAILS

STDREP2.DWG

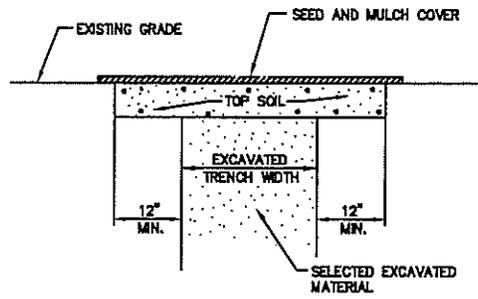
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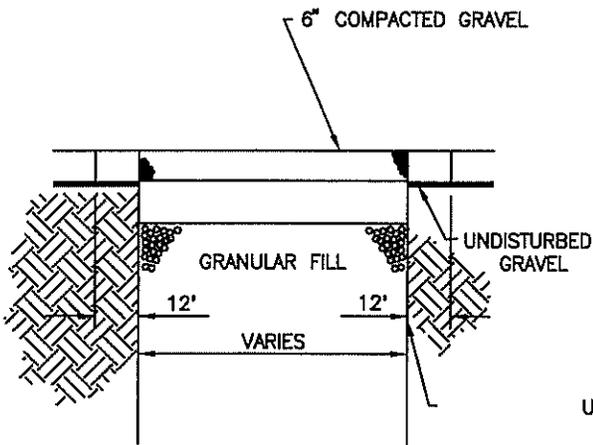
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FIGURE

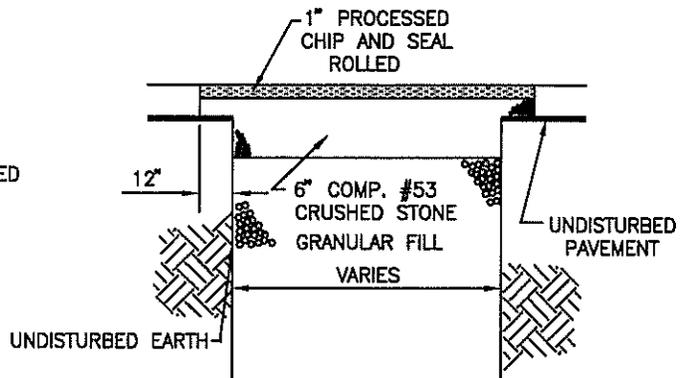
PS-9



GRASSED AREA



GRAVEL STREET REPAIR



CHIP & SEAL REPAIR

NOTE:

ALL STREETS ROADS & PAVED DRIVE CROSSINGS ARE TO BE BACKFILLED WITH GRANULAR MATERIAL & TOPPED WITH 10" OF No. 53 STONE OR APPROVED MATERIAL DURING CONSTRUCTION PRIOR TO FINAL PAVEMENT REPAIR

STREET AND DRIVE REPAIR DETAILS

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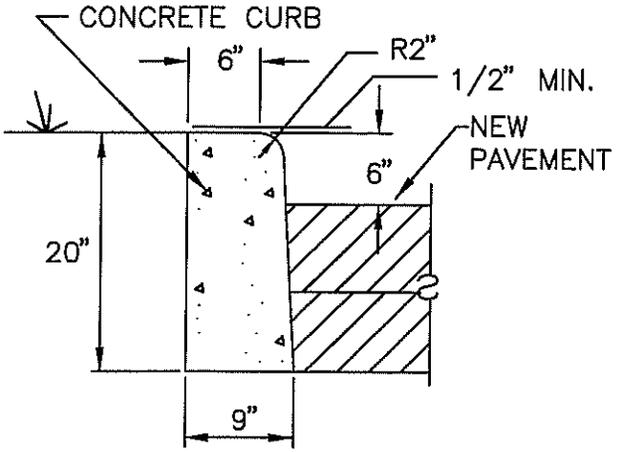
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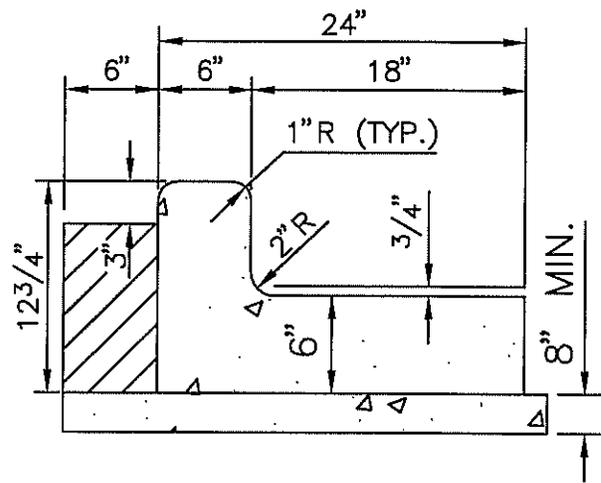
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STDREP1.DWG

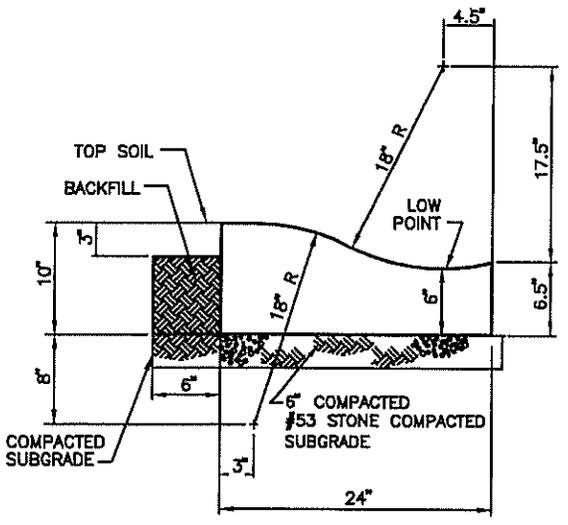


CONCRETE CURB DETAIL



NOTE: COMPACTED AGGREGATE NO.53 ON COMPACTED SUBGRADE

CONCRETE CURB AND GUTTER DETAIL



TYPICAL CONCRETE ROLL CURB AND GUTTER

CURB DETAILS

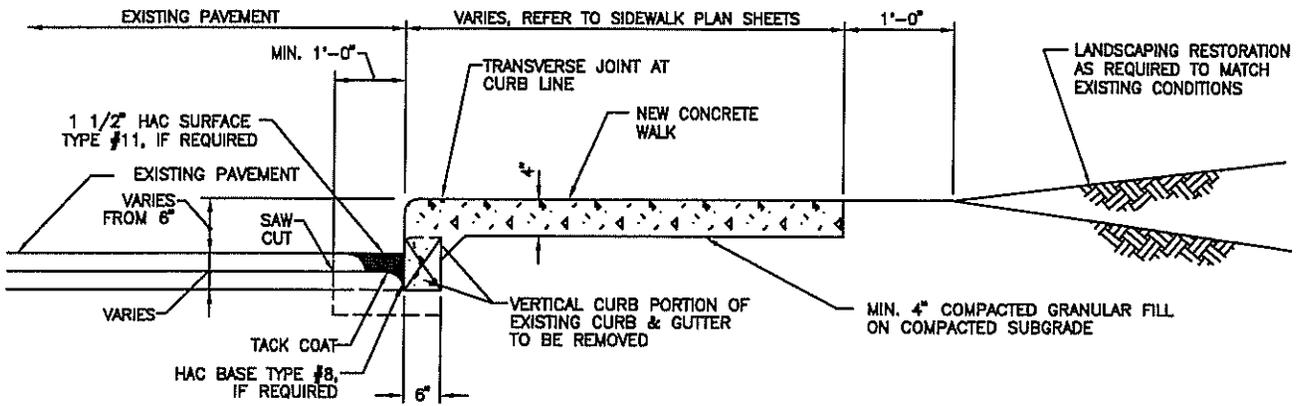
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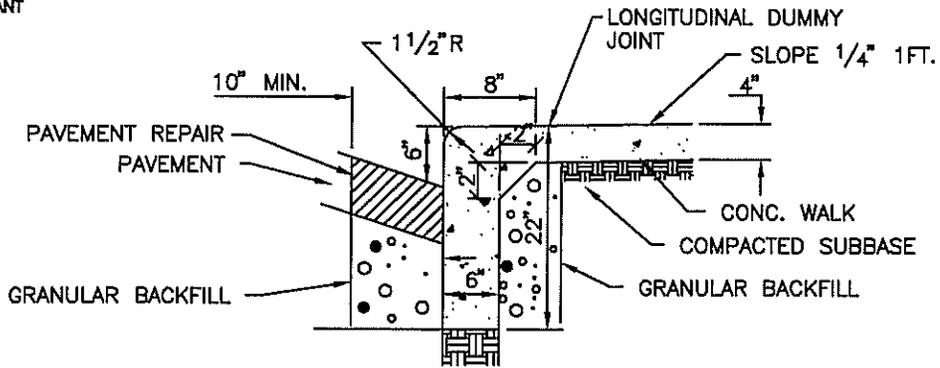
FIGURE
PS-11

