

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION

Road No./County:

S. Main Street (US 421/State Road (SR) 39), Airport Road, Gordon Road, and City Park Loop/White County

Designation Number(s):

1702835

Project
Description/Termini:

Intersection Improvement Project in White County. Along Gordon Road the project begins approximately 367 feet west of the intersection and ends at the intersection. Along S. Main Street (US 421/ SR 39) the project begins approximately 650 feet north of the intersection with Gordon Road to approximately 460 feet south. Along Airport Road, the project begins approximately 375 south of the intersection with Gordon Road and S. Main Street and ends at the intersection. Along City Park Loop, the project begins at the intersection with S. Main Street and ends approximately 357 feet east of the intersection. Along Oak Road, the project begins at the intersection with S. Main Street and ends approximately 76 feet east of the intersection.

	Categorical Exclusion, Level 2 – Required Signatories: INDOT DE and/or INDOT ESD
	Categorical Exclusion, Level 3 – Required Signatories: INDOT ESD
X	Categorical Exclusion, Level 4 – Required Signatories: INDOT ESD and FHWA
	Environmental Assessment (EA) – Required Signatories: INDOT ESD and FHWA
	Additional Investigation (AI) – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority

Approval

INDOT DE Signature and Date

INDOT ESD Signature and Date

FHWA Signature and Date

Release for Public Involvement

N/A

INDOT DE Initials and Date



7-8-2021

INDOT ESD Initials and Date

Certification of Public Involvement

INDOT Consultant Services Signature and Date

INDOT DE/ESD Reviewer Signature and Date:

Name and Organization of CE/EA Preparer:

Chris Kunkel/Lochmueller Group

Indiana Department of Transportation

County White Route Gordon Road, S. Main Street (US 421/SR 39), S. Airport Road, City Park Loop, Oak Drive Des. No. 1702835

Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*? ☐ Yes ☒ No
If No, then:
Opportunity for a Public Hearing Required? ☒ ☐

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were mailed to potentially affected property owners within the project area on March 8, 2019 notifying them about the project and that individuals responsible for land surveying and field activities may be seen in the area. A sample copy of the Notice of Entry letter is included in Appendix G, G1.

Section 106:

To meet the public involvement requirements of Section 106, a legal notice of FHWA finding of "No Adverse Effect" was published in the *Monticello Herald Journal* on March 11, 2021 offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on April 10, 2021. No comments were received within the 30-day comment period. The text of the public notice and the affidavit of publication appear in Appendix D, page D110 to D111.

Public Involvement:

The project will meet the minimum requirements described in the current *Indiana Department of Transportation (INDOT) Public Involvement Manual* which requires the project sponsor to offer the public an opportunity to submit comment and/or request a public hearing. Therefore, a legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

Section 4(f):

Due to impacts that constitute a *de minimis* use to Monticello City Park, a Section 4(f) resource, a public notice was published in the *Monticello Herald Journal* on November 4, 2020. The notice offered the public an opportunity to submit comments regarding the effect the project will have on the public recreational resource. The public comment period ended 30 days later on December 4, 2020. No responses were received within the 30-day comment period.

Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or to natural resource.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: White County Board of Commissioners INDOT District: LaPorte

Local Name of the Facility: Gordon Road, S. Main Street, Airport Road, and City Park Loop

Funding Source (mark all that apply): Federal ☒ State ☐ Local ☒ Other ☐

*If other is selected, please identify the _____

This is page 2 of 28 Project name: Gordon Road Intersection Improvement Project Date: July 1, 2021

Indiana Department of Transportation

County White Route Gordon Road, S. Main Street (US 421/SR 39), S. Airport Road, City Park Loop, Oak Drive Des. No. 1702835

funding source:

PURPOSE AND NEED:

The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.

Need:

The need for this project stems from the poor level of service (LOS) for the existing five-legged intersection. There are six LOS levels ranging from LOS A (free flow) to LOS F (oversaturated). LOS C is considered acceptable. According to a traffic study performed by Lochmueller Group in January 2020 (Appendix I, I14 to I23), the existing LOS (2019) for the overall intersection is LOS C during both AM (7:30 AM to 8:30 AM) and PM (4:30 PM to 5:30 PM) peak hours. All approaches exhibit LOS C or better with the exception of the eastbound approach (Gordon Road) which exhibits LOS D during the AM peak hour and LOS E at PM peak hour. The design year (2042) overall LOS for the current signalized intersection is forecasted to be LOS F, which as stated above is oversaturated and considered unacceptable. The eastbound and northbound approaches are forecasted to have LOS of D or worse for both the AM and PM Peaks. The remaining approaches are forecasted to have LOS C or better. An additional need of the project is the lack of availability of non-motorized transportation routes in the area of the intersection. Currently, no pedestrian facilities exist that connect the north entrance of Monticello City Park to the south entrance of Monticello City Park nor does the multi-use path along the north side of Gordon Road (Monticello/White County Trail) connect to Monticello City Park.

Purpose:

The purpose of the proposed project is to improve the LOS of each of the approaches to a minimum of LOS C for the design year and to improve the connection of the existing pedestrian facilities available in the area.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: White Municipality: Monticello

Limits of Proposed Work: Along Gordon Road the project begins approximately 367 feet west of the intersection and ends at the intersection with S. Main Street. Along S. Main Street (US 421/ SR 39) the project begins approximately 650 feet north of the intersection with Gordon Road to approximately 460 feet south. Along Airport Road, the project begins approximately 375 south of the intersection with Gordon Road and S. Main Street and ends at the intersection. Along City Park Loop, the project begins at the intersection with S. Main Street and ends approximately 357 feet east of the intersection. Along Oak Road, the project begins at the intersection with S. Main Street and ends approximately 76 feet east of the intersection.

Total Work Length: 1.4 Mile(s) Total Work Area: 3.27 Acre(s)

Is an Interstate Access Document (IAD)¹ required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes ¹	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: <input type="text"/>	

¹If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.

Location:

The project is located at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop within the City of Monticello, White County, Indiana. Specifically, the project is located in Section 4, Township 26 North, Range 3 West of Union Township as depicted on the Monticello South U.S. Geological Survey 1:24,000 scale quadrangle (Appendix B, B2).

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Existing Conditions:

The existing intersection is a five-legged intersection that is controlled with a traffic signal. Within the project area, S. Main Street (US 421/SR 39) is a north-south roadway functionally classified as a minor arterial. The roadway typical cross-section consists of two 12-foot wide asphalt travel lanes (one in each direction). The shoulder along the southbound lane is asphalt and is approximately 9 feet wide. Along the northbound lane, there is a 6-foot to 12-foot wide asphalt paved shoulder. Additionally, the southbound approach of S. Main Street widens near Gordon Road to provide for an 11-foot designated right-turn lane. North of the Gordon Road intersection, along the east paved shoulder, there is also an additional 2 to 12-foot wide aggregate pull-off that serves as public street parking for Monticello City Park. There is also an existing 5-foot wide concrete sidewalk that extends for approximately 235 feet in front of the Dollar General store on the west side of S Main Street (US 421/SR 39) north of the intersection to be improved. The City of Monticello is advancing a project to add sidewalks along both sides of Main Street (Des. No. 1600938) that terminate on the east side at the north entrance to the park and on the west side at the existing sidewalk in front of the Dollar General. The posted speed limit is 35 miles per hour (mph).

Airport Road is a north-south roadway functionally classified as a minor arterial. The typical roadway section consists of two 11-foot wide asphalt travel lanes (one in each direction) with no discernible shoulder. The posted speed limit is 25 mph.

Gordon Road is an east-west roadway functionally classified as a minor arterial. The roadway typical section consists of two 11-foot wide asphalt travel lanes (one in each direction) with asphalt shoulders of varying size, up to 2-foot wide on both sides of the roadway. The posted speed limit is 35 mph. An existing 10-foot paved multi-use path runs along the north side of Gordon Road, Monticello/White County Trail, separated from the roadway by an approximately 3-foot wide grass area, on the west side of the project area until it reaches the intersection with S. Main Street. Currently, there are Americans with Disabilities Act (ADA) compliant curb ramps at the northwest corner of Gordon Road and S. Main Street. However, no crosswalk exists to cross either Gordon Road or S. Main Street at the intersection.

Oak Drive is an east-west roadway functionally classified as a local road. The roadway typical section consists of two 10-foot asphalt travel lanes (one in each direction) with no discernible shoulder. The posted speed limit is 25 mph.

City Park Loop is a one-way roadway that runs through Monticello City Park. The existing section consists of one 10-foot wide travel lane and no discernible shoulder. City Park Loop serves as vehicular access to the park property that enters to the north of the intersection and exits at the intersection to be improved by this project. The posted speed limit is 10 mph.

Existing storm water drainage at the intersection is currently directed by shallow roadside swales and an existing enclosed storm sewer system.

The City of Monticello has a sidewalk construction project (Des. No. 1600938) north of this intersection improvement project. That project, which is being processed separately from this project, will add sidewalks along both sides of S. Main Street north to Tioga Road. The sidewalks being constructed along the east side of S. Main Street as part of Des. No. 1600938 will end on the north side of City Park Loop and at the existing sidewalk in front of Dollar General on the west side of S. Main Street. Des. No. 1600938 is independent of this intersection improvement project as it provides pedestrian connectivity to existing facilities and destinations and does not rely on any other project to facilitate pedestrian movement to these facilities and destinations. The construction of Des. No. 1600938 should be completed ahead of this project. Construction for Des. No. 1600938 is set to begin in 2021.

Adjacent land use consists of residential development, commercial properties, as well as recreational and religious facilities.

Preferred Alternative:

The preferred alternative involves the construction of a roundabout at the five-legged intersection. The roundabout will consist of one, 18-foot wide asphalt circulating travel lane and a 12-foot, 6-inch wide concrete truck apron with a 2-foot wide mountable concrete roll curb. The center of the roundabout will consist of a 115-foot diameter sodded area.

The approaches of Gordon Road, S. Main Street, Airport Road, and City Park Loop will all shift generally to the south or southeast and widen approaching the proposed roundabout. The shifting of northbound S. Main Street will require the relocation of 27 concrete markers currently used to delineate the boundary of the Monticello City Park property. Up to 200 feet of the existing alignment of City Park Loop at its exit to S. Main Street will be realigned to the south. The project will also include updates to the curb and curbside drainage along the affected roadways. All reconstructed roadways will include 2-foot, 7-inch curb and gutter along both sides of the roadway. Additionally, a section of Oak Drive will be reconstructed to full depth with new curbs and drainage. Oak Drive will maintain its current alignment and will not tie into the roundabout. Instead, it will continue to connect with S. Main Street south of the

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reconstructed intersection.

The new roadway typical section of Gordon Road will consist of two 12-foot wide travel lanes (one in each direction) that widen to 17 feet as they get near to the new roundabout. There will also be a raised median with a roll curb that will vary in width from 2 feet to 31 feet, 8 inches. The existing shoulder will be replaced by the 2-foot, 7-inch curb and gutter on both sides.

The new roadway typical section of S. Main Street (US 421/SR 39) will consist of two 12-foot wide travel lanes (one in each direction) that widen to 16 feet, 11 inches as they near the roundabout. The typical section will also have a raised median with a roll curb that will range from 3 feet, 6 inches to 28 feet, 9 inches wide north of the new roundabout and from 2 feet, 7 inches to 20 feet, 6 inches wide south of the roundabout. There will also be an 8-foot, 6-inch wide shoulder along the west side of the road north of the new roundabout. South of the roundabout, there will be paved shoulders on both sides of the road ranging in width from 2 feet, 7 inches to 6 feet wide.

The new roadway typical section of S. Airport Road will consist of two 12-foot wide travel lanes (one in each direction) that widen to 17-feet, 10 inches wide on the northbound lane and 15 feet, 3 inches wide on the southbound lane near the roundabout. The typical section will also have a raised median with a roll curb that will range from 5 feet wide to 31 feet, 10 inches wide. The northbound lane will also have an 8-foot wide shoulder that transitions to new curb and gutter as it approaches the new roundabout.

The new roadway typical section of City Park Loop will consist of a one-way (westbound) 14-foot to 18-foot, 5-inch westbound travel lane with a 10-foot-wide hot mix asphalt (HMA) multi-use path that will be separated by a grass area that varies from 2-feet, 6 inches wide to 50 feet wide.

The new roadway typical section of Oak Drive will consist of two travel lanes that vary from 9 feet, 3 inches to 9 feet, 10 inches wide in each direction.

The project will construct additional non-motorized facilities in the area of the intersection. The existing multi-use path (Monticello/White County Trail) along the north side of Gordon Road within the project area will be constructed to follow the shifted alignment of Gordon Road as it approaches the roundabout and turn north along the west side of S. Main Street where it will end at the first commercial entrance drive north of the intersection. At the roundabout, there will be a crosswalk that will provide non-motorized transportation access to Monticello City Park. Within the park, a 10-foot wide multi-use path will be constructed starting at a point southeast of the baseball field and follows along the north side of City Park Loop and then head north along the east side of S. Main Street where it will end at the north entrance of Monticello City Park.

The project will also involve the addition of 14, 40-foot tall light poles at the intersection (Appendix B, B44). The project will require that 27 of the concrete markers at the southwest corner of the park property be relocated outside the proposed right-of-way (ROW) limits and north of the proposed sidewalk (Appendix B, B38). The sign for Monticello Christian Church at the southwest corner of the intersection is located within the temporary ROW limits required for the construction of a temporary travel lane between Gordon Road and S. Airport Road as part of the Maintenance of Traffic (MOT) (Appendix B, B28 and B40). As a result, this sign will be relocated nearer to the church within the ROW limits after construction is complete.

The project will also require new drainage within the project area. This includes new drainage inlets, manholes, and drainage that ties into existing combined sewer system.

The MOT will take place in phases to ensure that thru traffic has access to the intersection along S. Main Street throughout construction (Appendix B, B25 to B36). A detour for Gordon Road and S. Airport Road will be in place to facilitate traffic to S. Main Street. The detour will utilize Gordon Road, 6th Street, and US 24. See the *Maintenance of Traffic (MOT) During Construction* section of the document for details.

Every effort to avoid, minimize, and/or mitigate project impacts will be made.

After construction of the preferred alternative, the overall LOS of the intersection will be LOS A in both the AM and PM peak hours in the construction year (2022) and an LOS A for the AM peak hours and an LOS B for the PM peak hours for the Design Year (2042). This meets the need of achieving an LOS of at least a LOS C. Constructing new and extending existing non-motorized facilities in the area will improve the availability of these facilities within the area of the intersection.

Logical Termini/Independent Utility:

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The termini for the project are the logical beginning and endpoint of the project to improve this intersection and meet the purpose and need of the project. This project is independent of any other project and will not rely on the construction of any other project to be completed. The pedestrian facilities provide connectivity to existing facilities or destinations such as Monticello City Park, adjacent residential areas, and churches.

OTHER ALTERNATIVES CONSIDERED:

Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.

No Build Alternative: This alternative would not involve any improvements to the intersection. This alternative would not involve any immediate cost or result in any environmental impacts. If no improvements are made to the intersection, the LOS of the intersection will likely become sub optimal and there would be no improvement in the availability of non-motorized facilities in the area. This alternative was dismissed from further consideration because it would not address the purpose and need of the project.

No other alternatives were considered for this project.

The No Build Alternative is not feasible, prudent or practicable because (Mark all that apply)

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems; or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe):

X

ROADWAY CHARACTER:

If the proposed action includes multiple roadways, complete and duplicate for each roadway.

Name of Roadway Gordon Road
 Functional Classification: Minor Arterial
 Current ADT: 5,228 VPD (2022) Design Year ADT: 7,956 VPD (2042)
 Design Hour Volume (DHV): 718 Truck Percentage (%) 6.8%
 Designed Speed (mph): 35 Legal Speed (mph): 35

Existing			Proposed		
Number of Lanes:	2		2		
Type of Lanes:	Through Lanes		Through Lanes		
Pavement Width:	22-26	ft.	34-38	ft.	
Shoulder Width:	0-2	ft.	N/A	ft.	
Median Width:	N/A	ft.	2-31.7	ft.	
Sidewalk Width:	10	ft.	10	ft.	