CURBS.DWG

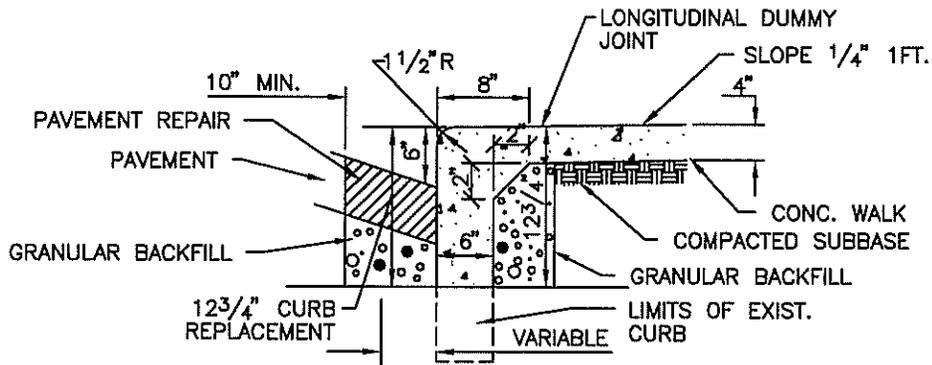


SIDEWALK WITH MONOLITHIC CURBING

NOTE: SLOPE SIDEWALKS 1/4" PER FOOT TOWARD PAVEMENT OR DRAINAGE SWALE AS EXISTING CONDITIONS WARRANT



MONOLITHIC CONC. CURB



MONOLITHIC CONC. CURB REPLACEMENT

MONOLITHIC CURB/SIDEWALK DETAILS

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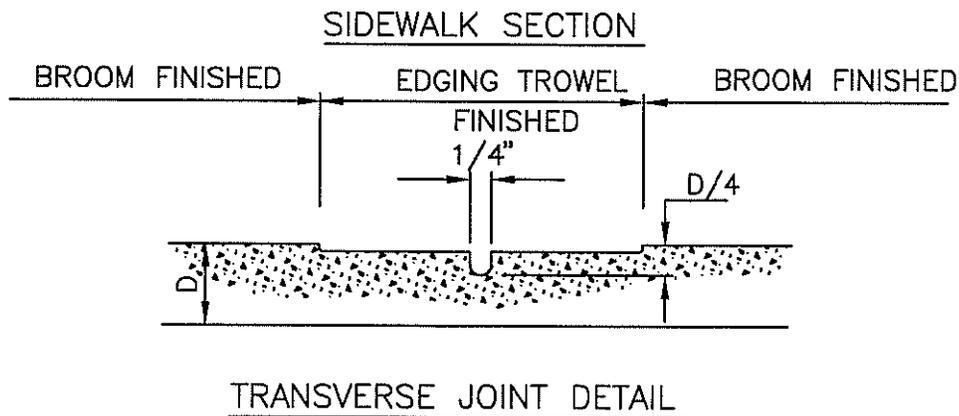
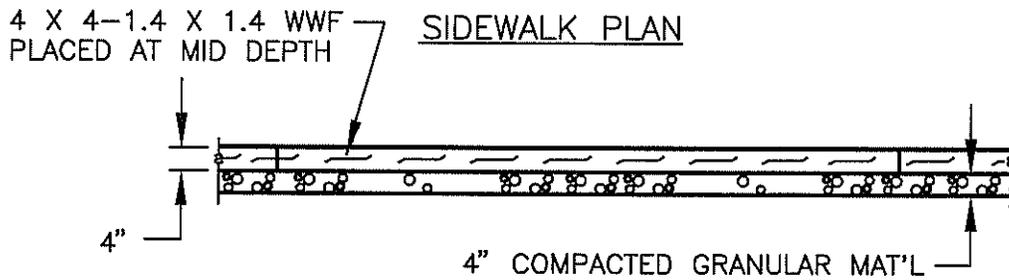
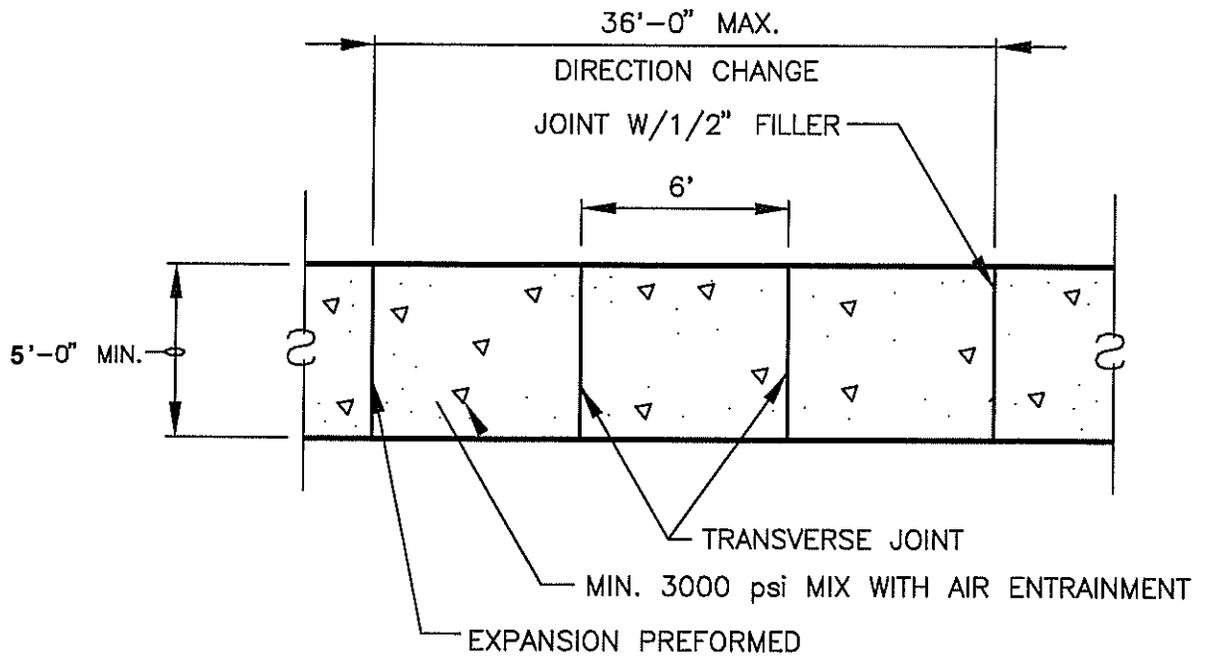
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FIGURE

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SIDE1.DWG



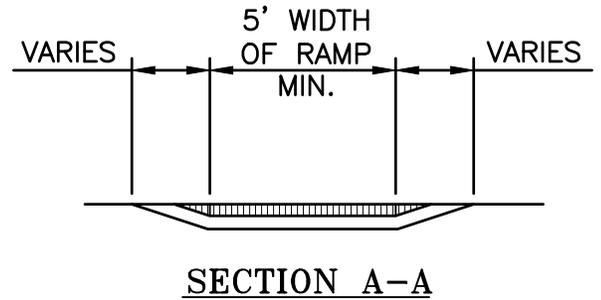
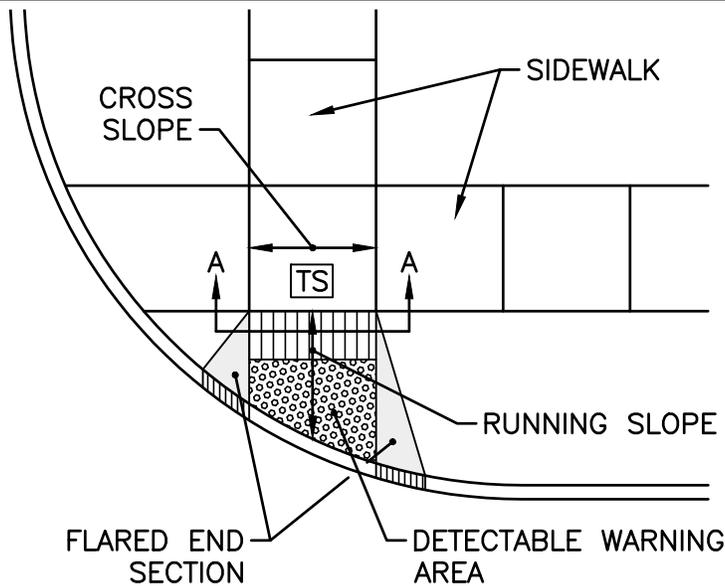
TYPICAL SIDEWALK DETAIL

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FIGURE
PS-13



PLAN [TS] = TURNING SPACE

GENERAL NOTES:

1. ALL SLOPES ARE ABSOLUTE RATHER THAN RELATIVE TO THE SIDEWALK OR ROADWAY GRADE. SLOPE AT LEAST 0.50% LESS THAN THE MAXIMUM ARE PREFERRED.
2. RAMP OR BLENDED TRANSITION. A RAMP OR BLENDED TRANSITION SHALL BE USED TO LOWER OR RAISE THE SIDEWALK TO CONNECT WITH THE STREET OR HIGHWAY.
3. TURNING SPACE. A TURNING SPACE SHALL BE PROVIDED AT THE TOP OF A PERPENDICULAR RAMP, BOTTOM OF A PARALLEL RAMP, OR WHERE THE PEDESTRIAN TRAVEL REQUIRES A CHANGE IN DIRECTION. A COMMON TURNING SPACE MAY BE SHARED BY ADJACENT RAMP. THE TURNING SPACE SHALL HAVE A MINIMUM CLEAR DIMENSION OF 4FTX4FT. WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK OF THE SIDEWALK BY A CURB, RETAINING WALL, BUILDING, OR FEATURE COVER 2 INCHES IN HEIGHT, THE MINIMUM CLEAR DIMENSION SHALL BE 4FTX5FT, WITH THE 5-FT DIMENSION IN THE DIRECTION OF THE RUNNING SLOPE.
4. FLARED SIDE. A FLARED SIDE SHALL BE USED ADJACENT TO A WALKABLE SURFACE. A FLARED SIDE MAY BE USED ADJACENT TO A NON-WALKABLE SURFACE. A FLARED SIDE SHALL HAVE A MAXIMUM SLOPE OF 10.00% MEASURED PARALLEL TO THE BACK OF CURB.
5. CLEAR SPACE. A CLEAR SPACE SHALL BE PROVIDED BEYOND THE BOTTOM GRADE BREAK OF A CURB RAMP WHOLLY CONTAINED WITHIN THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL PATH. THE CLEAR SPACE SHALL HAVE A MINIMUM CLEAR DIMENSION OF 4FT X 4FT.
6. DETECTABLE WARNING SURFACE. A DETECTABLE WARNING SURFACE SHALL CONSIST OF TRUNCATED DOMES AND BE PLACED AT EACH STREET, HIGHWAY OR RAILROAD CROSSING. THE DETECTABLE WARNING SURFACE SHALL EXTEND A MINIMUM OF 2 FT IN THE DIRECTION OF PEDESTRIAN TRAVEL AND BE PLACED THE ENTIRE WIDTH OF A RAMP, BLENDED TRANSITION, OR TURNING SPACE.
7. RUNNING SLOPE. THE RUNNING SLOPE OF A RAMP, BLENDED TRANSITION, OR TURNING SPACE SHALL BE MEASURED PARALLEL TO THE DIRECTION OF PEDESTRIAN TRAVEL.
 - A. A RUNNING SLOPE OF 2.00% OR LESS IS CONSIDERED LEVEL.
 - B. A RAMP SHALL HAVE A MAXIMUM RUNNING SLOPE OF 8.33% BUT SHALL NOT REQUIRE A RAMP LENGTH TO EXCEED 15 FT.
 - C. A BLENDED TRANSITION SHALL HAVE A MAXIMUM RUNNING SLOPE OF 5.00%.
 - D. A TURNING SPACE SHALL HAVE A MAXIMUM RUNNING SLOPE OF 2.00%.
8. WIDTH. UNLESS OTHERWISE NOTED, MINIMUM WIDTH OF A RAMP, BLENDED TRANSITION, OR TURNING SPACE, EXCLUDING FLARED SIDES OR OR RETURN CURB, SHALL BE 5FT.
9. CROSS SLOPE EXCEPTIONS. THE CROSS SLOPE OF A RAMP, BLENDED TRANSITION, OR TURNING SPACE SHALL BE MEASURED PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAVEL.
 - A. THE MAXIMUM CROSS SLOPE AT A PEDESTRIAN STREET CROSSING WITHOUT YIELD OR STOP CONTROL SHALL BE 5.00%
 - B. THE MAXIMUM CROSS SLOPE AT A PEDESTRIAN STREET CROSSING WITH YIELD OR STOP CONTROL SHALL BE 2.00%.
 - C. THE MAXIMUM CROSS SLOPE AT A MIDBLOCK CROSSING SHALL BE THE ESTABLISHED GRADE OF THE ADJACENT ROADWAY.
10. OBJECTS SUCH AS UTILITY COVER, VAULT FRAME, AND GRATING SHALL BE PLACED OUTSIDE THE CURB RAMP.
11. CURB RAMP SHALL BE PLACED WITHIN THE MARKED CROSSWALK AREA.
12. DRAINAGE INLETS SHOULD BE LOCATED UPHILL FROM A CURB RAMP TO PREVENT PONDING IN THE PATH OF PEDESTRIAN TRAVEL.

SIDEWALK RAMP DETAIL

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FIGURE

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GENERAL NOTES

1. ALL MATERIAL AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
2. BARRICADE TYPE III-A AND BARRICADE TYPE III-B WITH SIGNS SHALL INCLUDE CONSTRUCTION SIGNS, TYPICAL SIGN STANDARD, AND CONSTRUCTION WARNING LIGHTS AS SHOWN ON THE STANDARD DETOUR SIGN SHEETS.
3. CONSTRUCTION SIGN TYPE A AND TYPE B SHALL INCLUDED THE SUPPORT, ALL NECESSARY HARDWARE, AND REQUIRED CONSTRUCTION WARNING LIGHTS.
4. THE TEMPORARY BITUMINOUS DIVIDER SHALL INCLUDE THE STANDARD TUBULAR AND TRAFFIC PAINT AS SHOWN.
5. THE FOR BARRICADES TYPE III-A AND III-B. BARRICADES TYPE III-B WITH SIGNS, TYPE A AND B CONSTRUCTION SIGNS, AND CONCRETE BARRIER SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
6. WOOD MATERIALS ARE TO BE DIMENSION LUMBER IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
7. STANDARD DRUMS, TYPE I AND TYPE II BARRICADES, SHALL BE USED ON CONVENTIONAL ROADS, URBAN STREETS AND ARTERIALS. ONLY STANDARD DRUMS AND TYPE I BARRICADES SHALL BE USED ON THE EXPRESSWAYS, FREEWAYS, AND OTHER HIGH SPEED ROADWAYS.
8. UNLESS OTHERWISE SPECIFIED, DRUMS, TYPE I AND TYPE II BARRICADES, CONES, AND TUBULAR MARKERS SHALL BE SPACED AS FOLLOWS:
 - A) NUMERICALLY EQUAL TO THE POSTED SPEED LIMIT, BUT NOT LESS THAN 20 FEET ON TAPERS.
 - B) 50 FEET FRO A POSTED SPEED OF LESS THAN 40 M.P.H. ON TANGENTS.
 - C) 100 FEET FOR A POSTED SPEED EQUAL TO OR GREATER THAN 40 M.P.H. ON TANGENTS.
9. STANDARD CONES AND TUBULAR MARKERS SHALL BE A MINIMUM OF 18 INCHES IN HEIGHT FOR POSTED SPEEDS UP THROUGH 40 M.P.H. AND A MINIMUM OF 28 INCHES FOR POSTED SPEEDS OVER 40 M.P.H.
10. REFLECTORIZED BANDS MAY BE OMITTED FROM CONES OR TUBULAR MARKERS FOR LANE CLOSURES DURING DAYLIGHT HOURS.
11. THE STRIPES ON BARRICADES TYPE I AND II SHALL SLOPE DOWNWARD TOWARD THAT SIDE OF WHICH THE APPROACHING VEHICLE MUST PASS. FOR TYPE II-A AND II-B BARRICADES, THE STRIPES SHALL SLOPE DOWNWARD IN THE DIRECTION TOWARD WHICH THE TRAFFIC MUST TURN IN DETOURING, AND WHERE BOTH RIGHT AND LEFT TURNS ARE PROVIDED. THE STRIPING SHALL SLOPE DOWNWARD IN BOTH DIRECTIONS FROM THE CENTER OF THE BARRICADE.
12. ALL BARRICADES SHALL HAVE REFLECTORIZED STRIPING ON THOSE SIDES WHICH ARE APPROACHABLE BY TRAFFIC. WHEN BARRICADES ARE APPROACHABLE BY TRAFFIC IN BOTH DIRECTIONS, SPECIFIED CONSTRUCTION WARNING LIGHTS SHALL HAVE BI-DIRECTIONAL LENSES.
13. FOR BARRICADES TYPE I AND II LESS THAN 3 FEET IN LENGTH, THE WIDTH OF THE STRIPES SHALL BE 4 INCHES.
14. ALL BARRICADES SHALL BE WEIGHTED WITH SANDBAGS FOR STABILITY AS SHOWN.
15. THE DETOUR ARROW SIGN SHALL BE USED ONLY WHEN A DETOUR ROUTE HAS BEEN SIGNED.
16. UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE ON NON-PORTABLE SUPPORTS ONLY AS DESCRIBED HEREIN. A PORTABLE SUPPORT IS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR SHALL LIGHT WEIGHT TRAILER IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. NON-PORTABLE SUPPORTS ARE DRIVEN METAL OR WOODEN POSTS AS DESIGNATED ON STANDARD DETOUR SIGNS SHEET AND SIGN DESIGN DETAILS SHEETS. ALL SIGNS SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT.
17. THE MINIMUM VERTICAL CLEARANCE FOR CONSTRUCTION SIGNS SHALL BE AS FOLLOWS:
 - A) 7 FEET BETWEEN THE TOP AND THE CURB AND THE BOTTOM OF THE SIGN ON CURBED ROADWAY SECTIONS.
 - B) 5 FEET BETWEEN THE EDGE OF PAVEMENT AND THE BOTTOM OF THE SIGN ON NON-CURBED ROADWAY SECTIONS.
 - C) AN ADVISORY SPEED PLATE, REQUIRED TO BE PLACED WITH ANOTHER WARNING SIGN, SHALL BE MOUNTED ON THE POST CLOSEST TO THE ROADWAY AT A MINIMUM CLEARANCE OF 4 FEET ABOVE THE EDGE OF PAVEMENT. THE BOTTOM OF THE CONSTRUCTION WARNING SIGN SHALL NOT BE LOWER THAN THE TOP OF TE ADVISORY SPEED PLATE.
18. THE MINIMUM HORIZONTAL CLEARANCE FOR CONSTRUCTION ON NON-CURBED ROADWAY SECTIONS SHALL BE AS FOLLOVED:
 - A) THE GREATER OF 12 FEET FROM THE EDGE OF THE PAVEMENT OR 6 FEET FROM THE EDGE OF THE PAVED SHOULDER TO THE NEAR EDGE OF THE SIGN FOR NON-PORTABLE SUPPORT MOUNTED SIGNS AND FOR PORTABLE SUPPORT MOUNTED SIGNS DURING THE NIGHT-TIME HOURS.
 - B) 6 FEET FROM THE EDGE OF PAVEMENT TO THE NEAR EDGE OF THE SIGN FOR PORTABLE SUPPORT MOUNTED SIGNS DURING DAYLIGHT HOURS.
19. THE MINIMUM HORIZONTAL CLEARANCE FOR CONSTRUCTION SIGNS ON CURBED ROADWAY SECTIONS SHALL BE 2 FEET FROM THE FACE OF THE CURB TO THE NEAR EDGE OF THE SIGN.
20. LATERAL PLACEMENT OF CONSTRUCTION SIGNS MAY BE ADJUSTED BY THE ENGINEER ONLY IN R/W IS RESTRICTED.
21. A 1 FOOT SECTION OF THE CONCRETE BARRIER SHALL BE REMOVED OR OMITTED EVERY 25 FEET TO PROVIDE DRAINAGE ON TILT PAVEMENTS.

TRAFFIC CONTROL

**CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS**

FIGURE

PS-15

REVISION DATE

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DATE: MAY 21st 2001

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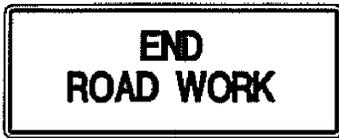
XG20-1
60"x36"

39



XG20-2
60"x24"

40



XG20-2a
60"x24"

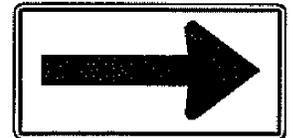
41 42



XM4-9(R or L)
30"x24"

XM4-9-B(R or L)
60"x48"

43 44



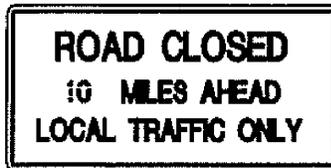
XW1-6
48"x24"
XW1-6-A
60"x30"

45



R11-2
48"x30"

46



R11-3
60"x30"

47



R11-4
60"x30"

TRAFFIC CONTROL SIGN DETAILS

REVISION DATE

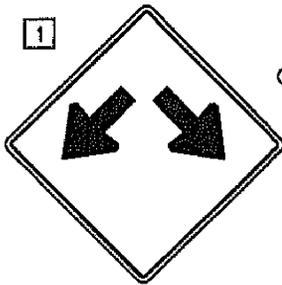
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CITY of MONTICELLO, INDIANA STANDARD DETAILS PUBLIC STREETS AND ALLEYS

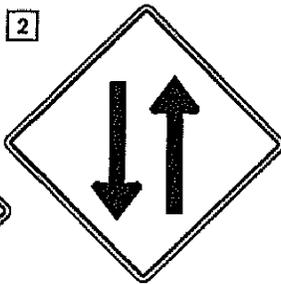
FIGURE

PS-16

DATE: MAY 21st 2001



1
XW12-1-C
48"X48"



2
XW6-3-B
48"X48"



3
XW20-1
48"X48"
XW20-1-A
60"X60"



4
XW20-2
48"X48"



5
XW20-3
48"X48"



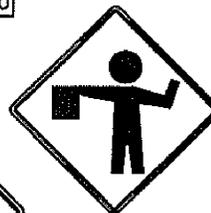
7
XW20-4
48"X48"



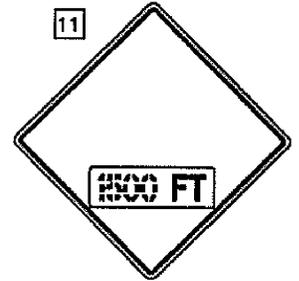
8
XW20-5(RorL)
48"X48"



9
XW20-7
48"X48"



10
XW20-7a
36"X36"



NOTE:

28"X10" METAL PLATE, COVERED WITH FEDERAL ORANGE REFLECTIVE MATERIAL WITH BLACK NUMERALS DESIGNATING PREDETERMINED DISTANCE, MAY BE ATTACHED OVER THE WORD "AHEAD" TO MORE SPECIFICALLY LOCATE THE SUBJECT HAZARD.