Setting:

Topography:

X

Urban
Level

Suburban
Rolling

X

Rural
Hilly

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Name of Roadway S. Main Street (US 421/SR 39)
Functional Classification: Minor Arterial

Current ADT: 9,084 VPD (2022) Design Year ADT: 13,823 VPD (2042)
Design Hour Volume (DHV): 840 Truck Percentage (%) 4.3%
Designed Speed (mph): 35 Legal Speed (mph): 35

Existing			Proposed		
Number of Lanes:	3		2		
Type of Lanes:	2 Through Lanes, 1 turn lane		Through Lanes		
Pavement Width:	39-45	ft.	25.5-36	ft.	
Shoulder Width:	6-12	ft.	2.6-8.5	ft.	
Median Width:	N/A	ft.	2-30	ft.	
Sidewalk Width:	5	ft.	10	ft.	

Setting: ☐ Urban ☐ Suburban ☒ Rural
Topography: ☒ Level ☐ Rolling ☐ Hilly

Name of Roadway S. Airport Road
Functional Classification: Minor Arterial
Current ADT: 2,079 VPD (2022) Design Year ADT: 3,164 VPD (2042)
Design Hour Volume (DHV): 275 Truck Percentage (%) 10.2%
Designed Speed (mph): 25 Legal Speed (mph): 25

Existing			Proposed		
Number of Lanes:	2		2		
Type of Lanes:	Through Lanes		Through Lanes		
Pavement Width:	22	ft.	24-32	ft.	
Shoulder Width:	N/A	ft.	8	ft.	
Median Width:	N/A	ft.	5-32	ft.	
Sidewalk Width:	N/A	ft.	N/A	ft.	

Setting: ☐ Urban ☐ Suburban ☒ Rural
Topography: ☒ Level ☐ Rolling ☐ Hilly

Name of Roadway City Park Loop
Functional Classification: N/A
Current ADT: N/A VPD (20--) Design Year ADT: N/A VPD (20--)
Design Hour Volume (DHV): N/A Truck Percentage (%) N/A
Designed Speed (mph): 10 Legal Speed (mph): 10

Existing			Proposed		
Number of Lanes:	1		1		
Type of Lanes:	Through Lanes		Through Lanes		
Pavement Width:	10	ft.	14-18.5	ft.	
Shoulder Width:	N/A	ft.	N/A	ft.	
Median Width:	N/A	ft.	N/A	ft.	
Sidewalk Width:	N/A	ft.	10	ft.	

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Setting: ☐ Urban ☐ Suburban ☒ Rural
Topography: ☒ Level ☐ Rolling ☐ Hilly

Name of Roadway Oak Drive
Functional Classification: Local Road
Current ADT: N/A VPD (20--) Design Year ADT: N/A VPD (20--)
Design Hour Volume (DHV): N/A Truck Percentage (%) N/A
Designed Speed (mph): N/A Legal Speed (mph): N/A

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	Through Lanes		Through Lanes	
Pavement Width:	20	ft.	18.6-19.7	ft.
Shoulder Width:	N/A	ft.	N/A	ft.
Median Width:	N/A	ft.	N/A	ft.
Sidewalk Width:	N/A	ft.	N/A	ft.

Setting: ☐ Urban ☐ Suburban ☒ Rural
Topography: ☒ Level ☐ Rolling ☐ Hilly

BRIDGES AND/OR SMALL STRUCTURE(S):

If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
(Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	N/A		N/A	
Number of Spans:	N/A		N/A	
Weight Restrictions:	N/A	ton	N/A	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	N/A	ft.	N/A	ft.
Outside to Outside Width:	N/A	ft.	N/A	ft.
Shoulder Width:	N/A	ft.	N/A	ft.

Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.

No bridges or small structures are located within the project area.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?		X
Is a temporary roadway proposed?	X	
Will the project involve the use of a detour or require a ramp closure? (describe below)	X	
Provisions will be made for access by local traffic and so posted.	X	
Provisions will be made for through-traffic dependent businesses.	X	
Provisions will be made to accommodate any local special events or festivals.	X	
Will the proposed MOT substantially change the environmental consequences of the action?		X
Is there substantial controversy associated with the proposed method for MOT?		X

Discuss closures and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Any local concerns about access and traffic flow should be detailed as well.

The MOT will be implemented in phases in order to maintain thru traffic along S. Main Street (US 421/SR 39) (Appendix B, B24 to B35). Phase 1 will involve the construction of the City Park Loop, Gordon Road, and S. Airport Road approaches. While those approaches are being constructed, two lanes (one in each direction) of S. Main Street will remain open. Access to S. Main Street from Gordon Road and S. Airport Road will be restricted. During this phase, a temporary single-lane signalized access road will be constructed in the southwest corner to maintain traffic between S. Airport Road and Gordon Road in both directions. A detour will be in place utilizing Gordon Road, 6th Street/S. 500 E, and US 24 to facilitate traffic from S. Airport Road and Gordon Road to S. Main Street. Additionally, the south entrance to the park, City Park Loop, will be closed during Phase 1. Access to the park will be maintained through the north entrance. During Phase 2, the remaining portions of the roundabout and the sidewalk along S. Main Street will be constructed. This phase will involve temporary lane closures for traffic along S. Main Street north and south of the new roundabout with temporary signals in place to facilitate one direction of traffic at a time. This phase will also involve the construction at Oak Drive where one lane will remain open at all times to maintain traffic. Phase 1 will likely be in place for 7 months beginning approximately April 2023 while Phase 2 will begin the following construction season and be in place for 6 months.

This MOT plan does require the acquisition of temporary ROW during construction, the clearing of 5 trees, and relocation of the sign on the Monticello Christian Church property.

ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 408,100 (2019-2020) Right-of-Way: \$ 40,000 (2020) Construction: \$ 2,777,520 (2023)

Anticipated Start Date of Construction: Spring 2023

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RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.49	0.00
Commercial	0.18	0.00
Agricultural	0.00	0.00
Forest	0.20	0.00
Wetlands	0.00	0.00
Other: Religious Facilities	0.16	0.46
Other: Recreational Facilities	0.58	0.00
TOTAL	1.61	0.46

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.

The existing ROW along Gordon Road extends approximately 44 feet north and 25 feet south of the roadway centerline. The existing ROW along S. Main Street (US 421/SR 39) extends from 30 to 40 feet east and from 40 to 68 feet west of the roadway centerline. The existing ROW along Airport Road extends 20 feet east and approximately 37 to 45 feet west of the roadway centerline. There is no existing ROW along City Park Loop. There is a 30-foot wide strip of existing ROW approximately 25 feet south of the existing alignment of City Park Loop that run approximately 750 feet east of the intersection to be improved. The existing ROW width along Oak Drive extends approximately 21 feet north and 21 feet south of the roadway centerline. The land use of the existing ROW consists of maintained roadside, some forested, and mainly existing roadway.

The project requires a total of approximately 1.61 acres of permanent ROW. The land use of the new permanent ROW consists of residential (0.49 acre), commercial (0.18 acre), forested (0.20 acre), religious facility (0.16 acre), and recreational (0.58 acre). The project also requires approximately 0.46 acre of temporary ROW from the Monticello Christian Church property for the temporary travel lane. The new ROW widths along Gordon Road will be from 54 to 74 feet north and 35 to 58 feet south of the roadway centerline. The new ROW widths along S. Main Street (US 421/SR 39) will be from 48 to 68 feet west and 39 to 80 feet east of the roadway centerline. The new ROW widths along Airport Road will be from 35 to 100 feet west of the roadway centerline and from 30 feet to 35 feet east of the roadway centerline. The new ROW widths along Oak Drive will extend 27 feet north and from 25 to 30 feet south of the roadway centerline.

In 2018, the City of Monticello purchased the property that appears on the aerial map at the corner of S. Main Street and Oak Drive from the White County Economic Development Commission. The purpose of this purchase was to allow for additional parking for Monticello City Park. The construction of the park fell through; however, the land had already been purchased by the City and the house demolished. The acquisition of ROW from this property occurred independent of this project. The land was purchased as part of a project that was not federally funded.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

Indiana Department of Transportation

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Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A - EARLY COORDINATION:

List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

Early coordination letters were sent on January 30, 2020 (Appendix C, C1 to C6).

Agency	Date Sent	Date Response Received	Appendix
US Army Corps of Engineers (USACE), Louisville District	January 30, 2020	N/A	N/A
US Department of Agriculture, Natural Resources Conservation Service (NRCS)	January 30, 2020	February 10, 2020	Appendix C, C17
National Park Service, Midwest Regional Office	January 30, 2020	N/A	N/A
U.S. Department of Housing and Urban Development	January 30, 2020	N/A	N/A
FHWA, Indiana Division	January 30, 2020	N/A	N/A
Indiana Department of Natural Resources (IDNR), Division of Fish and Wildlife (DFW)	January 30, 2020	February 28, 2020	Appendix C, C18 to C19
Indiana Geological Survey	January 30, 2020	January 30, 2020	Appendix C, C12 to C13
INDOT, Environmental Services Division (ESD)	January 30, 2020	February 6, 2020	Appendix C, C16
INDOT, Office of Aviation	January 30, 2020	N/A	N/A
INDOT, Laporte District Environmental Scoping Manager	January 30, 2020	N/A	N/A
INDOT, Laporte District Project Manager	January 30, 2020	N/A	N/A
IDEM (electronic submission)	January 30, 2020	January 30, 2020	Appendix C, C7 to C11
White County Board of Commissioners	January 30, 2020	N/A	N/A
White County Highway Department	January 30, 2020	N/A	N/A
White County Surveyor's Office	January 30, 2020	N/A	N/A
White County Council	January 30, 2020	N/A	N/A
White County Emergency Management Agency	January 30, 2020	N/A	N/A
White County Area Planning Commission (Floodplain Administrator)	January 30, 2020	February 4, 2020	Appendix C, C15
White County Drainage Board	January 30, 2020	N/A	N/A
White County Economic Development Organization	January 30, 2020	N/A	N/A
White County Sheriff's Department	January 30, 2020	N/A	N/A
Union Township Trustee	January 30, 2020	N/A	N/A
Monticello City Parks Board	January 30, 2020	N/A	N/A
Monticello Street Department	January 30, 2020	N/A	N/A
Monticello Mayor's Office	January 30, 2020	N/A	N/A
Monticello City Council	January 30, 2020	N/A	N/A
Monticello Fire Department	January 30, 2020	February 3, 2020	Appendix C, C14
Monticello Police Department	January 30, 2020	N/A	N/A
Monticello Christian Church	January 30, 2020	N/A	N/A
Twin Lakes School Corporation	January 30, 2020	N/A	N/A
IDEM, Poncie's Water Sports Project Manager	March 31, 2021	March 31, 2021	Appendix C, C39

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Indiana Department of Transportation

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SECTION B – ECOLOGICAL RESOURCES:

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, Watercourses & Other Jurisdictional Features			
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Total stream(s) in project area: N/A Linear feet Total impacted stream(s): N/A Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e. location, flow direction, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A
N/A	N/A	N/A	N/A	N/A

Describe all streams, rivers, watercourses and other jurisdictional features adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the Red Flag Investigation (RFI) report (Appendix E, E1 to E14) there are 11 streams, rivers, watercourse or other jurisdictional features within the 0.5 mile search radius. That number was confirmed by the site visit on September 30, 2019 by Lochmueller Group. There are no streams, rivers, watercourses, or other jurisdictional features present within or adjacent to the project area. Therefore, no impacts are expected.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on February 28, 2020 with recommendations to avoid or minimize impacts to waterways (Appendix C, C18 to C19). However, since no impacts to waterways will occur with this project, those recommendations are not applicable.

An automated letter was generated from the Indiana Department of Environmental Management (IDEM) website on January 30, 2020 (Appendix C, C7 to C11). Applicable recommendations from the Proposed Roadway Letter include coordination with appropriate permitting agencies. However, since no impacts to waterways will occur with this project, those recommendations are not applicable.

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Open Water Feature(s)			
Reservoirs			
Lakes			
Farm Ponds			
Retention/Detention Basin			
Storm Water Management Facilities			
Other: _____			

Describe all open water feature(s) identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

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Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E14) there is one open water feature within the 0.5 mile search radius. That number was confirmed by the site visit on September 30, 2019 by Lochmueller Group. There are no open water feature present within or adjacent to the project area.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on February 28, 2020 with no recommendations related to open water features (Appendix C, C18 to C19).

An automated letter was generated from the IDEM website on January 30, 2020 (Appendix C, C7 to C11). Applicable recommendations from the Proposed Roadway Letter include coordination with appropriate permitting agencies. However, since no impacts to open waters will occur with this project, those recommendations are not applicable.

	<u>Presence</u>	<u>Impacts</u>	<u>Yes</u>	<u>No</u>
Wetlands	<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>
Total wetland area: <u>N/A</u> Acre(s)		Total wetland area impacted: <u>N/A</u> Acre(s)		

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e. location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

	<u>Documentation</u>	<u>ESD Approval Dates</u>
Wetlands (Mark all that apply)		
Wetland Determination	<input type="checkbox"/>	<input type="checkbox"/>
Wetland Delineation	<input type="checkbox"/>	<input type="checkbox"/>
USACE Isolated Waters Determination	<input type="checkbox"/>	<input type="checkbox"/>

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

Describe all wetlands identified adjacent or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.

Based on the desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E14) there are five wetlands within the 0.5 mile search radius. No wetland areas were observed during the site visit on September 30, 2019 by Lochmueller Group. There are no wetland features present within or adjacent to the project area.

The USACE did not respond to the early coordination letter. The IDNR DFW responded on February 28, 2020 with recommendations to avoid or minimize impacts to wetlands (Appendix C, C18 to C19). However, since no impacts to wetlands will occur with this project, those recommendations are not applicable.

An automated letter was generated from the IDEM website on January 30, 2020 (Appendix C, C7 to C11). Applicable recommendations from the Proposed Roadway Letter include coordination with appropriate permitting agencies. However, since no impacts to wetlands will occur with this project, those recommendations are not applicable.

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	Presence	Impacts	Yes	No
Terrestrial Habitat	<input checked="" type="checkbox"/>		<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total terrestrial habitat in project area: 1.32 Acre(s) Total tree clearing: 0.29 Acre(s)

Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc.) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measure to avoid, minimize, and mitigate if impacts will occur.

Based on a desktop review, a site visit on September 30, 2019 by Lochmueller Group, and the aerial map of the project area (Appendix B, B3), there are maintained lawns, forested areas, and maintained vegetated roadside. The dominant vegetation in the area consists of red maple (*Acer rubra*), black locust (*Robinia pseudoacacia*), Kentucky bluegrass (*Poa pratensis*), red fescue (*Festuca rubra*), tall fescue (*Schedonorus arundinaceus*), and perennial ryegrass (*Lolium perenne*). The project will require the removal of approximately 0.29 acre of forested habitat. Approximately 0.09 acre of the forested habitat exists within existing ROW. Overall, the project will disturb approximately 1.32 acre of terrestrial habitat.

Due to ground disturbance exceeding 1.0 acre, an IDEM Rule 5 Notice of Intent (NOI) will likely be required.

Avoidance of these terrestrial habitat impacts is not practicable because doing so would not allow for the purpose and need of the project to be met. All attempts to minimize impacts to terrestrial habitats will be made.

The IDNR DFW responded on February 28, 2020 with recommendations to minimize impacts to terrestrial habitats (Appendix C, C18 to C19). These recommendations include revegetating all bare and disturbed land, time of year restrictions on tree clearing, and tree mitigation guidelines. All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

Protected Species Federally Listed Bats

Information for Planning and Consultation (IPaC) determination key completed
Section 7 informal consultation completed (IPaC cannot be completed)
Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS: NE ☐ NLAA ☒ LAA ☐

Other Species not included in IPaC

Additional federal species found in project area (based on IPaC species list)
State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Migratory Birds

Known usage or presence of birds (i.e. nests)
State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.

Based on a desktop review and the RFI report (Appendix E, E1 to E15), completed by Lochmueller Group on September 11, 2019, the IDNR White County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR DFW early coordination response letter dated February 28, 2020 (Appendix C, C18 to C19), the Natural Heritage Program's Database has been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, C20 to C25). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (NLEB) (*Myotis septentrionalis*). No additional species were generated

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in the IPaC species list other than the Indiana bat and northern long-eared bat.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and NLEB*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on March 10, 2021, and based on the responses provided, the project was found "Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, C26 to C38). INDOT reviewed and verified the effect finding on March 17, 2021 and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. Completion of IPaC resulted in the generation of several Avoidance and Minimization Measures (AMMs). These AMMs include ensuring everyone working on the project are aware all AMMs and environmental commitments, modifying the project to avoid tree removal when possible, applying time of year restrictions for tree removal to be outside of the bat active season, only allowing tree removal to that which is specified in the project plans, not removing documented Indiana bat or NLEB habitat during any time of the year, directing temporary lighting away from suitable habitat during the active season, and using downward facing, full cut-off lenses on any new or replacement permanent lighting. AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available, or if project plans are changed, USFWS will be contacted for consultation.