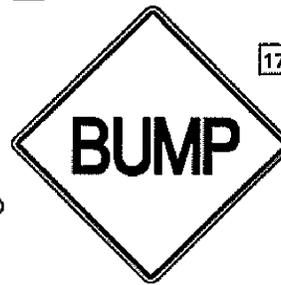
12
XW21-1
36"X36"



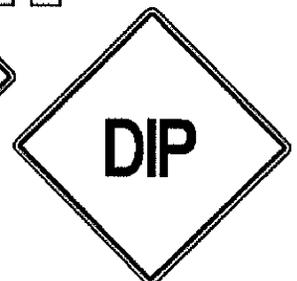
13
XW21-1a
36"X36"



14
XW21-5-A
36"X36"



15
XW8-1-A
36"X36"



17
XW8-2-A
36"X36"

16
XW8-1-B
48"X48"

18
XW8-2-B
48"X48"

TRAFFIC CONTROL SIGN DETAILS

REVISION DATE

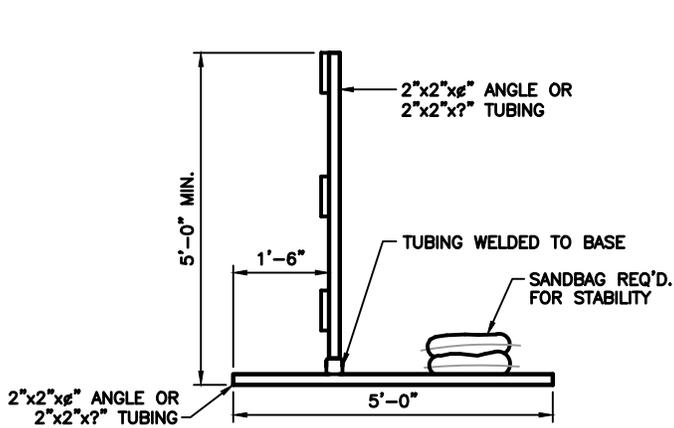
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CITY of MONTICELLO, INDIANA
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PUBLIC STREETS AND ALLEYS

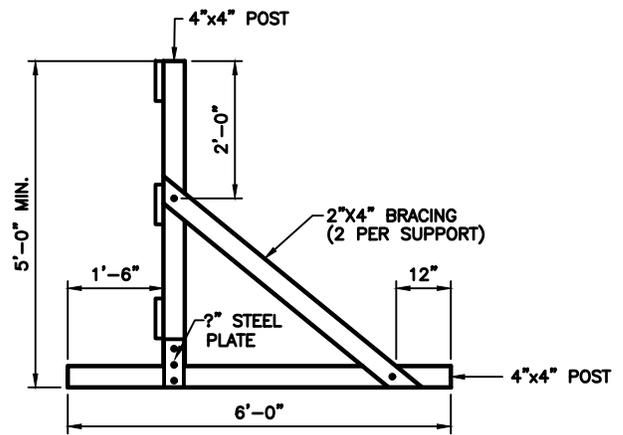
FIGURE

PS-17

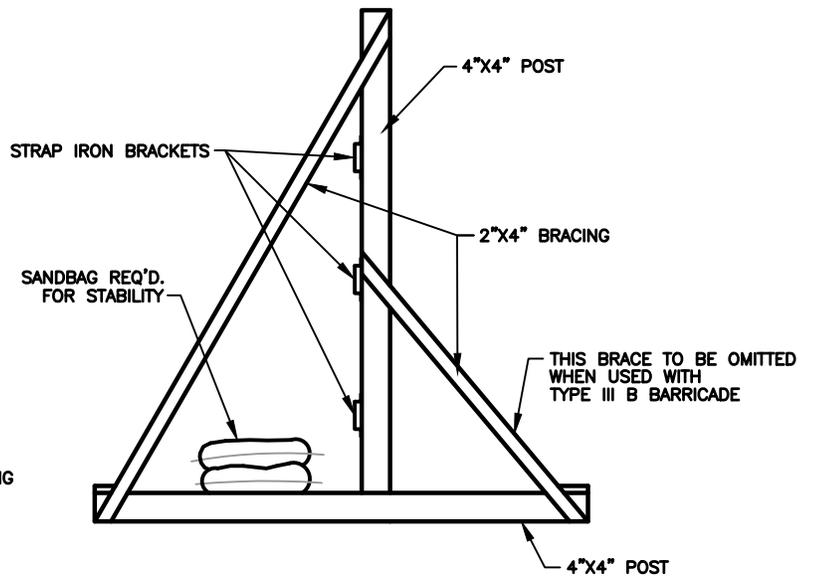
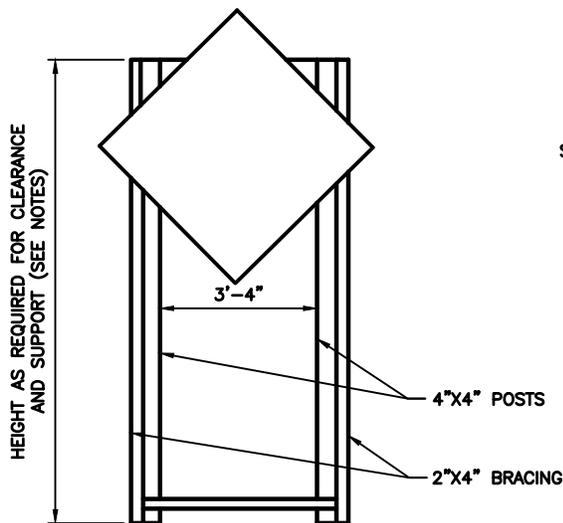
DATE: MAY 21st 2001



STEEL SUPPORTS



WOOD SUPPORTS



DETAIL OF TYPICAL SIGN STANDARD (T.S.S.)

TRAFFIC CONTROL SIGN STANDARD DETAILS

REVISION DATE
09/2019

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**CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS**

FIGURE

PS-18

GENERAL NOTES:

1. DISTANCES SHOWN ARE TYPICAL EXCEPT MINIMUM DISTANCES MAY BE VARIED BASED ON FIELD CONDITIONS.
2. THE SPACING OF DRUMS AND CONES ON TANGENTS SHALL BE AS FOLLOWS:
 - a. WHERE THE POSTED SPEED LIMIT IS 50 mph. OR GREATER, THE SPACING SHALL BE 100 ft.
 - b. WHERE THE POSTED SPEED LIMIT IS LESS THAN OR EQUAL TO 45 mph., THE SPACING SHALL BE 50 ft.
3. THE SPACING OF CHANNELIZING DEVICES ON TAPERS SHALL BE NUMERICALLY EQUAL IN FEET TO THE POSTED SPEED LIMIT IN mph.
4. THE FLASHING ARROW SIGN SHALL NOT BE PLACED ON A SIDEWALK. THE FLASHING ARROW SIGN SHALL BE PLACED AT A DISTANCE OF L/3 FROM THE BEGINNING OF THE TAPER.
5. FOR TEMPORARY LANE CLOSURES DURING DAYLIGHT HOURS, CONES OR TUBULAR MARKERS MAY BE USED IN LIEU OF DRUMS.
6. TEMPORARY PAVEMENT MARKINGS WILL NOT BE REQUIRED FOR TEMPORARY DAYLIGHT LANE CLOSURES.
7. MINIMUM PAVEMENT SECTION FOR 1000 TRUCKS PER DAY OR LESS SHALL CONSIST OF 165 #/syd OF HMA SURFACE, ON 330 #/syd OF HMA INTERMEDIATE, ON 935 #/syd OF HMA BASE, ON 8 IN SUBGRADE TREATMENT. IF THE TRUCK COUNT FOR THE CROSSOVER IS GREATER THAN 1000 TRUCKS PER DAY, THE REQUIRED PAVEMENT SECTION WILL BE PROVIDED ELSEWHERE IN THE PLANS.
8. TEMPORARY HIGHWAY ILLUMINATION, WHEN SPECIFIED, SHALL BE AS DETAILED ELSEWHERE IN THE PLANS.
9. ONCE THE CROSSOVERS HAVE BEEN REMOVED, THE LINE SHALL BE RESTRIPEDED YELLOW, IF THE PAVEMENT IS TO AGAIN TO BE USED FOR ONE-WAY TRAFFIC.
10. FOR TEMPORARY CROSSOVER TYPE B, THIS LINE SHALL BE REMOVED WHEN THE TRAFFIC PATTERN IS SWITCHED.
11. THE ADVISORY SPEED PLATE WILL NOT BE REQUIRED WHEN THE EXISTING POSTED SPEED LIMIT IS LESS THAN 55 mph.
12. SPACING OF CHANNELIZING DEVICES AT THIS LOCATION SHALL BE 20 ft.
13. THE "TWO-WAY TRAFFIC" (XW6-3B) AND "DO NOT PASS" (R4-1-B) SIGNS SHALL ALTERNATE EVERY 2600 ft. THROUGHOUT THE TWO-LANE TWO-WAY OPERATION.
14. FOR A BRIDGE CONTRACT, THE DISTANCE MAY BE ADJUSTED BY THE ENGINEER AS REQUIRED. HOWEVER, IT SHALL BE AS CLOSE TO THE MINIMUM AS POSSIBLE.
15. ONCE THE CROSSOVERS HAVE BEEN REMOVED, THIS LINE SHALL BE RESTRIPEDED BROKEN WHITE, IF THE PAVEMENT IS TO AGAIN BE USED FOR ONE WAY TRAFFIC.

LEGEND

-  REMOVAL OF PAVEMENT MARKINGS AND PRISMATIC REFLECTORS
 -  TYPE III-A OR III-B BARRICADES AS SHOWN
 -  CHANNELIZING DEVICE
 -  FLAGGER
 -  WORK AREA
 -  FLASHING ARROW SIGN
 -  POLICE CAR (OPTIONAL)
 -  CONSTRUCTION SIGN AND SUPPORTS
 -  TEMPORARY PAVEMENT MARKING
 -  TYPICAL SIGN STANDARD (ROAD CLOSURE SIGN ASSEMBLY)
 -  DOUBLE HEADED FLASHING ARROW SIGN
 -  DIRECTION OF TRAFFIC
- W= WIDTH OF OFFSET

TRAFFIC CONTROL DEVICE LEGEND AND GENERAL NOTES

REVISION DATE

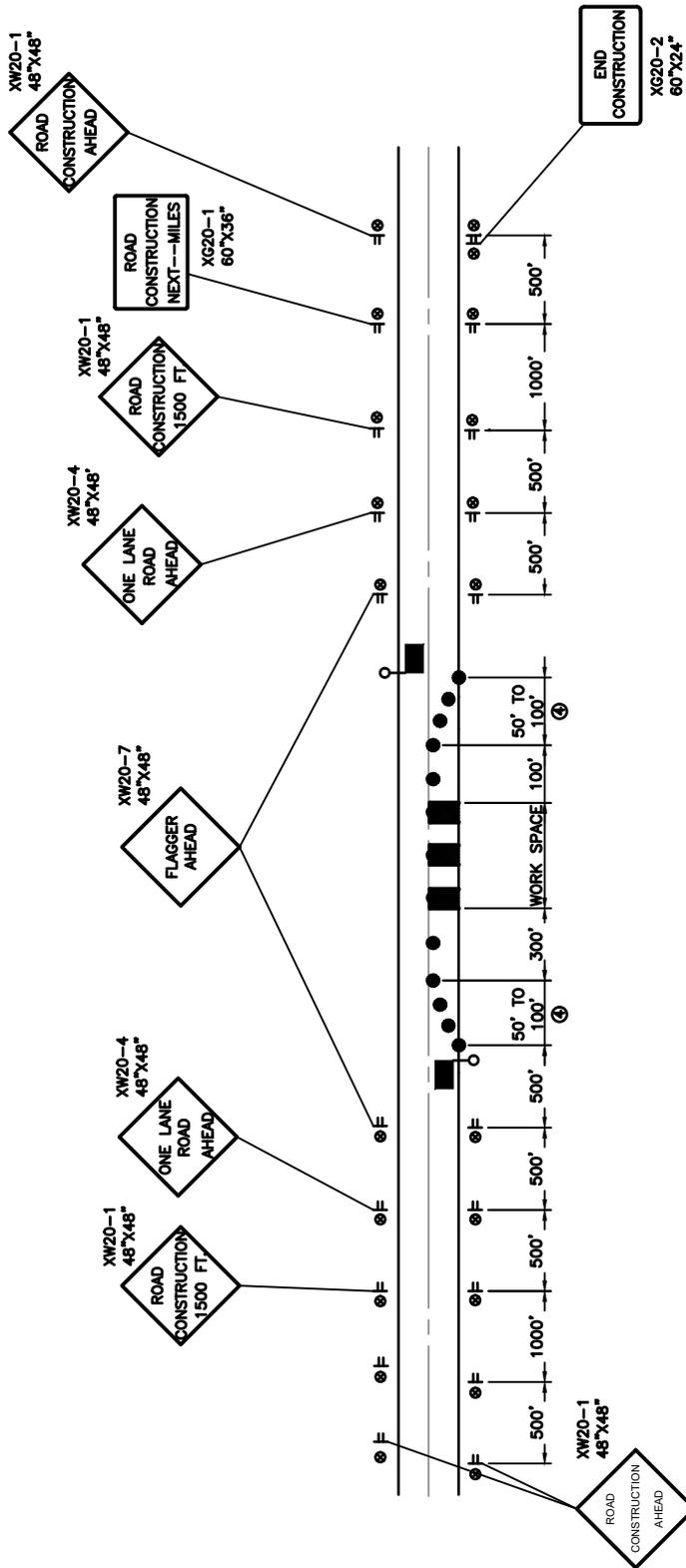
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STANDARD DETAILS
PUBLIC STREETS AND ALLEYS**

DATE: MAY 21st 2001

FIGURE

PS-19



**TYPICAL APPLICATIONS OF TRAFFIC CONTROL DEVICES
FOR SINGLE LANE TWO-WAY TRAFFIC WITH
FLAGGER CONTROL**

DETAIL NO. 1
NO SCALE

TRAFFIC CONTROL DEVICE LOCATIONS

**CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS**

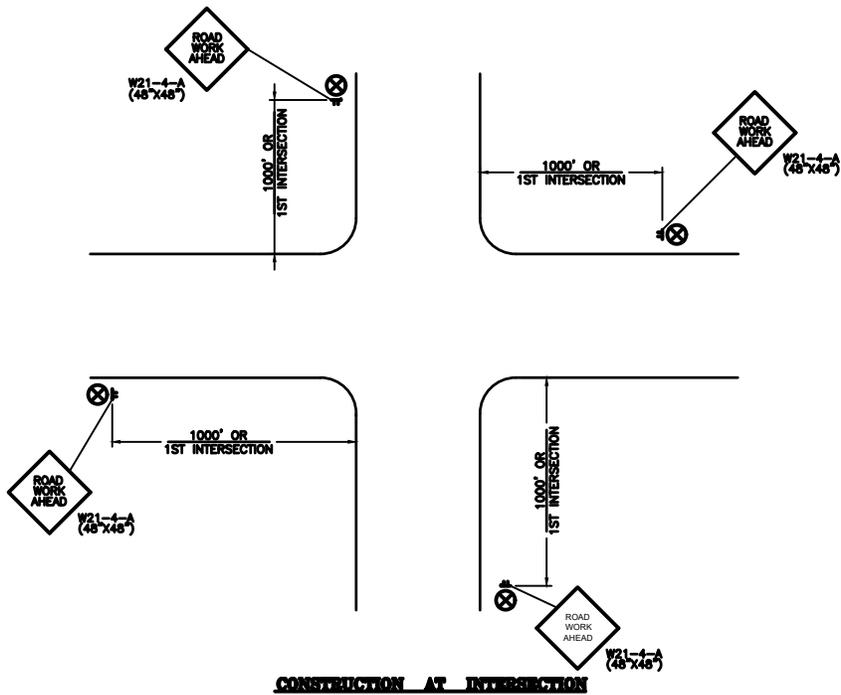
FIGURE

PS-20

REVISION DATE

09/2019

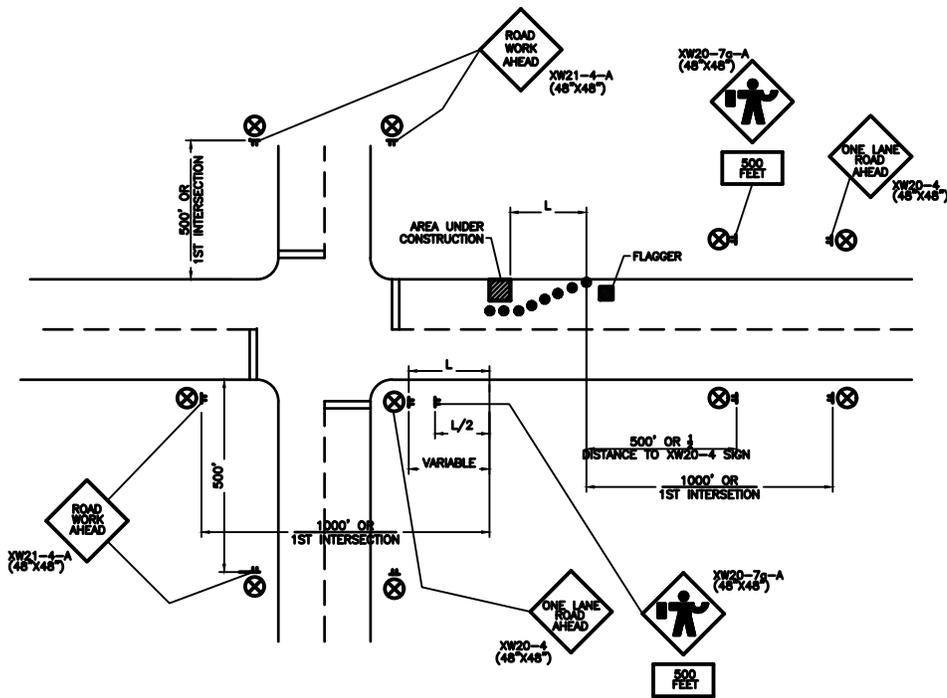
DATE: MAY 21st 2001



CONSTRUCTION AT INTERSECTION

DETAIL NO. 2

NO SCALE



CONSTRUCTION ON PAVEMENT (2 LANES)

DETAIL NO. 3

NO SCALE

TRAFFIC CONTROL DEVICE LOCATIONS

CITY of MONTICELLO, INDIANA
 STANDARD DETAILS
 PUBLIC STREETS AND ALLEYS

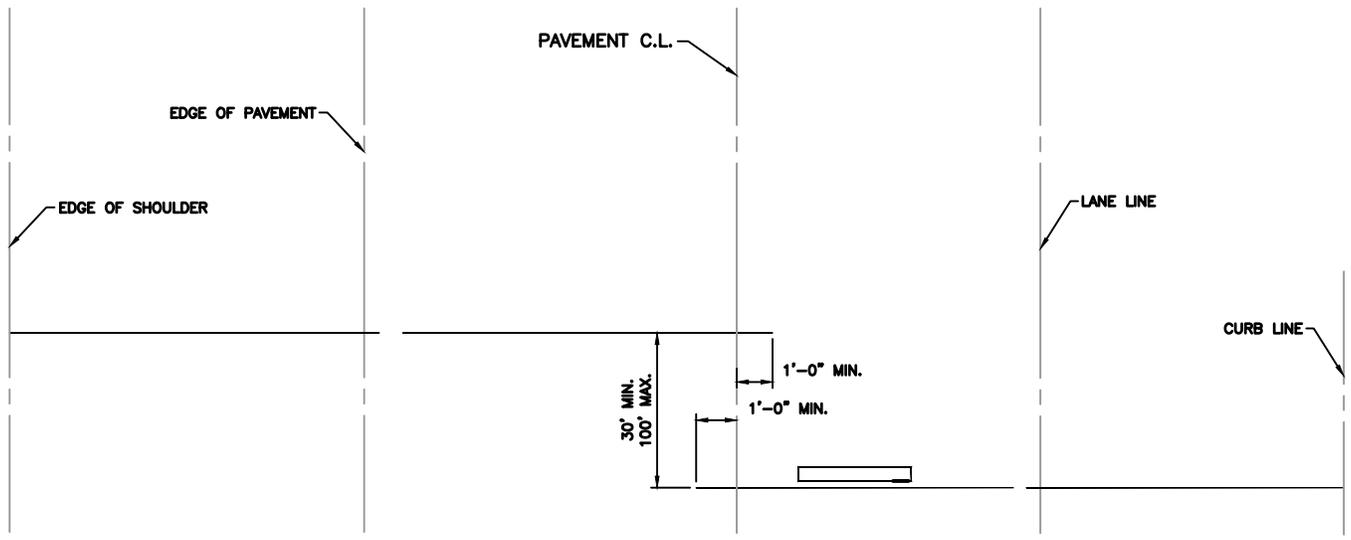
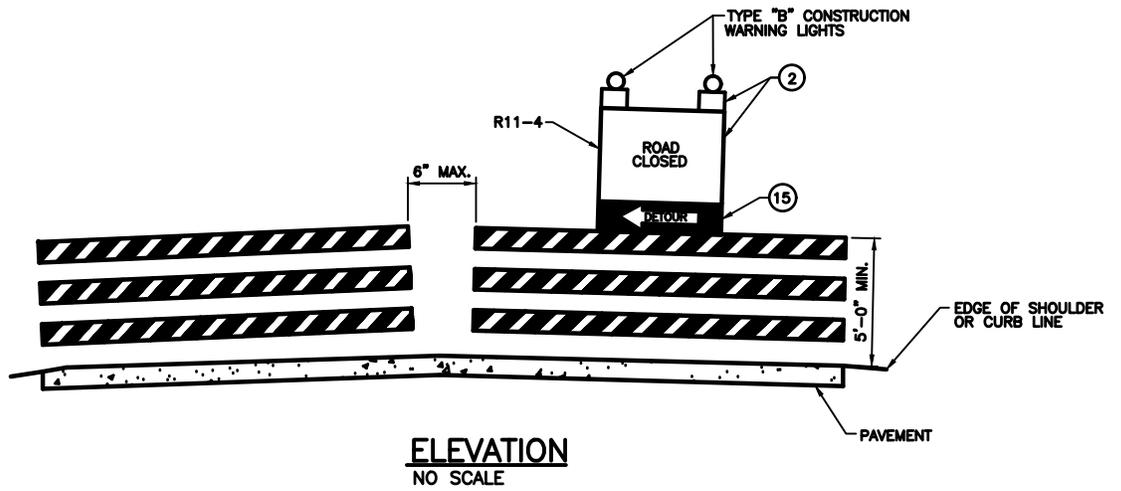
FIGURE