Geological and Mineral Resources

Project located within the Potential Karst Features Area of Indiana
Karst features identified within or adjacent to the project area
Oil/gas or exploration/abandoned wells identified in the project area

Yes

No

X
X
X

Date Karst Study/Report reviewed by INDOT EWPO (if applicable): N/A

Discuss if project is located in Potential Karst Features Area of Indiana and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Describe if any impacts will occur to any karst features. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Karst MOU and coordinated and reviewed by INDOT EWPO)

Based on a desktop review, the project is located outside the designated karst region of Indiana as outlined in the October 13, 1993 Karst Memorandum of Understanding (MOU). According to the topo map of the project area (Appendix B, B2) and the RFI report (Appendix E, E1 to E14) there are no karst features identified within or adjacent to the project area. In the early coordination response dated January 30, 2020, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, C12 to C13). They indicated high liquefaction potential, high potential for encountering bedrock resources, low potential to encounter sand and gravel resources, the presence of a floodway, and the presence of petroleum exploration wells within 0.5-mile of the project. No petroleum exploration wells are present within or near the project area according to the September 30, 2019 site visit and the IDNR Oil and Gas Well Records website (<https://www.in.gov/dnr/dnroil/>) accessed by Lochmueller Group on March 26, 2021. Response from IGWS has been communicated with the designer on March 9, 2021. No impacts are expected.

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SECTION C – OTHER RESOURCES

Drinking Water Resources

Wellhead Protection Area(s)
Source Water Protection Area(s)
Water Well(s)
Urbanized Area Boundary
Public Water System(s)

Presence

X
X

Impacts

Yes	No
	X
	X

Is the project located in the St. Joseph Sole Source Aquifer (SSA):
If Yes, is the FHWA/EPA SSA MOU Applicable?
If Yes, is a Groundwater Assessment Required?

Yes	No
	X

Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.

Sole Source Aquifer

The project is located in White County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

Wellhead Protection Area and Source Water

The IDEM Wellhead Proximity Determinator website (<http://www.in.gov/idem/cleanwater/pages/wellhead/>) was accessed on March 9, 2021 by Lochmueller Group. This project is not located within a Wellhead Protection Area or Source Water Area. No impacts are expected.

Water Wells

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on March 9, 2021 by Lochmueller Group. The nearest well is located in the northwest quadrant of the intersection. The feature will not be affected as it lies outside of the project area. Therefore, no impacts are expected. Should it be determined during the right-of-way phase that these wells will be affected, a cost to cure will likely be included in the appraisal to restore the wells.

Urban Area Boundary

Based on a desktop review of the INDOT map (<https://entapps.indot.in.gov/MS4/>) by Lochmueller Group on March 9, 2021, this project is located in an Urban Area Boundary (UAB); however, no coordination is needed because there is no current Rule 13 permit issued by the IDEM.

Public Water System

Based on a desktop review, a site visit on September 30, 2019 by Lochmueller Group, the aerial map of the project area (Appendix B, B3), and the preliminary design plans, there are public water systems within the project area. Utility coordination has begun for this project and will continue throughout project development to ensure that impacts are minimal.

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Floodplains

Project located within a regulated floodplain
Longitudinal encroachment
Transverse encroachment
Homes located in floodplain within 1000' up/downstream from project

Presence

Impacts

Yes	No

If applicable, indicate the Floodplain Level?

Level 1 ☐ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.

The IDNR Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on March 1, 2021 by Lochmueller Group. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, F2). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

Farmland

Agricultural Lands
Prime Farmland (per NRCS)

Presence

Impacts

Yes	No

Total Points (from Section VII of CPA-106/AD-1006*)

N/A

*If 160 or greater, see CE Manual for guidance.

Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.

Based on a desktop review, a site visit on September 30, 2020 by Lochmueller Group, and the aerial map of the project area (Appendix B, B3), there is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected. An early coordination letter was sent on January 30, 2020, to the NRCS. The NRCS responded on February 10, 2020 stating that the project will not cause a conversion of prime farmland (Appendix C, C17).

SECTION D – CULTURAL RESOURCES

Minor Projects PA Category(ies) and Type(s) INDOT Approval Date(s) N/A
X

Full 106 Effect Finding

No Historic Properties Affected ☐ No Adverse Effect ☒ Adverse Effect ☐

Eligible and/or Listed Resources Present

NRHP Building/Site/District(s) ☒ Archaeology ☐ NRHP Bridge(s) ☐

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Documentation Prepared (mark all that apply)

APE, Eligibility and Effect Determination
800.11 Documentation
Historic Properties Report or Short Report
Archaeological Records Check and Assessment
Archaeological Phase Ia Survey Report
Archaeological Phase Ic Survey Report
Other:

X
X
X
X
X

ESD Approval Date(s)

March 1, 2021
March 1, 2021
June 4, 2020
June 4, 2020
June 4, 2020

SHPO Approval Date(s)

March 31, 2021
March 31, 2021
July 9, 2020
June 29, 2020
June 29, 2020

Memorandum of Agreement (MOA)

☐

MOA Signature Dates (List all signatories)

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If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s) and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.

Area of Potential Effect (APE):

The aboveground APE for this project encompasses all resources immediately adjacent to the project area and those that may not be immediately adjacent but that have a proximate viewshed of the project area. The project area encompasses the area required to support the purpose and need of the project. On Airport Road the APE extends 550 feet south of the project area, including resources one property back from the road along both sides. On Gordon Road the APE extends approximately 600 feet west of the project area and encompasses resources one property back from the road along both sides. On S. Main Street (US 421/SR 39) the APE extends approximately 760 feet north of the project area and encompasses resources one property back from the road along both sides. On US 421/SR 39, the APE extends approximately 450-feet southeast of the project area, stopping just shy of the Tippecanoe River. The eastern side of the APE includes City Loop Drive and Oak Drive, extending between 200 feet and 260 feet to capture all resources within the viewshed. The APE includes all areas where ground disturbance may occur or where auditory or visual impacts might be expected.

Coordination with Consulting Parties:

Early coordination was initiated with potential consulting parties on September 12, 2019, as listed below, with a letter inviting organizations and individuals to be consulting parties (Appendix D, D71 to D75). A copy of the Historic Properties Report (HPR) (all parties) and the Archaeological Report (tribes only) were uploaded to INSCOPE (INDOT's publicly accessible website) for viewing by potential consulting parties on September 12, 2019. In response to the letter, the SHPO asked that property owners be invited to be Consulting Parties if right-of-way was to be taken from adjacent properties. Therefore, Monticello Christian Church and Monticello Parks Department were invited to be added as Consulting Parties. The following is a list of invited organizations and individuals and the date of their response. Those who indicated they wished to serve as consulting parties are in bold. Please note, INDOT acts on behalf of the FHWA, the lead federal agency, and the State Historic Preservation Officer (SHPO) is considered an automatic consulting party.

Section 106 Invited Consulting Parties	Date of Response
Kankakee-Iroquois Regional Planning Commission	No Response
White County Commissioners	No Response
White County Highway Department	No Response
Town of Monticello Mayor	No Response
Town of Monticello Street Department	No Response
White County Historian	No Response
White County Historical Society	No Response
Indiana Landmarks - Northern Regional Office	No Response
Absentee Shawnee Tribe of Oklahoma	No Response
Eastern Shawnee Tribe of Oklahoma	No Response
Miami Tribe of Oklahoma	June 4, 2020
Peoria Tribe of Indians of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	September 25, 2019
Monticello Christian Church	No Response
Monticello Parks Department	No Response

Indiana Department of Transportation

County	White	Route	Gordon Road, S. Main Street (US 421/SR 39), S. Airport Road, City Park Loop, Oak Drive	Des. No.	1702835
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Archaeology:

A Phase 1a archaeological reconnaissance survey was conducted by 106 Consulting LLC on February 17, 2020 (Appendix D, pages D104 to D106). No archaeological sites were encountered within the proposed project area. Therefore, it was recommended that the project be allowed to proceed as planned. The report of these findings was submitted to INDOT CRO on February 17, 2020 for review. After INDOT CRO concurrence on April 8, 2020, the report was sent to SHPO who also concurred with the findings of the report on June 29, 2020 (Appendix D, pages D89 to D90). The report was sent to the Absentee Shawnee Tribe of Oklahoma, Eastern Shawnee Tribe of Oklahoma, Miami Tribe of Oklahoma, Peoria Tribe of Indians of Oklahoma, and the Pokagon Band of Potawatomi Indians utilizing INSCOPE (INDOT's publicly accessible website). The Miami Tribe of Oklahoma responded on June 4, 2020 accepting consulting party status and offered "no objection" to the project. No comments regarding the report were received from the remaining tribes consulted.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (IC 14-21-1-27 and -29) requires that the discovery must be reported to the IDNR within two business days.

Historic Properties:

The National Register of Historic Places (NRHP), the Indiana Register of Historic Sites and Structures (State Register), the State Historic Architectural and Archaeological Research Database (SHAARD), the SHAARD GIS were consulted, and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM) were consulted. No resources within the APE were identified as being listed in the National Register. The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009) by Mead & Hunt was reviewed. No bridges eligible for listing in the NRHP are located within the project area. One cemetery, Rothrock II/Britton Cemetery, was identified within the APE. However, the cemetery boundary is not within 100 feet of the project area.

A qualified historian with Lochmueller Group performed a site inspection of the APE on September 30, 2019, in an effort to identify above-ground resources that, in the opinion of the qualified historian, were deemed eligible for listing in the National Register. As a result of the site inspection and background investigation efforts discussed in the HPR, two resources were identified that are recommended eligible for the National Register within the APE: Monticello City Park (IHSSI #181-426-29118) and Monticello Christian Church (Lochmueller #8). Other properties within the APE were documented during site inspection, such as the Oak Park Subdivision southeast of the project area; however, these properties were deemed ineligible for listing in the National Register.

Monticello City Park (IHSSI #181-426-29118) – Monticello City Park is located in southern Monticello and has multiple structures and features associated with Works Progress Administration (WPA) construction. Between 1935 and 1938 workers for the WPA built several structures in Monticello City Park in the WPA Rustic architecture style. The Monticello City Park is eligible for listing in the National Register under Criterion A as a local example of a WPA-associated property and Criterion C for its architectural significance. The markers to be removed have been attributed to the WPA-era of the park anecdotally, since the bulk of park's improvements occurred during that period. However, no documentation has been found to establish their construction by the WPA or when the markers were added to the park boundary. Furthermore, no information was obtained regarding whether or not the current location of the markers is where they were placed originally or has any historic importance. The markers have been replaced throughout the years by the parks department, when they have been damaged by vehicular traffic, using a mold designed to replicate the design. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello City Park in a manner that would diminish its historic integrity.

Monticello Christian Church (Lochmueller #8) - The Monticello Christian Church, located at 105 Gordon Road in Monticello, was constructed in 1968 in the Contemporary style. Sparsely ornamented, the structure is dominated by three steeply pitched gable roofs and freestanding tower. The exterior is clad in brick veneer and there are concrete sills on all the lower windows. The central gable features a large, multi-light, rectangular stained-glass window on the north elevation, rising from the lower hipped roof to the gable roof peak. On the south elevation, the central gable has a tall window in the shape of a thin Latin cross. The sanctuary seats face the cross window. The reinforced concrete tower, constructed in a narrow, cruciform plan, rises in front of the central gable and ends high above the roof peak, where it is crowned by three metal Latin crosses. The Monticello Christian Church is eligible for listing in the National Register under Criterion C for its architectural significance. The trees that are to be removed are recent additions to the property and are not considered contributing to the historic integrity of the property. Additionally, the sign to be removed and replaced is a modern addition to the church property and does not contribute to the architectural significance of the property. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello Christian Church in a manner that would diminish its historic integrity.

The HPR was completed by Lochmueller Group on May 29, 2020. (Appendix D, pages D102 to D103). The HPR was submitted to

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the INDOT CRO who concurred with the findings on June 4, 2020. The HPR was subsequently submitted to the SHPO and the other consulting parties on June 4, 2020. In a letter dated June 4, 2020, the Miami Tribe of Oklahoma responded to the HPR accepting consulting party status and offered "no objection" to the project. The SHPO staff responded to the HPR on June 29, 2020 and concurred with the recommendations of the report (Appendix D, D89 to D90). In a follow-up letter regarding the eligibility of the Oak Park Subdivision on July 9, 2020, the SHPO staff clarified the response in their June 29, 2020 letter, stating that they agreed that the subdivision was not eligible for listing in the National Register (Appendix D, D91 to D92).

Documentation Findings:

In response to SHPO comments received on July 9, 2020, a preliminary effects letter was prepared by Lochmueller Group staff. The letter detailed the specific work activity to be undertaken by the project in relationship to all identified historic properties. Finally, the Effects Letter explained the case for a finding of "No Adverse Effect." The reasoning is that the construction of the project will not alter these historic resources in a way that would diminish their historic integrity.

On December 17, 2020, a preliminary effects letter recommending a finding of "No Adverse Effect" was uploaded to INSCOPE and an email was sent to consulting parties notifying them of the letter. A hard copy of the letter was mailed to SHPO on that same day. The SHPO responded to the letter on January 19, 2021 concurring with the finding in the effects letter and stated that they do not believe the characteristics that qualify the identified historic properties for inclusion in the NRHP will be diminished as a result of the project.

On March 1, 2021, INDOT, acting on behalf of the FHWA, issued a finding of "No Adverse Effect" for the project (Appendix D, D2 to D3). The supporting 800.11(*e) document and finding were sent to consulting parties on March 1, 2021. The SHPO concurred with the "No Adverse Effect" finding on March 31, 2021 (Appendix D, D107 to D108). There were no additional comments regarding the finding from the consulting parties.

Public Involvement:

To meet the public involvement requirements of Section 106, a legal notice of FHWA's finding of "No Adverse Effect" was published in *Monticello Herald Journal* on March 11, 2021 offering the public the opportunity to submit comment pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on April 10, 2021. No comments were received within the public comment period. The text of the public notice and the affidavit of publication appear in Appendix D, pages D110 to D111.

The Section 106 process has been completed and the responsibilities of the FHWA under Section 106 have been fulfilled.

SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

	<u>Presence</u>	<u>Use</u>	
		Yes	No
Parks and Other Recreational Land			
Publicly owned park	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	
Publicly owned recreation area			
Other (school, state/national forest, bikeway, etc.)	<input checked="" type="checkbox"/>		<input checked="" type="checkbox"/>
Wildlife and Waterfowl Refuges			
National Wildlife Refuge			
National Natural Landmark			
State Wildlife Area			
State Nature Preserve			
Historic Properties			
Site eligible and/or listed on the NRHP	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	
<u>Evaluations</u>			
<u>Prepared</u>			
Programmatic Section 4(f)			
"De minimis" Impact	<input checked="" type="checkbox"/>		
Individual Section 4(f)			
Any exception included in 23 CFR 774.13	<input checked="" type="checkbox"/>		

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Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife / waterfowl refuges, and NRHP eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E14) there are 10 potential Section 4(f) resources within the 0.5 mile search radius. According to additional research and a site visit on September 30, 2019, there are three Section 4(f) resources within or adjacent to the project area: Monticello Christian Church, Monticello City Park, and Monticello/White County Trail. Monticello Christian Church and Monticello City Park are both considered Section 4(f) historic resources. Monticello City Park is considered a Section 4(f) recreational resource as well since it is a publicly owned park.

Monticello Christian Church

Constructed in 1968, Monticello Christian Church was built in the contemporary style and is located at the southwest corner of the intersection to be improved. Monticello Christian Church is eligible for listing in the NRHP under Criterion C for its architectural significance. The construction of the roundabout at this intersection as a part of this project will encroach on the recommended NRHP boundary of Monticello Christian Church. In total, 0.21 acre of permanent ROW and 0.46 acre of temporary ROW will be acquired from the church property. The temporary ROW is required as part of the MOT which will construct a temporary, signalized one-way access road that connects Gordon Road and S. Airport Road. This road will be removed and the disturbed area will be replanted following the completion of the project and the temporary ROW will revert back to the church's ownership. The project will also relocate the modern church sign located at the southwest corner of the intersection closer to the church building. Five trees on the property will be cleared as part of this project, as well. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello Christian Church in a manner that would diminish its historic integrity. The project resulted in a Section 106 finding of "No Adverse Effect" on the Monticello Christian Church. According to the June 2020 Memorandum of Understanding (MOU) between the FHWA, the Indiana SHPO, and the INDOT, a *de minimis* use of a property applies for all projects that the SHPO has concurred with a "No Adverse Effect" finding. INDOT, CRO, acting on FHWA's behalf has determined the appropriate Section 106 finding is "No Adverse Effect." As such, a *de minimis* finding was determined to be appropriate and it was determined that no further analysis was required according to form 800.11 (Appendix D, D2 to D3). It should be noted that FHWA's approval of this environmental document is also FHWA's approval of the Section 4(f) *de minimis* finding. It should also be noted that the SHPO did not mention that they concurred with the *de minimis* finding in their concurrence response, stating they concur with the "No Adverse Effect" finding (Appendix D, D107 to D108).