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REVISION DATE

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(HALF PLAN WITH SHOULDER SECTION)

PLAN VIEW
NO SCALE

(HALF PLAN WITH CURB SECTION)

TRAFFIC CONTROL BARRICADE DETAILS

**CITY of MONTICELLO, INDIANA
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PUBLIC STREETS AND ALLEYS**

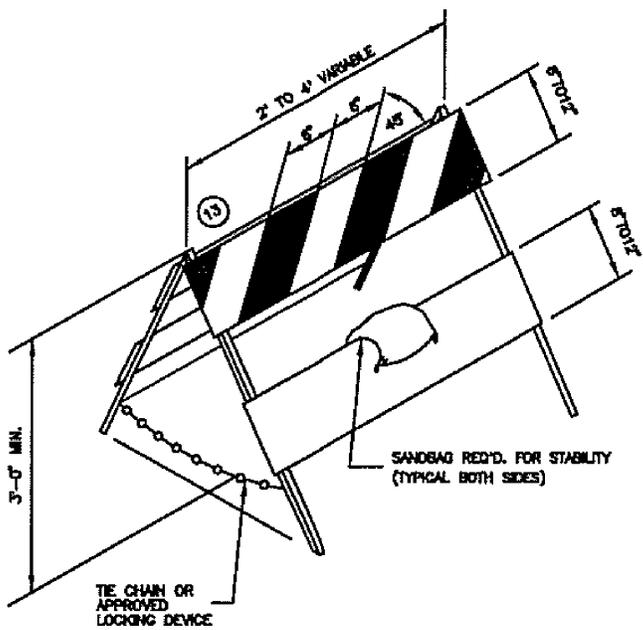
FIGURE

PS-22

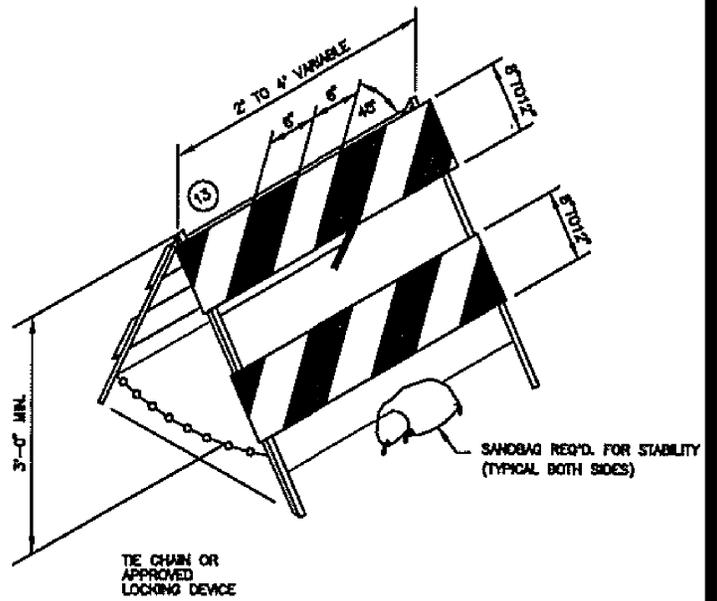
REVISION DATE

09/2019

DATE: MAY 21st 2001



BARRICADE TYPE I



BARRICADE TYPE II

TRAFFIC CONTROL BARRICADE DETAILS

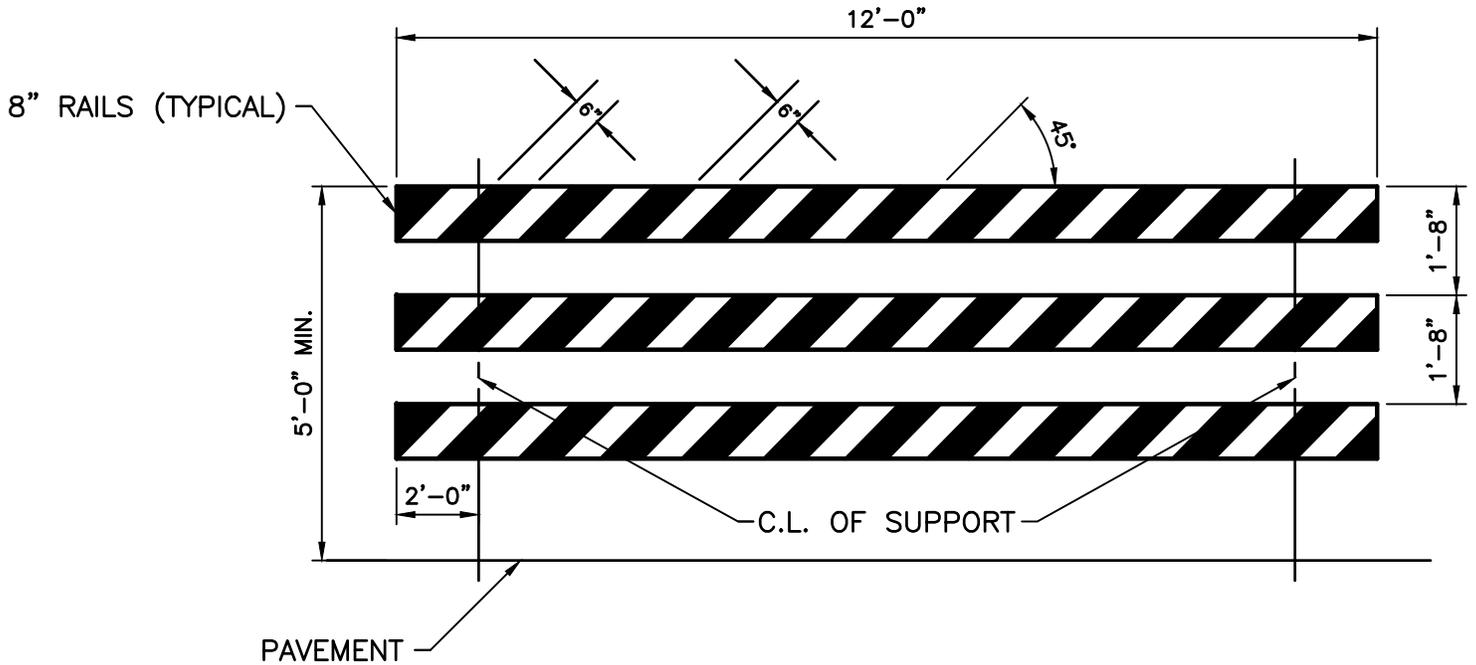
CITY of MONTICELLO, INDIANA
 STANDARD DETAILS
 PUBLIC STREETS AND ALLEYS

FIGURE

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NOTE:
RAILS SHALL BE REFLECTORIZED ON BOTH SIDES

TRAFFIC CONTROL BARRICADE TYPE III-B DETAILS

REVISION DATE

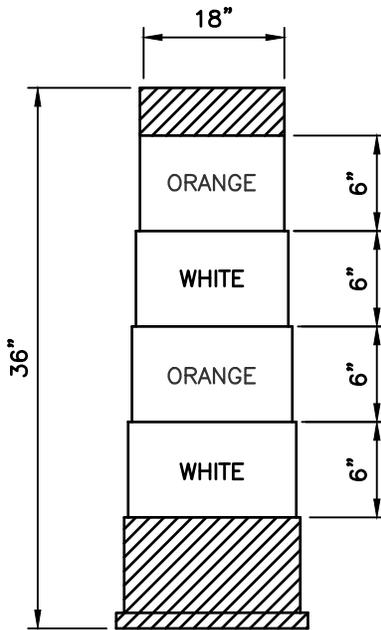
09/2019

**CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS**

FIGURE

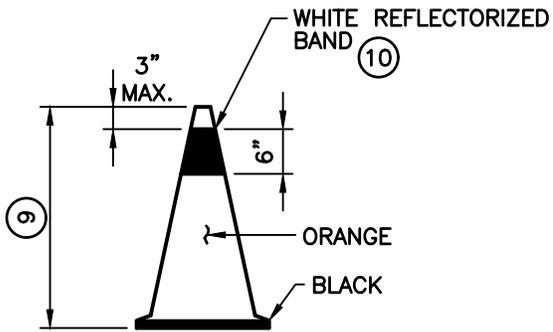
PS-24

DATE: MAY 21st 2001

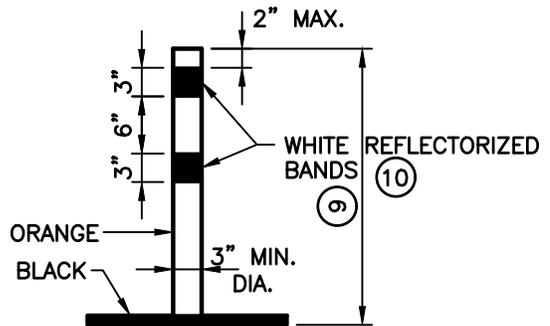


 BLACK OR OTHER APPROVED COLOR

STANDARD DRUM



STANDARD CONE



STANDARD TUBULAR MARKER

TRAFFIC CONTROL CONE/DRUM/TUBULAR MARKER DETAILS

REVISION DATE
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CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS

DATE: MAY 21st 2001

FIGURE

PS-25

GENERAL NOTES

1. TEMPORARY CONCRETE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
2. TEMPORARY CONCRETE BARRIER SHALL BE DELINEATED AS SHOWN. TYPE "C" CONSTRUCTION WARNING LIGHTS SHALL BE ATTACHED AS SHOWN AND SHALL BE SPACED NUMERICALLY EQUAL TO THE 85 PERCENTILE SPEED WITH A MINIMUM SPACING OF 20" C-C. WIDE ANGLE REFLECTORS SHALL BE MOUNTED AS SHOWN AND SHALL BE REQUIRED ON BOTH SIDES OF THE BARRIER WHEN THE BARRIER IS USED TO SEPARATE OPPOSING TRAFFIC.
3. THE DIMENSION OF THE LIFTING SLOTS ARE SUBJECT TO ADJUSTMENT AS NECESSARY TO ACCOMMODATE HANDLING EQUIPMENT.
4. HEX NUT MAY BE TACK WELDED TO BOTTOM SPACER TO FACILITATE INSTALLATION AND REMOVAL. BOLTS SHALL BE TORQUED ONLY TO TIGHT CONDITION. CLEARANCE BETWEEN SPACER S1 AND THE ENDS OF THE BARRIER SHOULD ALLOW ANGULAR DEFLECTION AT THE JOINTS TO PERMIT FLARE RATE OF 10:1.
5. WHEN ANCHORING AGAINST LATERAL MOVEMENT IS SPECIFIED FOR TEMPORARY CONCRETE BARRIER LOCATED ON CONCRETE OR BITUMINOUS PAVEMENT OF SHOULDER, THE METHOD OF ANCHORING SHALL BE AS SHOWN AND SHALL BE ACCORDANCE WITH THE STANDARD SPECIFICATIONS, INDOT, LATEST EDITION.

TRAFFIC CONTROL CONCRETE BARRIER GENERAL NOTES

REVISION DATE
09/2019

CITY of MONTICELLO, INDIANA STANDARD DETAILS PUBLIC STREETS AND ALLEYS

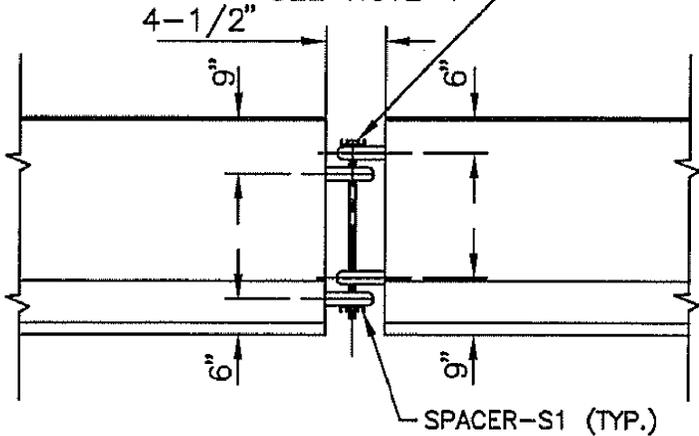
DATE: MAY 21st 2001

FIGURE

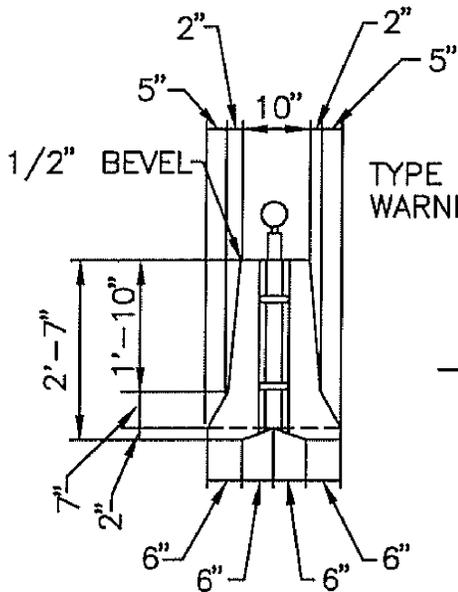
PS-26

1" DIA. BOLT X1'-10 (4" MIN. THREAD, HEX HEAD AND HEX NUT)
SEE NOTE 4

TABLE NO. 1	
CONSTR. ZONE SPEED	TAPER FLARE RATE
20	10:1

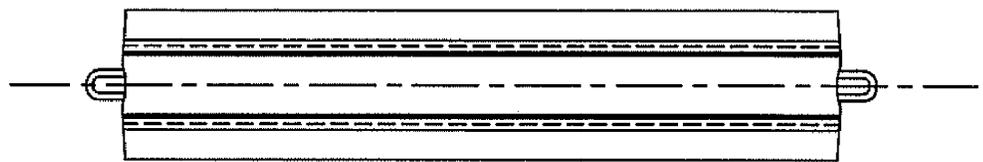


FRONT VIEW
NOT TO SCALE

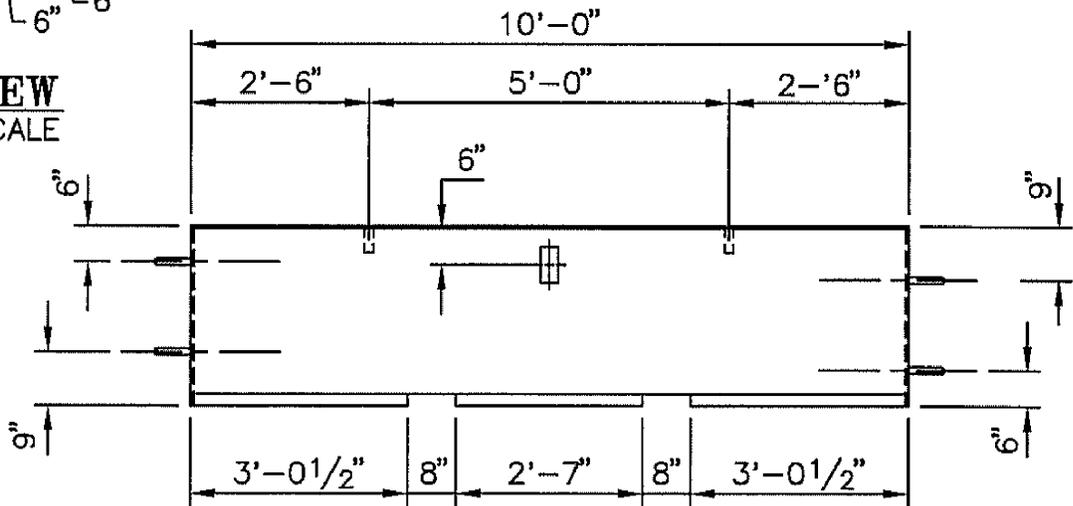


END VIEW
NOT TO SCALE

TYPE "C" CONSTRUCTION
WARNING LIGHT



TOP VIEW
NOT TO SCALE



FRONT VIEW
NOT TO SCALE

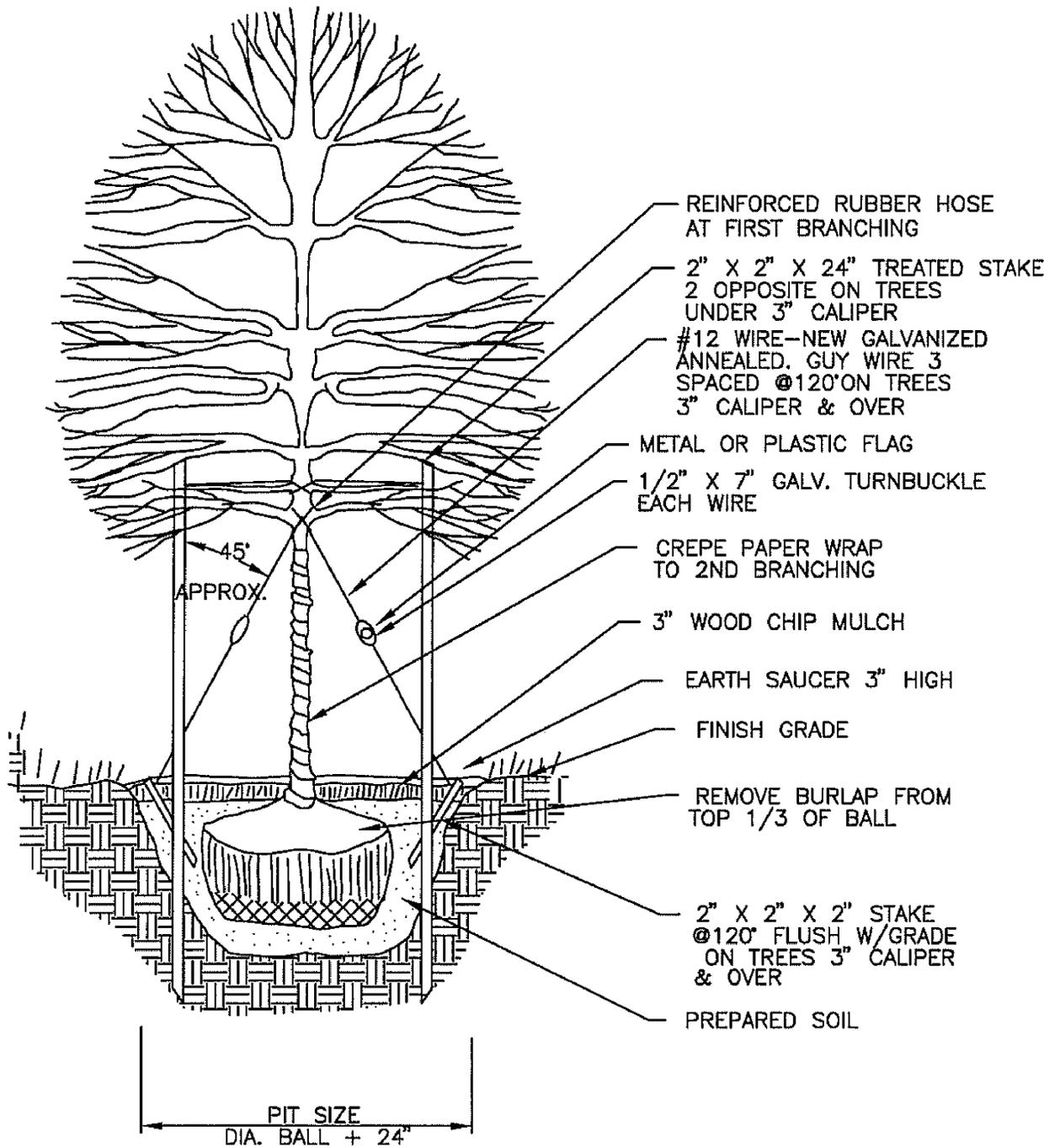
TRAFFIC CONTROL CONCRETE BARRIER DETAIL

REVISION DATE
09/2019

DATE: MAY 21st 2001

CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS

FIGURE
PS-27



TREE PLANTING DETAIL

CITY of MONTICELLO, INDIANA STANDARD DETAILS PUBLIC STREETS AND ALLEYS

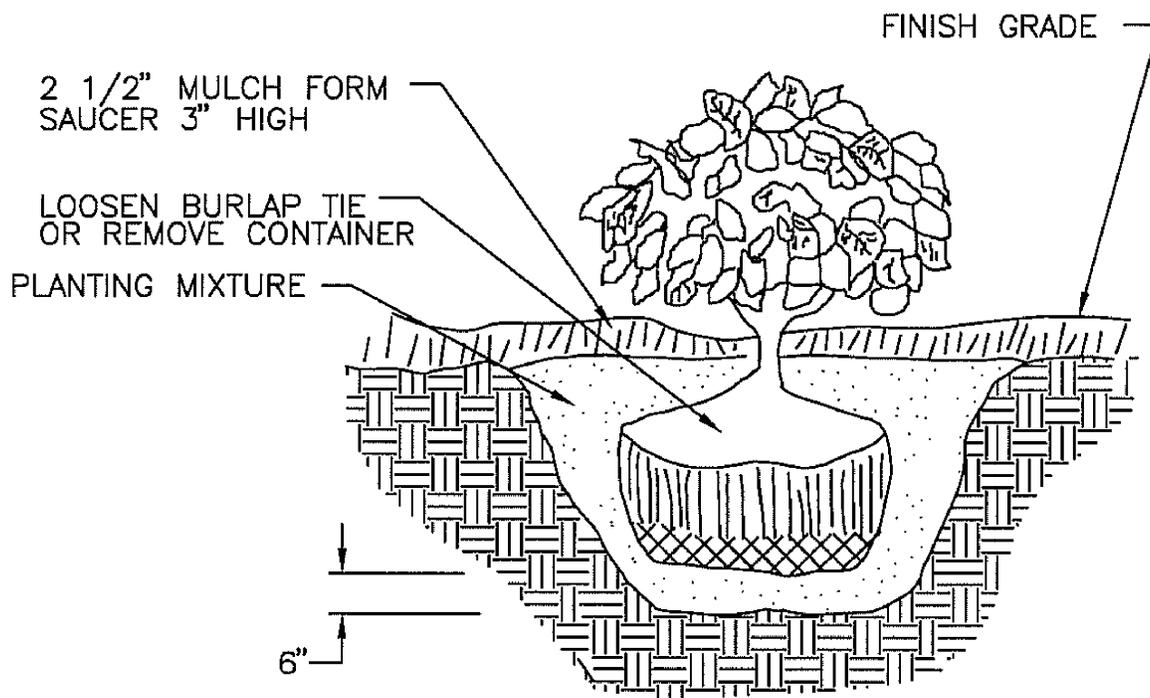
FIGURE