Monticello City Park

Monticello City Park is a city-owned park located in southern Monticello and has multiple structures and features associated with WPA construction. Between 1935 and 1938 workers for the WPA built several structures in Monticello City Park in the WPA Rustic architecture style. The Monticello City Park is eligible for listing in the NRHP under Criterion A as a local example of a WPA-associated property and Criterion C for its architectural significance. A portion of the new roundabout will be constructed in the southwest corner of the recommended NRHP boundary of the park. As a result of the project, the roadway within the park, City Park Loop, will be reconstructed and realigned within the boundary of the park. Additionally, 27 concrete markers along the perimeter of the park will be relocated to be within the new boundary of the park after construction (Appendix B, B38). Approximately 0.68 acre of permanent ROW will be acquired from the property of the park. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello City Park in a manner that would diminish its historic integrity. According to the June 2020 MOU between the FHWA, the Indiana SHPO, and the INDOT, a *de minimis* use of a property applies for all projects that the SHPO has concurred with a "No Adverse Effect" finding. INDOT, CRO, acting on FHWA's behalf has determined the appropriate Section 106 finding is "No Adverse Effect." As such, a *de minimis* finding was determined to be appropriate and it was determined that no further analysis was required according to form 800.11 (Appendix D, D2 to D3). It should be noted that FHWA's approval of this environmental document is also FHWA's approval of the Section 4(f) *de minimis* finding. It should also be noted that the SHPO did not mention that they concurred with the *de minimis* finding in their concurrence response, stating they concur with the "No Adverse Effect" finding (Appendix D, D107 to D108).

Monticello City Park is also a recreational Section 4(f) resource because it is a park, publicly owned and managed by the City of Monticello Parks Department, with baseball fields, basketball court, a playground, picnic pavilions, and walking trails. The project will

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require the acquisition of approximately 0.68 acre of permanent ROW from the park property along the south and southwest parts of the property. The *de minimis* finding applies because the project will not adversely affect the activities, features, or attributes that qualify the resource for protection under Section 4(f), the public was afforded the opportunity to comment on the *de minimis* finding and impacts to the park, and the official with jurisdiction (OWJ) concurred that the project will not adversely affect the Section 4(f) status of the project. The OWJ that manages the park, the Monticello Parks Department, concurred with the intent to issue a *de minimis* finding in a letter received on October 6, 2020 (Appendix I, I2). A public notice notifying the public of the intended *de minimis* finding and requesting public comment was published in the *Monticello Herald Journal* on November 4, 2020 (Appendix I, I3 to I4). No comments from the public were received within the 30-day comment period, which ended on December 4, 2020..

Monticello/White County Trail

The White County/Monticello Trail was constructed to improve access to Indiana Beach north of the city. The trail begins at Indiana Beach and follows along West Shafer Road/6th Street to Gordon Road where it continues east along Gordon Road. Currently, the trail ends at the intersection of Gordon Road, S. Main Street, Airport Road, and City Park Loop, where there is no crosswalk across S. Main Street at the intersection to connect to Monticello City Park. The White County/Monticello Trail is considered a Section 4(f) recreational resource because it is publicly owned and managed by the White County Board of Commissioners and is considered for recreational use. According to 23 CFR 774.13(g), transportation enhancement projects that meet the following stipulations are excepted from the requirement for Section 4(f) approval:

- The use of the Section 4(f) property is solely for the purpose of preserving or enhancing an activity, feature, or attribute that qualifies the property for Section 4(f) protection; and
- The official with jurisdiction (OWJ) over the Section 4(f) resource agrees in writing to paragraph (g)(1) of this section.

The OWJ, White County Board of Commissioners, concurred, in a letter dated May 27, 2021, that the project would be considered an enhancement to the trail once construction is complete (Appendix I, I24 to I25). Once construction is complete a crosswalk will be constructed that will connect the trail to Monticello City Park. Therefore, the use of the resource is solely for the purpose of enhancing the trail and none of the recreational land associated with the Monticello/White County Trail will be converted to a transportation use. The project would not adversely affect the recreational activities, features, and attributes that qualify White County/Monticello Trail for protection under Section 4(f) of the US Department of Transportation Act.

Section 6(f) Involvement

Presence

Use

Yes

No

Section 6(f) Property

☐
☐
☐

Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to a non-recreation use.

A review of 6(f) properties on the INDOT ESD website revealed a total of two properties in White County (Appendix I, I1). None of these properties are located within or adjacent to the project area. Therefore, there will be no impacts to 6(f) resources.

SECTION F – Air Quality

STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP?

Yes

No

☒
☐

Is the project located in an MPO Area?

☐
☒

Is the project in an air quality non-attainment or maintenance area?

☐
☒

If Yes, then:

Is the project in the most current MPO TIP?

☐
☐

Is the project exempt from conformity?

☐
☐

If No, then:

Is the project in the Transportation Plan (TP)?

☐
☐

Is a hot spot analysis required (CO/PM)?

☐
☐

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Location in STIP: Pages 614 and 615
Name of MPO (if applicable): N/A
Location in TIP (if applicable): N/A

Level of MSAT Analysis required?

Level 1a ☐ Level 1b ☒ Level 2 ☐ Level 3 ☐ Level 4 ☐ Level 5 ☐

Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.

STIP/TIP

This project is included in the Fiscal Year (FY) 2020-2024 Statewide Transportation Improvement Program (STIP) (Appendix H, H1 to H2).

Attainment Status

This project is located in White County, which is currently in attainment for all criteria pollutants according to the IDEM Office of Air Quality website (<https://www.in.gov/idem/airquality/2339.htm>). Therefore, the conformity procedures of 40 CFR Part 93 do not apply.

MSAT

The purpose of this project is to improve the LOS of this intersection and increase pedestrian connectivity by constructing a roundabout and a multi-use path at this intersection. This project has been determined to generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special mobile source air toxic (MSAT) concerns. As such, this project will not result in changes in traffic volumes, vehicle mix, basic project location, or any other factor that would cause a meaningful increase in MSAT impacts of the project from that of the no-build alternative.

Moreover, Environmental Protection Agency (EPA) regulations for vehicle engines and fuels will cause overall MSAT emissions to decline significantly over the next several decades. Based on regulations now in effect, an analysis of national trends with EPA's MOVES2014 model forecasts a combined reduction of over 90 percent in the total annual emissions rate for the priority MSAT from 2010 to 2050 while vehicle-miles of travel are projected to increase by over 45 percent. This will both reduce the background level of MSAT as well as the possibility of even minor MSAT emissions from this project.

SECTION G - NOISE

Noise

Yes

No

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy? ☐ ☒

Date Noise Analysis was approved/technically sufficient by INDOT ESD: N/A

Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.

This project is a Type III project. In accordance with 23 CFR 772 and the current Indiana Department of Transportation Traffic Noise Analysis Procedure, this action does not require a formal noise analysis.

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SECTION H – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

Will the proposed action comply with the local/regional development patterns for the area?
Will the proposed action result in substantial impacts to community cohesion?
Will the proposed action result in substantial impacts to local tax base or property values?
Will construction activities impact community events (festivals, fairs, etc.)?
Does the community have an approved transition plan?
If No, are steps being made to advance the community's transition plan?
Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.

The project will ultimately be beneficial to local business and properties due to improved traffic flow along in this area and the construction of the roundabout will not substantially change access to properties within the area. Overall, the negative impacts to property owners and local businesses within the project area will be minimal and will consist primarily of short-term construction impacts and the purchase of permanent and temporary strip ROW. No relocations are expected. Property owners will be provided access throughout the duration of the project to reduce impacts as much as possible. The project is not anticipated to result in substantial impacts to community cohesion. The proposed project is not expected to impact the surrounding community or cause economic impacts to the surrounding area. Therefore, this project will have minimal or no negative impacts to the community or local economy.

According to the Indiana Festivals website (<https://indianafestivals.org/>), accessed on March 18, 2021 by Lochmueller Group, there is one festival, the Spirit of Monticello Festival, scheduled within 10 miles of the project. The proposed project will use phased construction to facilitate traffic through the intersection during construction. Traffic to the festival may be slowed, but thru traffic along S. Main Street will be in place throughout construction (Appendix B, B20 to B31).

The ADA Transition Plan for the City of Monticello was approved and implemented on May 20, 2013 and updated on July 18, 2016. The project will comply with the published ADA Transition Plan and will not create any additional barriers to access. The project includes the extension of existing sidewalks and will increase pedestrian access in the area with ADA compliant sidewalks, crosswalks, and curb ramps.

Public Facilities and Services

Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation or public pedestrian and bicycle facilities.

Based on a desktop review, the aerial map of the project area (Appendix B, B3), and the RFI report (Appendix E, E1 to E14), there are three religious facilities, one school, three recreational facilities, one pipeline, two railroads, six trails, and one managed land located within the 0.5 mile of the project. Although not within 0.5 mile of the project area, the White County Airport is located 0.98 mile south of the project area. That number of public facilities was confirmed by the site visit on September 30, 2019 by Lochmueller Group. The Monticello Christian Church, Monticello City Park, and the Monticello multi-use trail are within or adjacent to the project area. Impacts to Monticello Christian Church and Monticello City Park include the acquisition of permanent and temporary ROW. Additionally, trees will be cleared at both properties. Access to all properties will be maintained during construction. Both of these resources are considered Section 4(f) resources. The Monticello/White County Trail will be reconstructed to follow the slightly shifted alignment of Gordon Road. This trail is considered to be both a transportation resource and a recreational resource. Therefore, it would be considered a Section 4(f) resource. Details about impacts and full coordination efforts can be found in *Section D: Cultural Resources* and *Section E – Section 4(f) Resources/Section 6(f) Resources* sections of this document.

Neither Monticello Christian Church nor Monticello Parks Department responded to early coordination. The Monticello Fire Department responded on February 3, 2020 detailing the minimum turning radius for their largest fire truck (Appendix C, C15). This information was forwarded on to the designer on February 4, 2020 and they confirmed that the design of the roundabout will accommodate their truck. The INDOT Office of Aviation responded to early coordination on January 30, 2020, stating that further coordination will be required if any object, obstruction, or equipment will exceed 70 feet in height (Appendix C, C40). No further

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coordination with INDOT Office of Aviation will occur since no objects, obstructions or equipment is expected to exceed 70 feet in height.

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high and disproportionate impacts to EJ populations?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high and adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize and mitigate these effects.

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. Per the current INDOT Categorical Exclusion Manual, an Environmental Justice (EJ) Analysis is required for any project that has two or more relocations or 0.5 acre of additional permanent right-of-way. The project will require 1.61 acres of permanent right-of-way. Therefore, an EJ Analysis is required.

Potential EJ impacts are detected by locating minority and low-income populations relative to a reference population to determine if populations of EJ concern exists and whether there could be disproportionately high and adverse impacts to them. The reference population may be a county, city or town and is called the community of comparison (COC). In this project, the COC is White County, Indiana. The community that overlaps the project area is called the affected community (AC). In this project, AC 1 is Census Tract 9585 and AC 2 is Census Tract 9586. An AC has a population of concern for EJ if the population is more than 50% minority or low-income or if the low-income or minority population is 125% of the COC. Data from the 2015-2019 American Community Survey was obtained from the US Census Bureau website on February 18, 2021 by Lochmueller Group. The data collected for minority and low-income populations within the AC are summarized in the below table.

	COC White County, Indiana	AC 1 Census Tract 9585	AC 2 Census Tract 9586
LOW-INCOME POPULATION			
Total Population for Whom Poverty Status is Determined	23,801	3,588	4,805
Total Population Below Poverty Level	2,198	440	546
Percent Low-Income	9.2%	12.3%	11.4%
125 Percent of COC	11.5%		
AC Percent Low-Income Greater Than 125 Percent of COC?		Yes	No
AC Percent Low-Income Greater Than 50 Percent?		No	No
Population of EJ Concern?		Yes	No
MINORITY POPULATION			
Total Population	24,149	3,691	4,976
Minority Population	2,577	250	754
Percent Minority	10.7%	6.8%	15.2%
125 Percent of COC	13.3%		

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AC Percent Minority Greater Than 125 Percent of COC?		No	Yes
AC Percent Minority Greater Than 50 Percent?		No	No
Population of EJ Concern?		No	Yes

AC 1, Census Tract 9585, has a percent low-income of 12.3% which is below 50% but is above the 125% COC threshold. AC 2, Census Tract 9586, has a percent low-income of 11.4% which is below 50% and is below the 125% COC. Therefore, AC 1 is a low-income population of EJ concern.

AC 1, Census Tract 9585, has a percent minority of 6.8% which is below 50% and is below the 125% COC threshold. AC 2, Census Tract 9586 has a percent minority of 15.2% which is below 50% but is above the 125% COC. Therefore, AC 2 is a minority population of EJ concern.

Overall, the negative impacts to property owners within the project area will be minimal and consist primarily of short-term construction impacts and the loss of a strip of ROW. No relocations are anticipated. The ROW to be acquired will not substantially diminish the existing use of the affected property owners. The maintenance of traffic (MOT) for this project will occur in phases to ensure motorists have access through the intersection throughout construction. Access will be maintained for property owners during construction of the project. Impacts from the project to any EJ community in this area will be beneficial due to the improved traffic flow at the intersection and the improved pedestrian connectivity in the City of Monticello. It is expected the project will not have a disproportionately high and adverse environmental or health impact to low-income or minority populations of EJ concern when compared to non EJ populations.

The EJ Analysis was sent to INDOT Environmental Services Division (ESD) on February 22, 2021. INDOT ESD concurred that the project will not have a disproportionately high and adverse environmental or health impact to populations of EJ concern when compared to non-EJ populations on April 5, 2021 (Appendix I, I5 to I13).

Relocation of People, Businesses or Farms

Will the proposed action result in the relocation of people, businesses or farms?
Is a BIS or CSRS required?

Yes No

	X
	X

Number of relocations: Residences: N/A Businesses: N/A Farms: N/A Other: N/A

Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.

No relocations of people, businesses, or farms will take place as a result of this project.

In 2018, the City of Monticello purchased the property that appears on the aerial map at the corner of S. Main Street and Oak Drive. The purpose of this purchase was to allow for additional parking for Monticello City Park. The construction of the park fell through; however, the land had already been purchased by the city and the house demolished. The acquisition of ROW from this property occurred independent of this project. The land was purchased as part of a project that was not federally funded.

SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation (RFI)
Phase I Environmental Site Assessment (Phase I ESA)
Phase II Environmental Site Assessment (Phase II ESA)
Design/Specifications for Remediation required?

Documentation

X

Date RFI concurrence by INDOT SAM (if applicable): September 12, 2019

Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.

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County White Route Gordon Road, S. Main Street (US 421/SR 39), S. Airport Road, City Park Loop, Oak Drive Des. No. 1702835

Based on a review of GIS and available public records, a RFI was concurred by INDOT SAM on September 12, 2019 by Lochmueller Group (Appendix E, E1 to E14). Three State Cleanup Sites, four underground storage tank (UST) sites, one leaking underground storage tank (LUST) site, six National Pollution Discharge Elimination System (NPDES) facilities, and six NPDES pipe locations are located within 0.5 mile of the project area. Unless discussed below, no impact will occur to this project.

One state cleanup site, Poncies Water Sports (AI ID #55012) is located 0.26 mile north of the project area. According to IDEM VFC, NFA Approval was received by IDEM in July 2019. However, a source of chlorinated solvent contamination was found south of this site and the plume extends south towards the project area. Coordination will be conducted with IDEM before further site activities occur. This has been included a firm commitment in this document.

Due to the amount of time that had passed since the approval of the RFI, Lochmueller underwent a subsequent review of the project area for additional resources that may impact the project area. One additional site, identified as a State Cleanup site and LUST site, was found 0.16 mile north of the project area. Through coordination with INDOT SAM on March 11, 2021, it was determined that no additional investigation for hazardous material concerns is required at this time.

Part IV – Permits and Commitments

PERMITS CHECKLIST

Permits (mark all that apply)

Likely Required

Army Corps of Engineers (404/Section10 Permit)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Other

IN Department of Environmental Management (401/Rule 5)

Nationwide Permit (NWP)
Regional General Permit (RGP)
Individual Permit (IP)
Isolated Wetlands
Rule 5
Other

X

IN Department of Natural Resources

Construction in a Floodway
Navigable Waterway Permit
Other

Mitigation Required

US Coast Guard Section 9 Bridge Permit

Others (Please discuss in the discussion below)

List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

Due to ground disturbance in excess of the 1.0 acre threshold (1.32 acres), a Rule 5 NOI will likely be required from the IDEM.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

Indiana Department of Transportation

County White Route Gordon Road, S. Main Street (US 421/SR 39), S. Airport Road, City Park Loop, Oak Drive Des. No. 1702835

ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

Firm:

1. If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT LaPorte District Environmental Section will be contacted immediately. (INDOT ESD and INDOT LaPorte District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
4. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
5. Tree Removal AMM 2: Apply time of year restrictions for tree removal when bats are not likely to be present (October 1 through March 31), or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. (USFWS, IDNR DFW)
6. Lighting AMM 2: When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable (USFWS)
7. Tree Removal AMM 3: Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
8. Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)
9. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
10. Poncies Water Sports (AI ID #55012) is located 0.26 mile north of the project area. According to IDEM VFC, NFA Approval was received by IDEM in July 2019. However, a source of chlorinated solvent contamination was found south of this site and the plume extends south towards the project area. Coordination will be conducted with IDEM before further site activities occur. (INDOT SAM)

Appendix A: INDOT Supporting Documentation

Threshold Chart.....	A1
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Appendix B: Graphics

General Location Map.....	B1
USGS Topographic Map (Monticello South Quadrangle)	B2
Aerial Map (2018).....	B3
Photo Location Map	B4
Site Photographs.....	B5-B14
Preliminary Plan Sheets.....	B15-B43

Appendix C: Early Coordination

Sample Early Coordination Letter (January 30, 2020).....	C1-C6
Indiana Department of Environmental Management Electronic Response (January 30, 2020).....	C7-C11
Indiana Geological Survey Electronic Response (January 30, 2020).....	C12-C13
Monticello Fire Department Response Email (February 3, 2020).....	C14
White County Area Plan Commission Response Letter (February 4, 2020).....	C15
Indiana Department of Transportation Environmental Service Division Response (February 6, 2020)	C16
Natural Resources Conservation Service Response Letter (February 10, 2020).....	C17
Indiana Department of Natural Resources (IDNR), Division of Fish and Wildlife Response Letter (February 28, 2020).....	C18-C19
United States Fish and Wildlife Service IPaC Official Species List (June 8, 2021).....	C20-C25
IPaC Concurrence Verification Letter (March 17, 2021).....	C26-C38
Indiana Department of Environmental Management Response Email (March 31, 2021).....	C39
Indiana Department of Transportation Office of Aviation response email (January 30, 2020).....	C40

Appendix D: Section 106 of the National Historic Preservation Act (NHPA)

Section 106 800.11(e) Documentation	D1-D106
SHPO Concurrence with Effects Finding (March 31, 2021).....	D107-D108
Public Notice published in the <i>Monticello Herald Journal</i>	D109
Proof of Publication from the <i>Monticello Herald Journal</i>	D110-D111

Appendix E: Red Flag Investigation

Red Flag Investigation.....	E1-E14
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Appendix F: Water Resources

NWI Wetlands Map	F1
FEMA Floodplain Map	F2

Appendix G: Public Involvement

Notice of Survey.....	G1
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Appendix H: Air Quality

Relevant pages from the INDOT FY 2020-2024 STIP	H1-H2
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Appendix I: Other Information

Land and Water Conservation Fund Grants: White County, Indiana	I1
Monticello City Parks Department Section 4(f) <i>de minimis</i> concurrence letter	I2
Public Notice published in the <i>Monticello Herald Journal</i>	I3
Proof of Publication from <i>Monticello Herald Journal</i>	I4
EJ Analysis Write-up	I5-I7
EJ Analysis Map.....	I8
Population Data	I9-I12
INDOT EJ Analysis concurrence (April 5, 2021)	I13
Traffic Study performed by Lochmueller Group.....	I14-I23
White County Board of Commissioners enhancement concurrence letter	I24-I25

Categorical Exclusion

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts³	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit ⁴
Wetland Impacts³	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
Right-of-way⁵	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)*	"No Effect", "Not likely to Adversely Affect" (With select AMMs ⁶)	"Not likely to Adversely Affect" (With any AMMs or commitments)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic ⁷
Threatened/Endangered Species (Any other species)*	Falls within guidelines of USFWS 2013 Interim Policy or "No Effect"	"Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁸
Sole Source Aquifer	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Section 4(f) Impacts	None	-	-	-	Any ⁹
Section 6(f) Impacts	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ¹⁰
Approval Level <ul style="list-style-type: none"> District Env. (DE) Env. Serv. Div. (ESD) FHWA 	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

¹ Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

² Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³ Total permanent impacts to streams (linear feet) and wetlands (acres).

⁴ US Army Corps of Engineers Individual 404 Permit

⁵ Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

⁶ Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

⁷ Projects that do not fall under a Species Specific Programmatic and results in a "Likely to Adversely Affect". Other findings can be processed as a lower level CE.

⁸ Potential for causing a disproportionately high and adverse impact.

⁹ Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

¹⁰ Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

* Includes the threatened/endangered species critical habitat

Note: Substantial public agency controversy may require a higher-level NEPA document.

Categorical Exclusion

Appendix B

Graphics



LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881

General Location Map

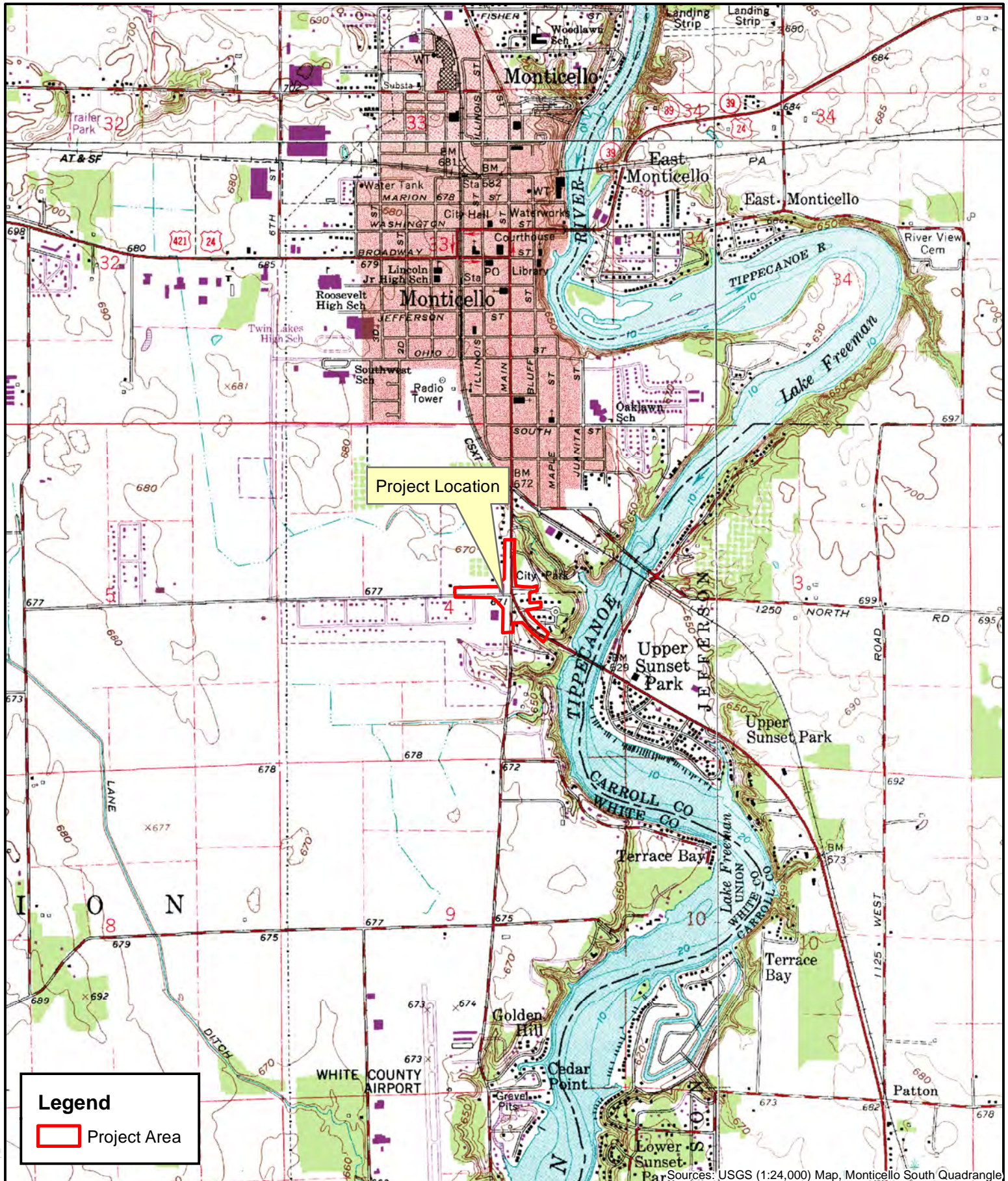
Des. No. 1702835

0 0.75 1.5 Miles

N

County: White
Township: Union
State: Indiana

Gordon Road Intersection Improvement
Gordon Road and US 421/SR 39
Created: 1/9/2020, C. Kunkel



Legend

Project Area

LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

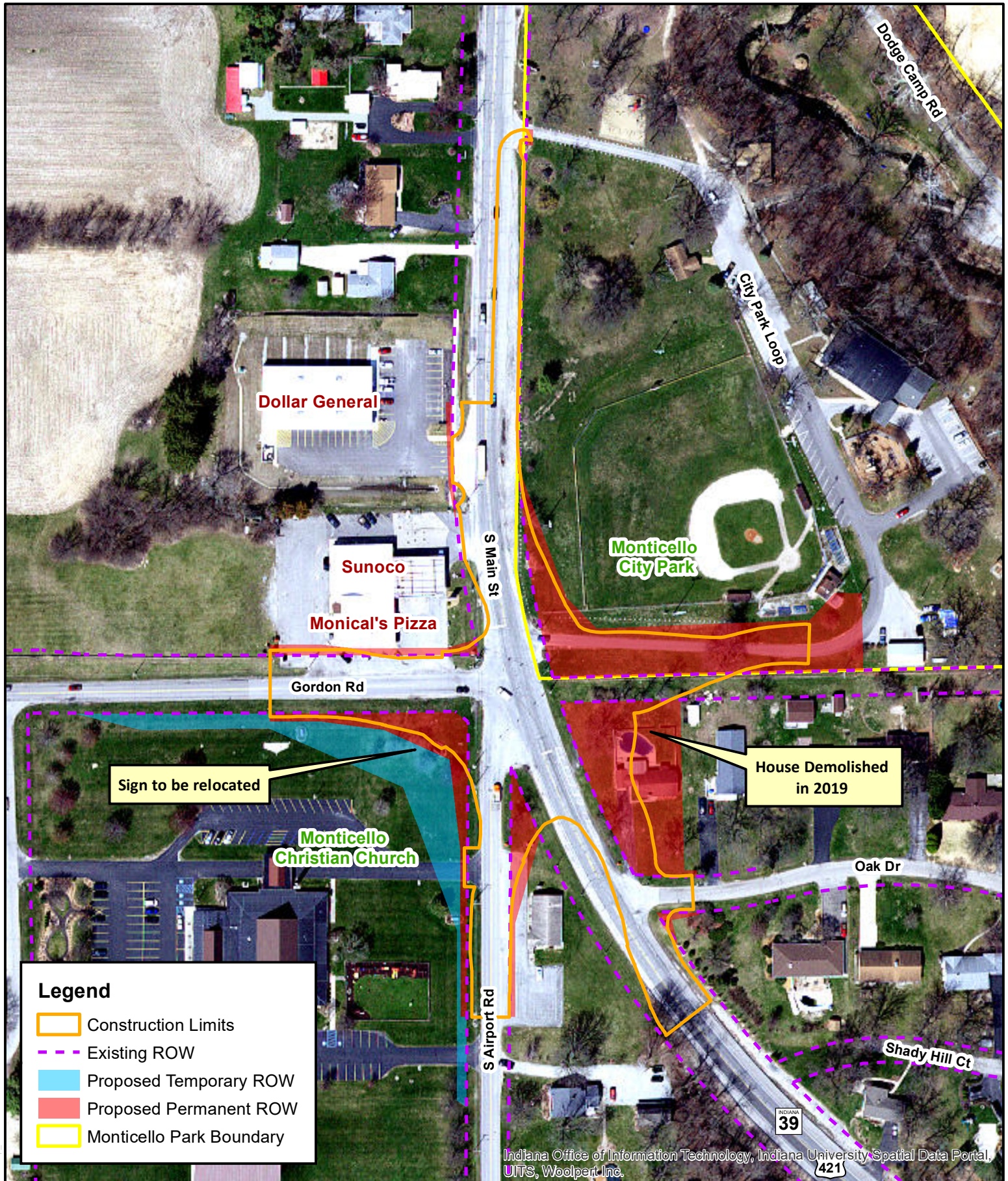
USGS Topographic Map
 Monticello South Quadrangle
 Des. No. 1702835

0 1,000 2,000
 Feet

County: White
 Township: Union
 State: Indiana

Gordon Road Intersection Improvement
 Gordon Road and US 421/SR 39
 Created: 1/3/2020, C. Kunkel

S:\2018\219-0028\Enviromaps\Topo Map_1702835.mxd



LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

Aerial Map (2018)

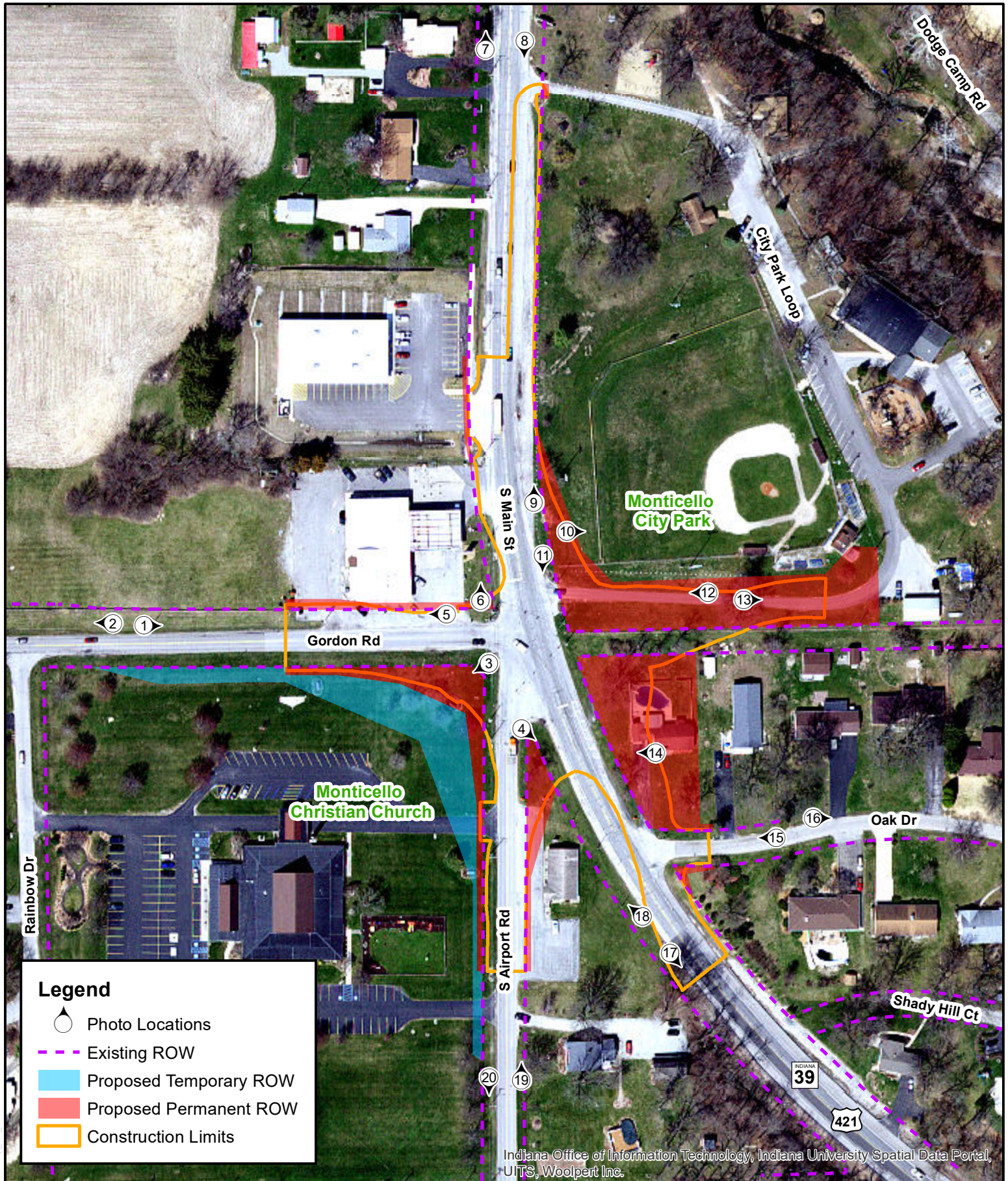
Des. No. 1702835

0 100 200 Feet

North Arrow

County: White
 Township: Union
 State: Indiana

Gordon Road Intersection Improvement
 Gordon Road and US 421/SR 39
 Created: 4/15/2021, C. Kunkel



Legend

- Photo Locations
- Existing ROW
- Proposed Temporary ROW
- Proposed Permanent ROW
- Construction Limits

Indiana Office of Information Technology, Indiana University Spatial Data Portal, UITS, Woolpert Inc.

LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

Photo Location Map

Des. No. 1702835

0 100 200 Feet

North Arrow

County: White
 Township: Union
 State: Indiana

Gordon Road Intersection Improvement
 Gordon Road and US 421/SR 39
 Created: 3/31/2021, C. Kunkel

S:\1702835\1702835-0028\Enviro\Map\Photo Map_C\1702835.mxd



1. Looking east along existing multi-use path along north side of Gordon Road



2. Looking west along existing multi-use path on the north side of Gordon Road



3. Looking southwest towards Monticello Christian Church



4. Looking southeast toward S. Main Street (US 421/SR 39)



5. Looking west along existing multi-use path along north side of Gordon Road



6. Looking north along S. Main Street (US 421/SR 39) from north side of Gordon Road



7. Looking north along the west side of S. Main Street (US 421/SR 39)



8. Looking south along the east side of S. Main Street (US 421/SR 39)



9. Looking north along east side of S. Main Street (US 421/SR 39)



10. Looking east toward Monticello City Park



11. Looking south along S. Main Street (US 421/SR 39) at intersection



12. Looking west along City Park Loop



13. Looking east along City Park Loop



14. Looking west across S. Main Street (US 421/SR 39)



15. Looking west along Oak Drive



16. Looking east along Oak Drive



17. Looking southeast along S. Main Street (US 421/SR 39)



18. Looking northwest along S. Main Street (US 421/SR 39)



19. Looking north along S. Airport Road



20. Looking south along S. Airport Road

PROJECT	DESIGNATION
1702835	1702835
CONTRACT	BRIDGE FILE
R-41180	N/A

STAGE 2 PLANS
9/14/2020

INDIANA DEPARTMENT OF TRANSPORTATION



INTERSECTION IMPROVEMENT STATE ROAD 39 AND GORDON ROAD PROJECT NO. 1702835

SEE SHEET 2 FOR
TRAFFIC AND
DESIGN DATA

WHITE COUNTY BOARD OF COMMISSIONERS

STEVE BURTON DATE

JOHN HEIMLICH DATE

DAVID DIENER DATE

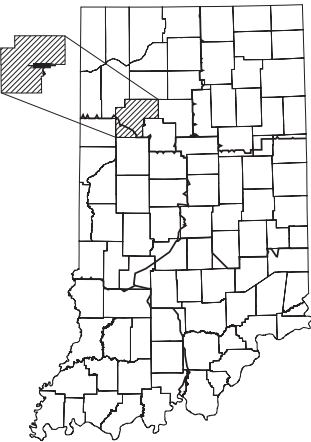
WHITE COUNTY AUDITOR

GAYLE ROGERS DATE

SUPERINTENDENT AND E.R.C.

MIKE KYBURZ DATE

Project Description:
Intersection Improvement at SR 39 and Gordon Road in the City of Monticello,
located in Section 4, T-26-N, R-3-W, Union Township, White County, Indiana



PROJECT LOCATION SHOWN BY
(White County)

BOP LATITUDE: 40°43'52" N BOP LONGITUDE: 86°45'43" W
EOP LATITUDE: 40°43'47" N EOP LONGITUDE: 86°45'42" W

BRIDGE LENGTH: N/A MI.
ROADWAY LENGTH: 1.2 MI.
TOTAL LENGTH: 1.4 MI.
MAX. GRADE: -2.23 %

HUC 14: # 05120106140020

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2020
TO BE USED WITH THESE PLANS



3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
PHONE: 317.222.3880
TOLL FREE: 800.423.7411

PLANS PREPARED BY:	Lochmueller Group, Inc.	(317) 222-3880 PHONE NUMBER
CERTIFIED BY:	DATE	
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION DATE	

BRIDGE FILE	
N/A	
DESIGNATION	
1702835	
SURVEY BOOK	SHEETS
ELECTRONIC	1 of 78
CONTRACT	PROJECT
R-41180	1702835

UTILITIES	
<div>GAS</div> <div>NIPSCO</div> <div>801 E. 86th Ave.</div> <div>Merrillville, IN 46410</div> <div>Dave Schaafsma</div> <div>dschaafsma@nisource.com</div>	<div>ELECTRIC</div> <div>NIPSCO</div> <div>801 E. 86th Ave.</div> <div>Merrillville, IN 46410</div> <div>Dave Schaafsma</div> <div>dschaafsma@nisource.com</div>
<div>COMMUNCIATIONS</div> <div>Comcast Cable</div> <div>720 Taylor St.</div> <div>Ft. Wayne, IN 46802</div> <div>John Gayday</div> <div>john_gayday@comcast.com</div> <div>(260) 458-5107</div>	<div>WATER</div> <div>City of Monticello</div> <div>227 N. Main St.</div> <div>Monticello, IN 47960</div> <div>Rod Pool</div> <div>water@monticelloin.gov</div>
<div>Centurylink</div> <div>213 W. Laporte St.</div> <div>Plymouth, IN 46563</div> <div>Bruce Emerick</div> <div>(574) 926-1247</div>	<div>WASTEWATER</div> <div>City of Monticello</div> <div>227 N. Main St.</div> <div>Monticello, IN 47960</div> <div>Adam Downey</div> <div>wastewater@monticelloin.gov</div>
<div>Lightstream</div> <div>PO Box 408</div> <div>306 S. St. Rd. 39</div> <div>Buffalo, IN 47925</div> <div>Brent Gillum</div> <div>bgillum@pwrtec.net</div> <div>(574) 278-7121</div>	

REVISIONS		
SHEET NO.	DATE	REVISED

GENERAL NOTES	
1.	All earth shoulders, median areas and cut and fill slopes shall be plain or mulch seeded except where sodding is specified.
2.	Pavement Design shall be finalized at a later date.

INDEX	
SHEET NO.	DRAWINGS INDEX
1	TITLE SHEET
2	INDEX AND GENERAL NOTES
3	GEOMETRIC TIE-INS
4 - 9	TYPICAL SECTIONS
10-21	MAINTENANCE OF TRAFFIC
22- 34	PLAN & PROFILE
35- 45	ROUNABOUT GEOMETRICS
46- 49	PAVEMENT MARKINGS AND SIGNS
50- 52	CURB RAMP DETAILS
53	LIGHTING DETAILS
54- 55	ROADWAY TABLES
56- 78	CROSS SECTIONS

TRAFFIC DATA	
	SR 39/US 421
A.A.D.T. (2022)	9,084 V.P.D.
A.A.D.T. (2042)	13,823 V.P.D.
D.H.V. (2042)	840 V.P.H.
DIRECTIONAL DISTRIBUTION	49% / 51%
TRUCKS	4.3% A.A.D.T.
	3.9% D.H.V.
DESIGN DATA	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	NONE

TRAFFIC DATA	
	GORDON RD
A.A.D.T. (2022)	5,228 V.P.D.
A.A.D.T. (2042)	7,956 V.P.D.
D.H.V. (2042)	718 V.P.H.
DIRECTIONAL DISTRIBUTION	47% / 53%
TRUCKS	6.8% A.A.D.T.
	5.9% D.H.V.
DESIGN DATA	
DESIGN SPEED	35 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	NONE

TRAFFIC DATA	
	S AIRPORT RD
A.A.D.T. (2022)	2,079 V.P.D.
A.A.D.T. (2042)	3,164 V.P.D.
D.H.V. (2042)	275 V.P.H.
DIRECTIONAL DISTRIBUTION	50% / 50%
TRUCKS	10.2% A.A.D.T.
	10.2% D.H.V.
DESIGN DATA	
DESIGN SPEED	25 M.P.H.
PROJECT DESIGN CRITERIA	RECONSTRUCTION (NON-FREEWAY)
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL
RURAL/URBAN	RURAL
TERRAIN	LEVEL
ACCESS CONTROL	NONE

Date: Sep 14, 2020, 9:02pm User Name: SFrish
File: S:_2018\218-0026\Road\CAO\Misc\DWG\1702835_Index.dwg

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: KMA	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE N/A	BRIDGE FILE N/A
INDEX AND GENERAL NOTES	VERTICAL SCALE N/A	DESIGNATION 1702835
	SURVEY BOOK	SHEET
	ELECTRONIC	2 of 78
	CONTRACT	PROJECT
	R-41180	1702835

SURVEY REFERENCE TIES

100

N: 1996087.841
E: 3042516.637

MAG NAIL W/TAG STAMPED
"BANNING ENG FIRM #0060"

1. NO PARKING SIGN 5.95' N33°19'51"E
2. SCHOOL BUS SIGN 46.8' N49°57'00"E
3. 45 MPH SIGN 46.9' N34°05'58"W
4. NO PARKING SIGN 145.2' N75°30'08"W

101

N: 1996187.847
E: 3042305.752

MAG NAIL W/TAG STAMPED
"BANNING ENG FIRM #0060"

1. GUARD RAIL 3.0'
2. 35 MPH SIGN 48.1' N49°01'49"E
3. NO PARKING SIGN 94.8' S47°30'24"E
4. IRON ROD IN MON. BOX 49.9' S53°14'34"E

102

N: 1996324.595
E: 3042149.332

MAG NAIL W/TAG STAMPED
"BANNING ENG FIRM #0060"

1. NO PARKING SIGN 6.5' S03°20'46"W
2. NO PARKING SIGN 46.8' N47°05'17"W
3. IRON ROD IN MON. BOX 29.2' N02°05'08"W
4. CENTERLINE ST RD. 39 21.0'

103

N: 1996693.299
E: 3041872.975

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. NW BUILDING CORNER 85.1' S02°50'08"W
2. NE BUILDING CORNER 89.1' S17°14'50"E
3. COR. ASPH. PARKING LOT 37.2' S17°41'46"W
4. REBAR W/CAP "RMA" 21.0' 66.2' N26°45'49"W

104

N: 1996324.068
E: 3041835.954

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. E. EDGE AIRPORT RD. 7.0' N00°00'00"E
2. N. UTILITY POLE 22.6' (AZ) 177°28'13"
3. NE UTILITY POLE 44.0' (AZ) 238°34'24"
4. SW CORNER HOUSE 65.0' N71°20'52"E

105

N: 1996648.271
E: 3042338.432

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. SE COR. 222 MAILBOX 34.0' N77°38'59"W
2. SW COR. HOUSE #202 56.1' N20°27'43"E
3. NW. FACE TRIPLE TREE 45.3' S31°57'03"E
4. E. EDGE DRIVE EXTENDED 7.2' N00°00'00"E

106

N: 1997167.075
E: 3041764.185

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. COR. PARKING LOT 18.0' (AZ)337°19'14"
2. EDGE CONC. WALK 6.4' (AZ)90°00'00"
3. EDGE CONC. WALK 6.6' (AZ)180°00'00"
4. DOLLAR GENERAL SIGN 10.2' (AZ)346°01'22"

107

N: 1996879.116
E: 3042177.255

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. CHAIN LINK FENCE 6.5' (AZ)180°00'00"
2. W. FACE UTILITY POLE 61.2' (AZ)92°12'19"
3. S. EDGE PAVEMENT 17.7' (AZ)0°00'00"
4. E. GATE POST 29.0' S08°25'56"W

108

N: 1997861.404
E: 3041860.641

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. NW COR. PARK BARN 65.2' (AZ)85°12'13"
2. SW COR. PARK BARN 67.6' (AZ)106°14'33"
3. STOP SIGN 50.8' (AZ)04°04'26"
4. E. EDGE OF MAIN ST. 7.8' (AZ)270°00'00"

109

N: 1996693.299
E: 3041872.975

5/8" REBAR W/CAP STAMPED
"BANNING ENG FIRM #0060"

1. COR. ASPH. PARKING LOT 76.0' S25°51'51"E
2. CENTER LIGHT POLE 101.0' S48°51'10"E
3. FLAG POLE 109.9' S80°32'10"E
4. SANITARY MANHOLE 84.2' N77°47'35"E

70244

N: 1996157.970
E: 3042345.752

IRON ROD IN MONUMENT BOX

1. MAG NAIL W/TAG "RMA" 11.86' N67°20'56"W
2. MAG NAIL W/TAG "RMA" 13.98' S37°32'48"E
3. MAG NAIL W/TAG "RMA" 41.82' N38°05'10"E

70393

N: 1996353.841
E: 3042148.267

IRON ROD IN MONUMENT BOX

1. MAG NAIL W/TAG "RMA" 32.67' N86°03'58"W
2. MAG NAIL W/TAG "RMA" 28.28' S06°44'38"W
3. MAG NAIL W/TAG "RMA" 24.26' N48°37'03"E

65044

N: 1998037.878
E: 3041835.257

IRON ROD IN MONUMENT BOX

1. MAG NAIL W/TAG 29.26' N52°24'15"W
2. MAG NAIL W/TAG 29.26' S43°24'15"W
3. MAG NAIL W/TAG 26.92' S55°08'12"E
4. MAG NAIL W/TAG 23.20' N80°54'07"E

60011

N: 1999586.374
E: 3043555.713

NE COR. SEC. 4, T26N, R3W

1. IRON PIPE W/MAG 43.40' (AZ) 273°
2. PK NAIL NW UTILITY POLE 48.07' (AZ) 183°
3. SW COR. BLOCK BUILDING 62.67' (AZ) 82°
4. NW COR. BLOCK BUILDING 49.44' (AZ) 44°

60010

N: 1999554.525
E: 3040823.534

POINT ON NORTH/SOUTH 1/4
SEC. 4, T26N, R3W

MAG NAIL W/WASHBOR "RMA FIRM #0074"

1. CENTERLINE SOUTH ST. 2' (AZ) 0°
2. TOP CENTER FIRE HYDRANT 94.82' (AZ) 77°
3. SW COR. CHAIN LINK FENCE 24.38' (AZ) 42°
4. NE COR. RAILROAD RAIL 29.81' (AZ) 0°

60012

N: 1996792.103
E: 3038111.312

NW COR. SW 1/4
SEC. 4, T26N, R3W

ALUMINUM WHITE COUNTY SURVEYOR MARKER

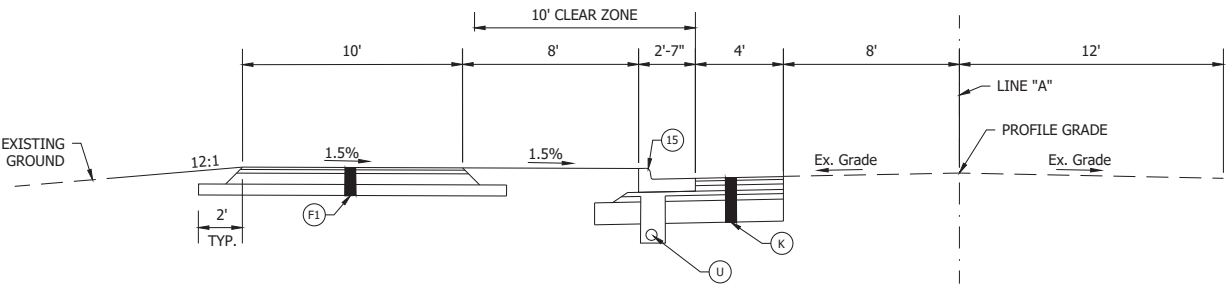
1. MAG NAIL IN CURB JOINT 21.55' (AZ) 261°
2. MAG NAIL E SIDE 45.61' (AZ) 185°
3. MAG NAIL E SIDE 49.50' (AZ) 351°
UTILITY POLE #770/083