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SHRUB PLANTING DETAIL

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PUBLIC STREETS AND ALLEYS**

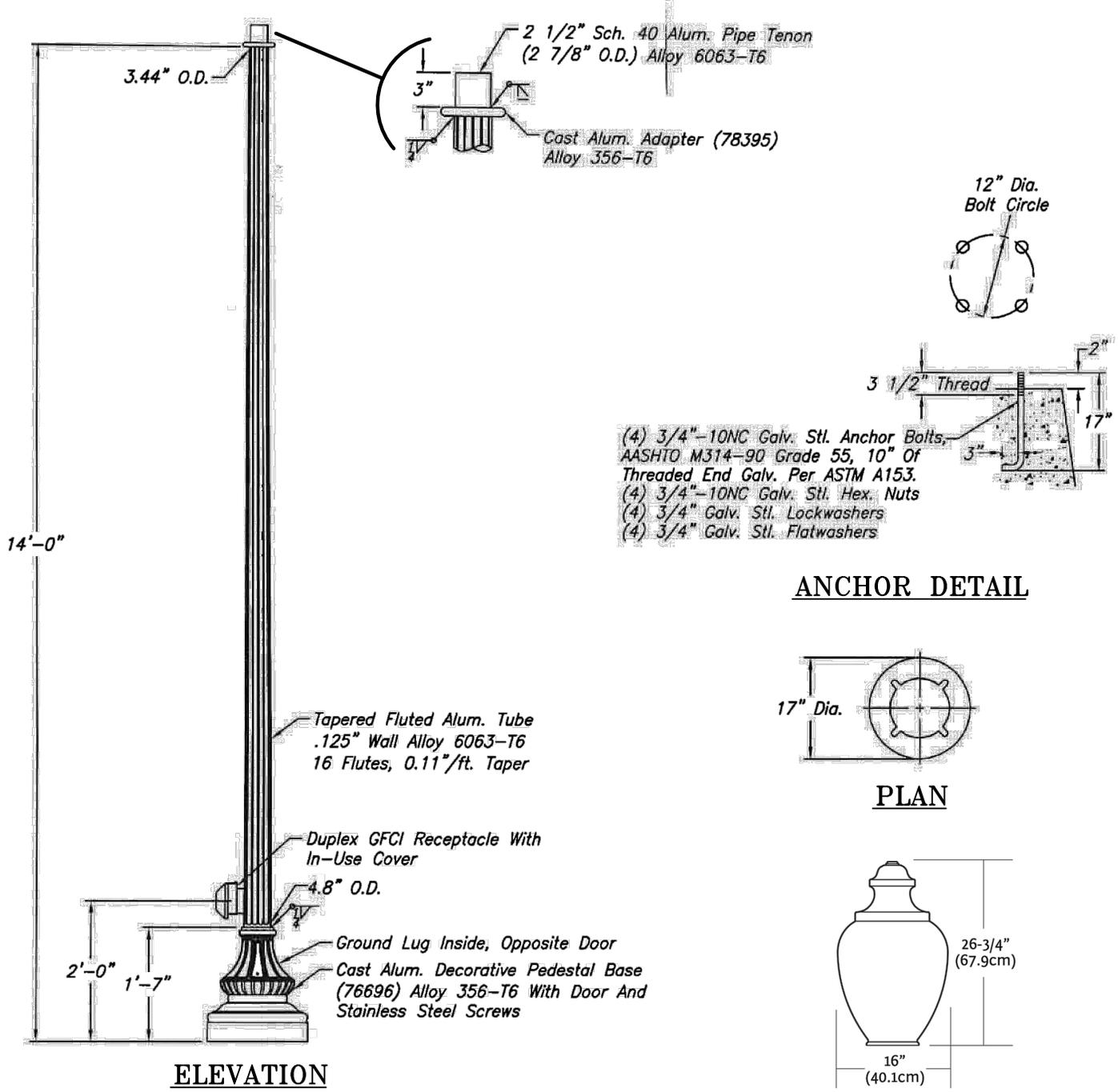
FIGURE

PS-29

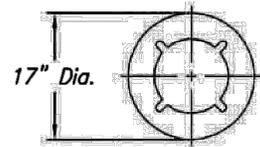
REVISION DATE

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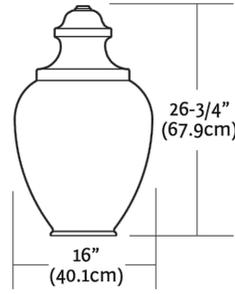
DATE: MAY 21st 2001



ANCHOR DETAIL



PLAN



ACT

ACRYLIC, CLEAR TEXTURED (STANDARD)

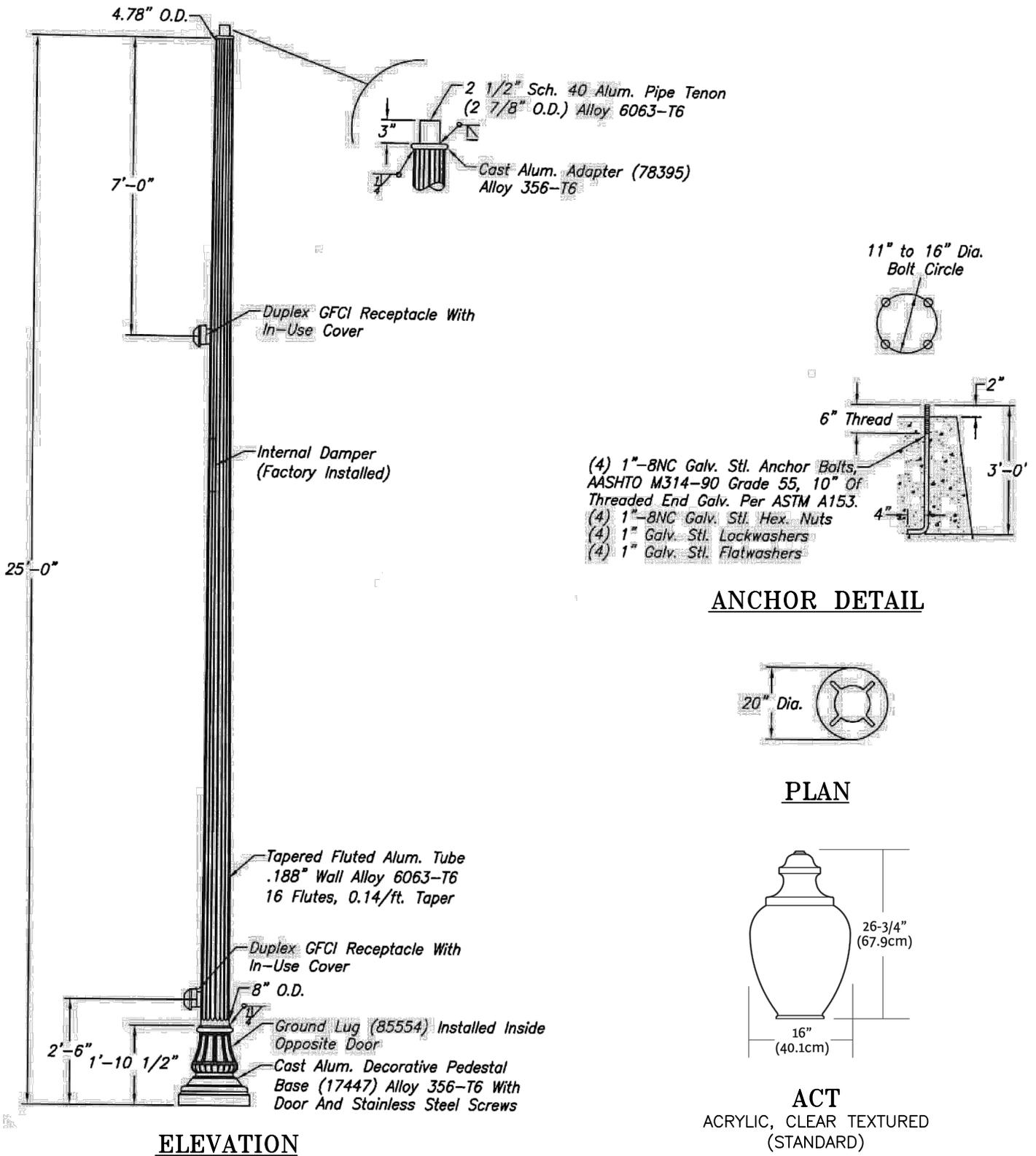
14- FEET LIGHTING POLE DETAIL

REVISION DATE
09/2019

DATE: MAY 21st 2001

**CITY of MONTICELLO, INDIANA
STANDARD DETAILS
PUBLIC STREETS AND ALLEYS**

FIGURE
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25-FEET LIGHTING POLE DETAIL

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DATE: MAY 21st 2001

**CITY of MONTICELLO, INDIANA
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FIGURE
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