Date: Sep 14, 2020, 9:03pm User Name: Sfrsch
File: S:_2018\218-0026\Road\CAI\Misc\DWG\1702835 Geometric Ties Exist.dwg

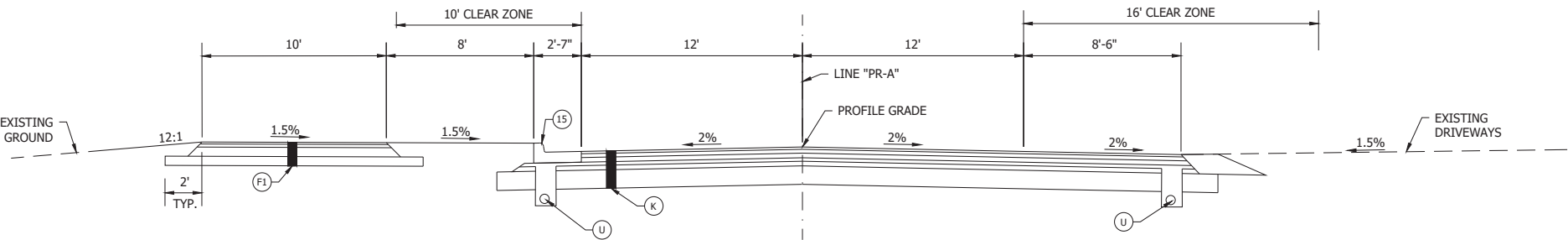
Des. No. 1702835

Appendix B: Graphics

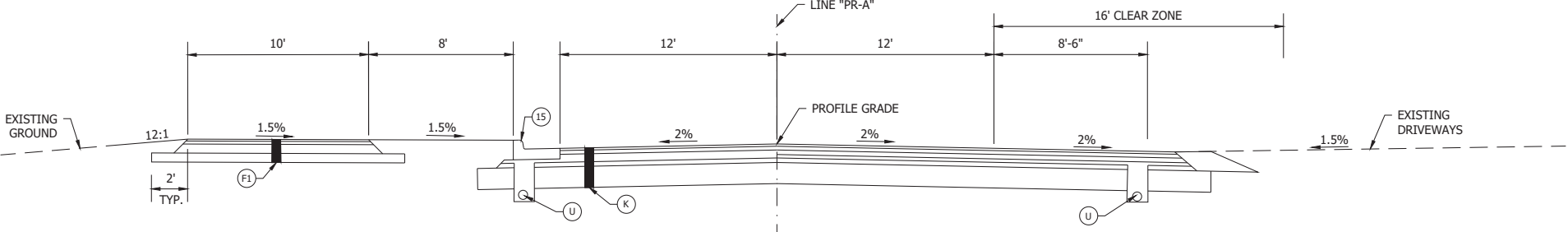
B17



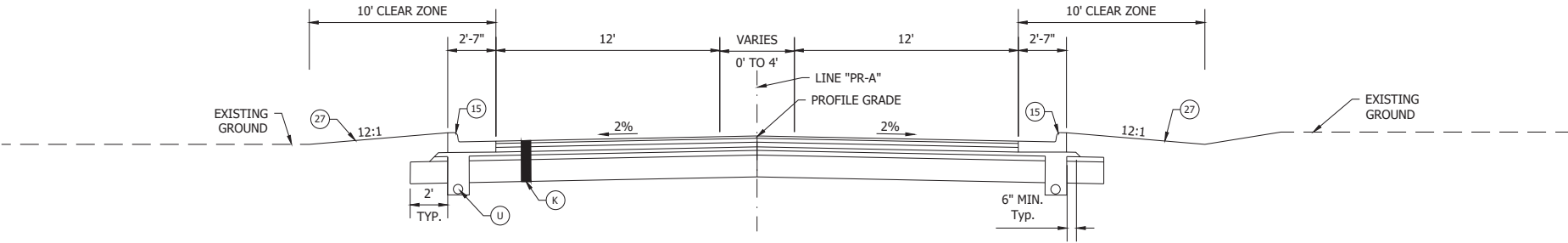
TYPICAL SECTION - SR 39/US 421
STA. 47+20.00 LINE "A" TO STA. 50+40.00 LINE "A"



TYPICAL SECTION - SR 39/US 421
STA. 50+40.00 LINE "PR-A" TO STA. 51+32.00 LINE "PR-A"



TYPICAL SECTION - SR 39/US 421
STA. 51+32.00 LINE "PR-A" TO STA. 51+72.00 LINE "PR-A"



TYPICAL SECTION - SR 39/US 421
STA. 51+72.00 LINE "PR-A" TO STA. 53+10.00 LINE "PR-A"

- (K) 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment, Type 1B
- (U) Pipe, Type 4, Circular, 6 IN.

- (F1) HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III
- (13) Curb, Concrete

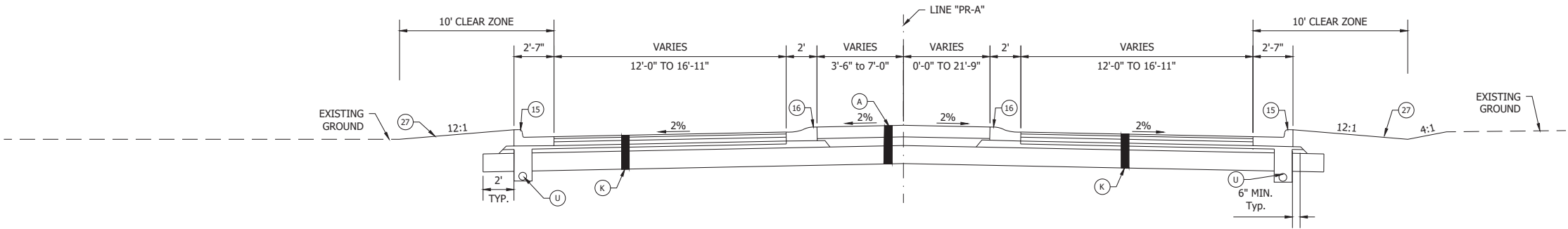
- (A) PCCP, 9 IN. on 9 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B
- (15) Curb and Gutter, Concrete
- (16) Curb and Gutter, B, Concrete
- (27) Mulched Seeding R

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: KMA	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

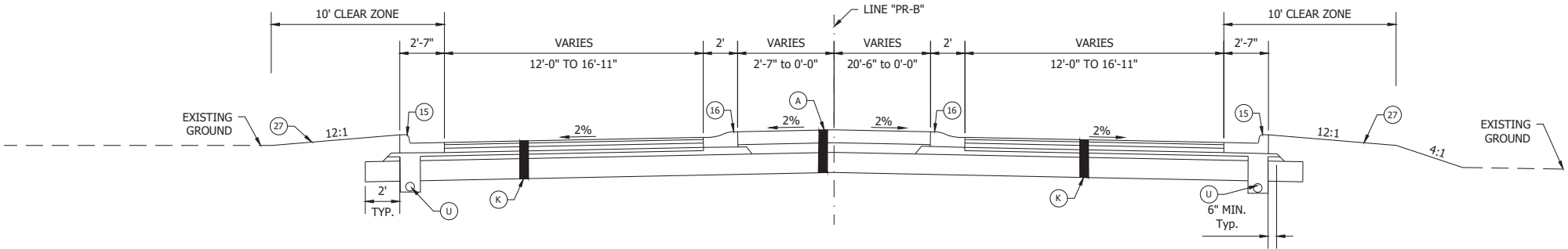
INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

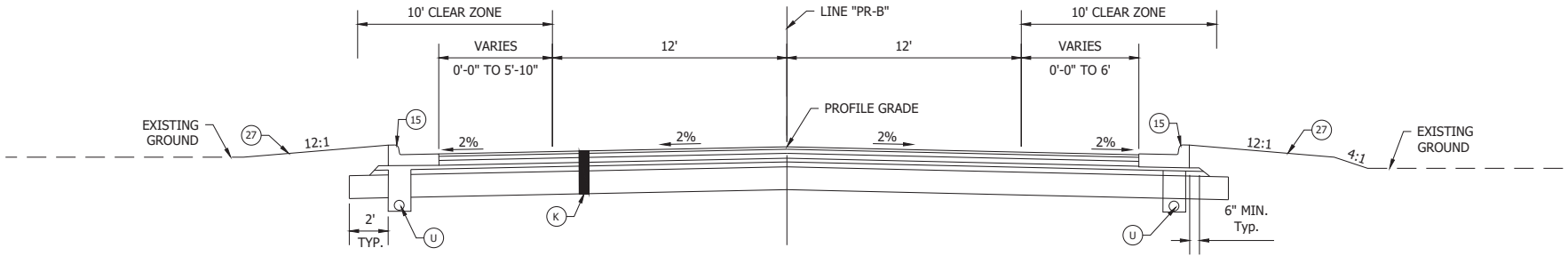
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1/4"=1'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1702835
SURVEY BOOK	SHEET
ELECTRONIC	4 of 78
CONTRACT	PROJECT
R-41180	1702835



TYPICAL SECTION - SR 39/US 421
STA. 53+10.00 LINE "PR-A" TO STA. 53+56.00 LINE "PR-A"



TYPICAL SECTION - SR 39/US 421
STA. 55+29.00 LINE "PR-B" TO STA. 56+21.00 LINE "PR-B"



TYPICAL SECTION - SR 39/US 421
STA. 56+21.00 LINE "PR-B" TO STA. 56+65.00 LINE "PR-B"

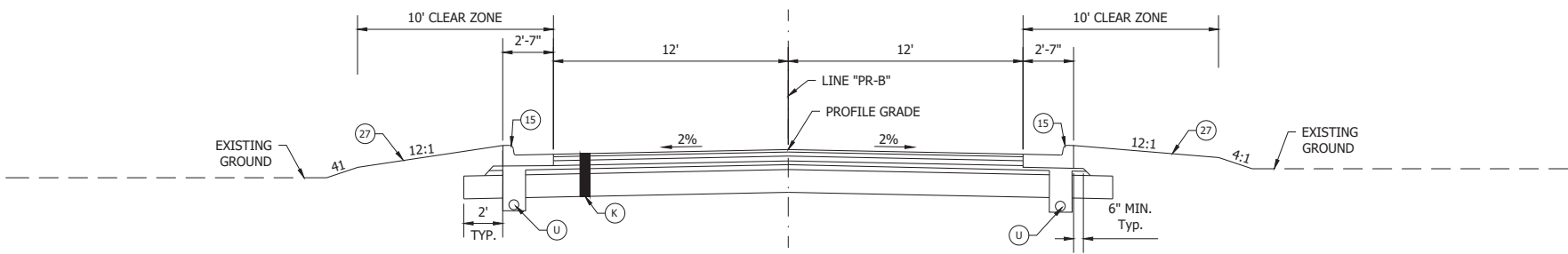
(K) 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B	(F1) HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III	(A) PCCP, 9 IN. on 9 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B	(27) Mulched Seeding R
(U) Pipe, Type 4, Circular, 6 IN.	(13) Curb, Concrete	(15) Curb and Gutter, Concrete	(16) Curb and Gutter, B, Concrete

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: KMA	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

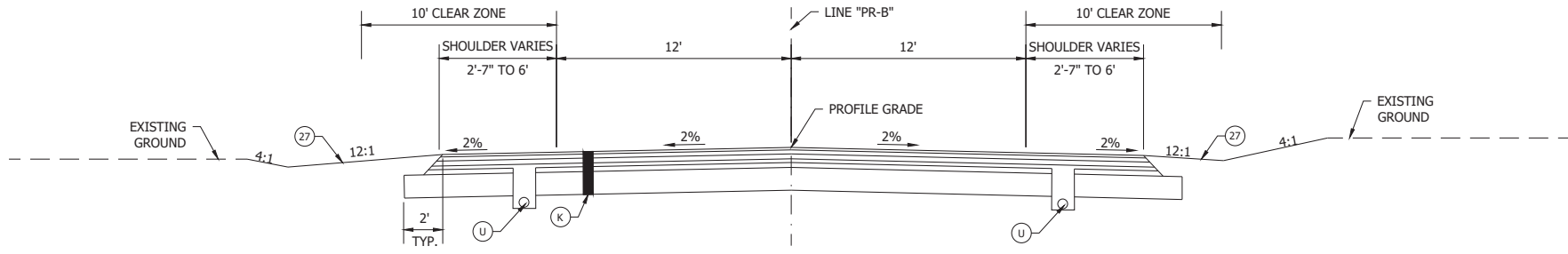
INDIANA DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS

HORIZONTAL SCALE 1/4"=1'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK ELECTRONIC	SHEET 5 of 78
CONTRACT R-41180	PROJECT 1702835

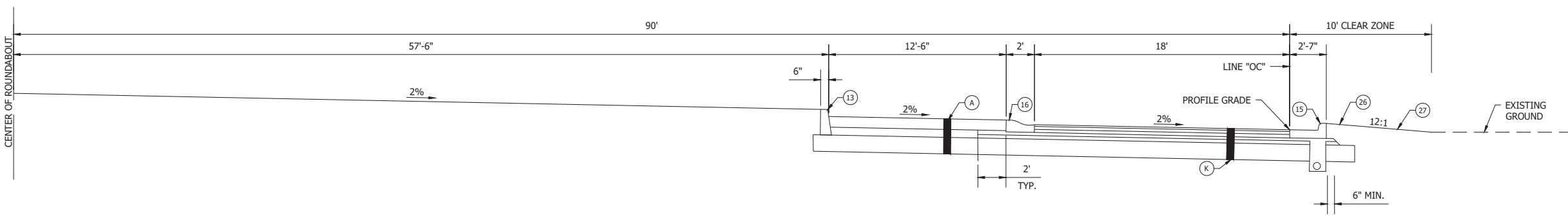
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TYPICAL SECTION - SR 39/US 421
STA. 56+65.00 LINE "PR-B" TO STA. 57+50.00 LINE "PR-B"



TYPICAL SECTION - SR 39/US 421
STA. 57+50.00 LINE "PR-B" TO STA. 58+30.00 LINE "PR-B"



TYPICAL SECTION - INSCRIBED CIRCLE
STA. 0+00.00 LINE "OC" TO STA. 5+65.24 LINE "OC"

- | | | | |
|--|---|--|-----------------------------------|
| (K) 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B | (F1) HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III | (A) PCCP, 9 IN. on 9 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B | (27) Mulched Seeding R |
| (U) Pipe, Type 4, Circular, 6 IN. | (13) Curb, Concrete | (15) Curb and Gutter, Concrete | (16) Curb and Gutter, B, Concrete |

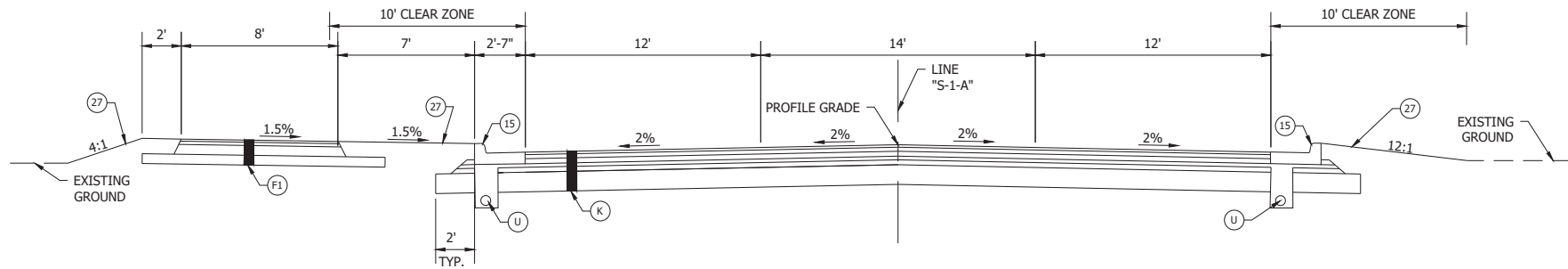
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: KMA	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

INDIANA
DEPARTMENT OF TRANSPORTATION

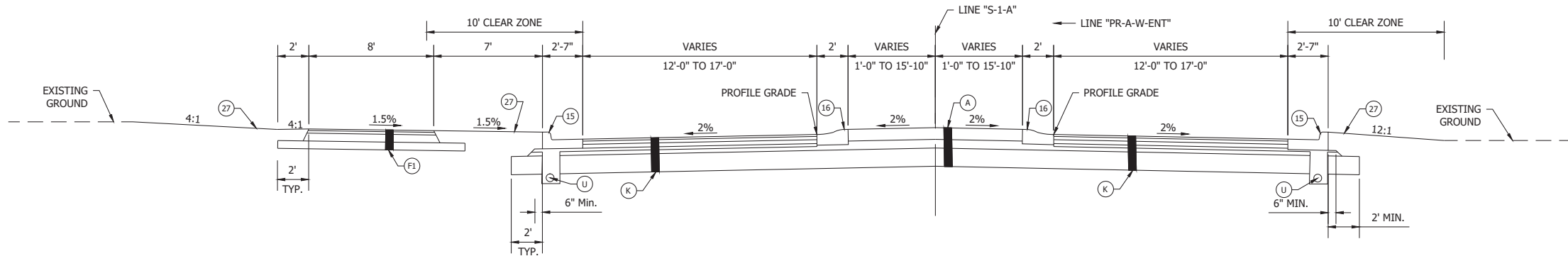
TYPICAL SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
1/4"=1'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1702835
SURVEY BOOK	SHEET
ELECTRONIC	6 of 78
CONTRACT	PROJECT
R-41180	1702835

Date: Sep 14, 2020, 9:03pm User Name: Sfrsch
File: S:_2018\1218-0026[Road]CAD\MiscDWG\1702835 Typical.dwg



TYPICAL SECTION - GORDON ROAD
STA. 0+00.00 LINE "PR-S-1-A" TO STA. 2+24.00 LINE "PR-S-1-A"



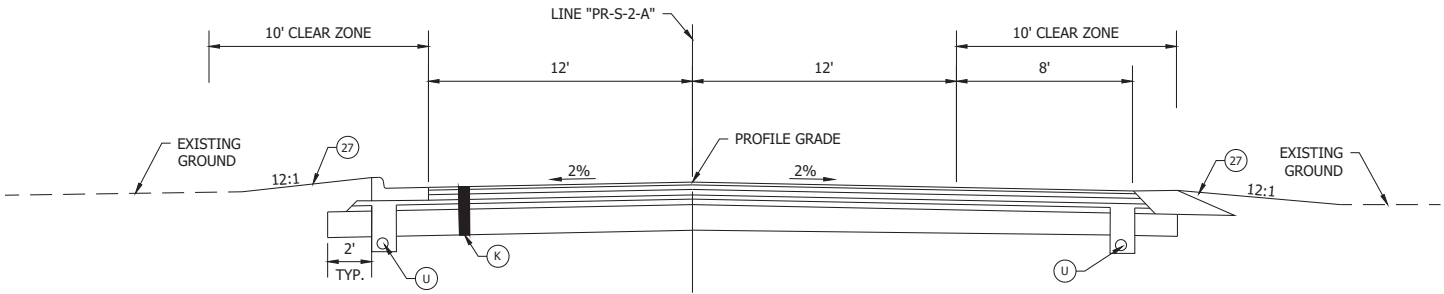
TYPICAL SECTION - GORDON ROAD
STA. 2+24.00 LINE "PR-S-1-A" TO STA. 3+18.00 LINE "PR-S-1-A"

Date: Sep 14, 2020, 9:03pm User Name: Sfrsch
File: S:_2018\1218-006\Road\CAI\Misc\DWG\1702835 Typical.dwg

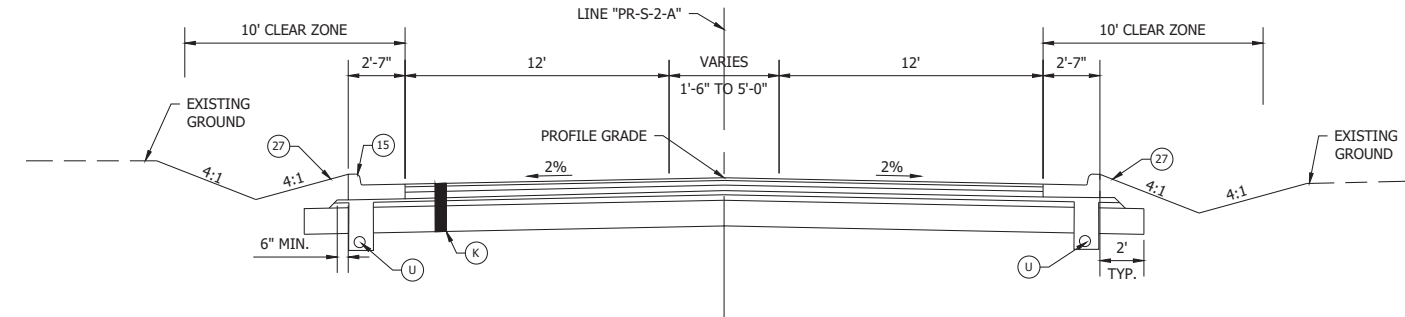
(K) 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B	(F1) HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III	(A) PCCP, 9 IN. on 9 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B	(27) Mulched Seeding R
(U) Pipe, Type 4, Circular, 6 IN.	(13) Curb, Concrete	(15) Curb and Gutter, Concrete	(16) Curb and Gutter, B, Concrete

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: KMA	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

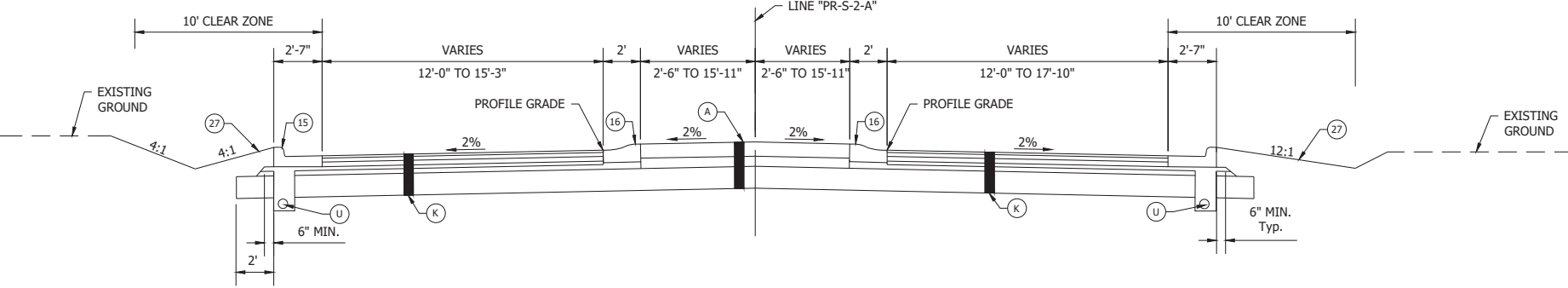
INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1/4"=1'	BRIDGE FILE N/A
		VERTICAL SCALE N/A	DESIGNATION 1702835
		SURVEY BOOK ELECTRONIC	SHEET 7 of 78
		CONTRACT R-41180	PROJECT 1702835



TYPICAL SECTION - S. AIRPORT ROAD
STA. 0+00.00 LINE "PR-S-2-A" TO STA. 2+18.00 LINE "PR-S-2-A"



TYPICAL SECTION - S. AIRPORT ROAD
STA. 2+18.00 LINE "PR-S-2-A" TO STA. 2+43.00 LINE "PR-S-2-A"



TYPICAL SECTION - S. AIRPORT ROAD
STA. 2+43.00 LINE "PR-S-2-A" TO STA. 2+84.00 LINE "PR-S-2-A"

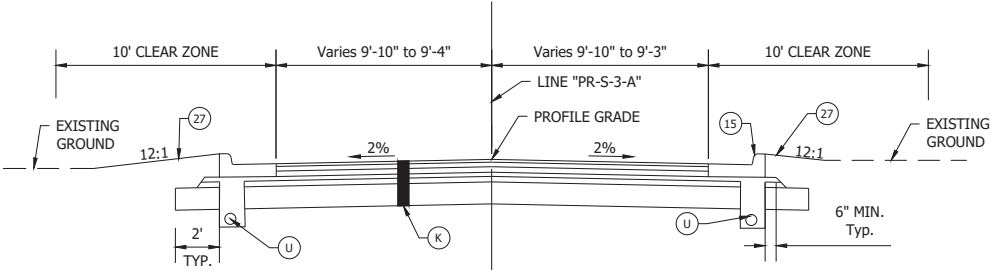
(K) 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B	(F1) HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III	(A) PCCP, 9 IN. on 9 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B	(27) Mulched Seeding R
(U) Pipe, Type 4, Circular, 6 IN.	(13) Curb, Concrete	(15) Curb and Gutter, Concrete	(16) Curb and Gutter, B, Concrete

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	---/---/----
DESIGNED: KMA	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

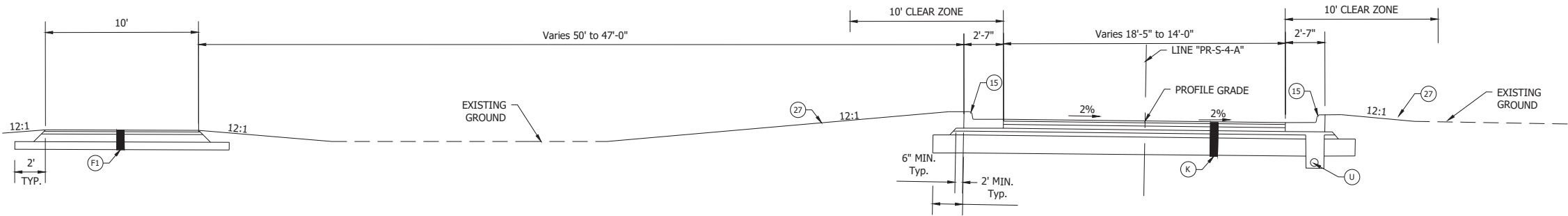
INDIANA DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS

HORIZONTAL SCALE 1/4"=1'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK ELECTRONIC	SHEET 8 of 78
CONTRACT R-41180	PROJECT 1702835

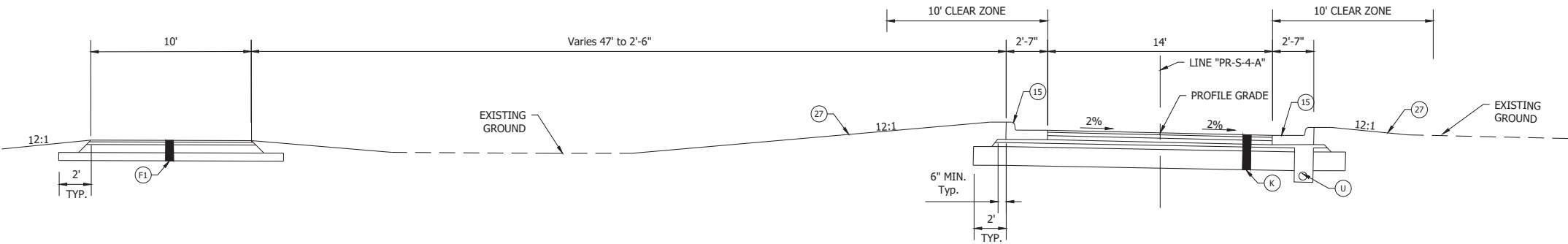
Date: Sep 14, 2020, 9:03pm User Name: SPSch
File: S:_2018\1702835\Road\CAI\Misc\DWG\1702835 Typical.dwg



TYPICAL SECTION - OAK DRIVE
STA. 0+41.00 LINE "PR-S-3-A" TO STA. 0+70.13 LINE "PRS--3-A"



TYPICAL SECTION - CITY PARK LOOP
STA. 0+94.00 LINE "PR-S-4-A" TO STA. 1+39.08 LINE "PR-S-4-A"



TYPICAL SECTION - CITY PARK LOOP
STA. 1+39.08 LINE "PR-S-4-A" TO STA. 3+30.00 LINE "PR-S-4-A"

(K) 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B	(F1) HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III	(A) PCCP, 9 IN. on 9 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B	(27) Mulched Seeding R
(U) Pipe, Type 4, Circular, 6 IN.	(13) Curb, Concrete	(15) Curb and Gutter, Concrete	(16) Curb and Gutter, B, Concrete

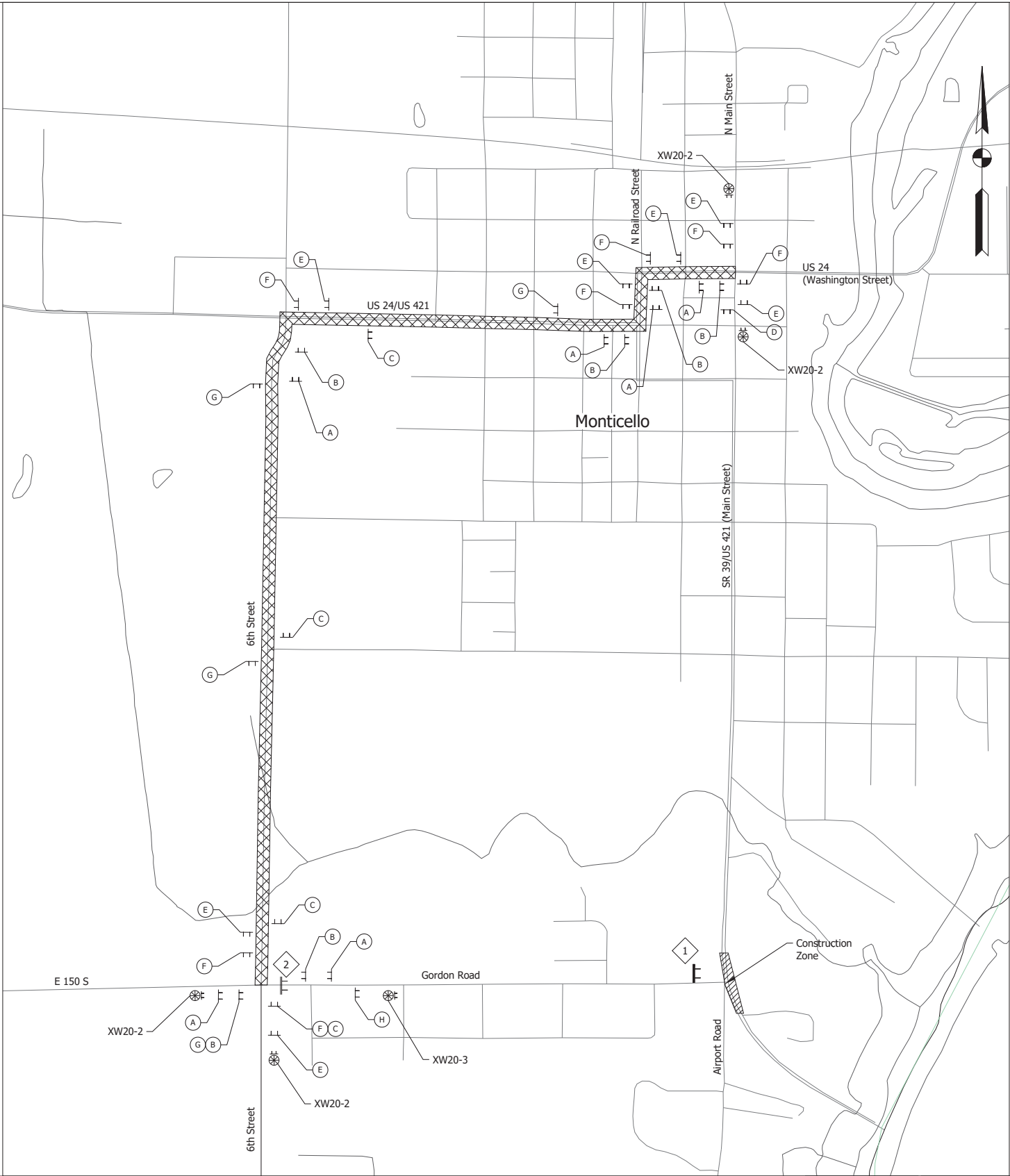
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER		DATE
DESIGNED: _____	KMA	DRAWN: _____	KMA
CHECKED: _____	JNH	CHECKED: _____	JNH

INDIANA DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS

HORIZONTAL SCALE 1/4"=1'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK ELECTRONIC	SHEET 9 of 78
CONTRACT R-41180	PROJECT 1702835

Date: Sep 14, 2020, 9:03pm User Name: SPSch
File: S:_2018\1702835\Road\CAI\Misc\DWG\1702835 Typical.dwg

Date: Sep 14, 2020, 9:03pm User Name: SFisch
File: S:\1_2018\18-0228\Road\CD\INDOT Detour Gordon & Airport.dwg



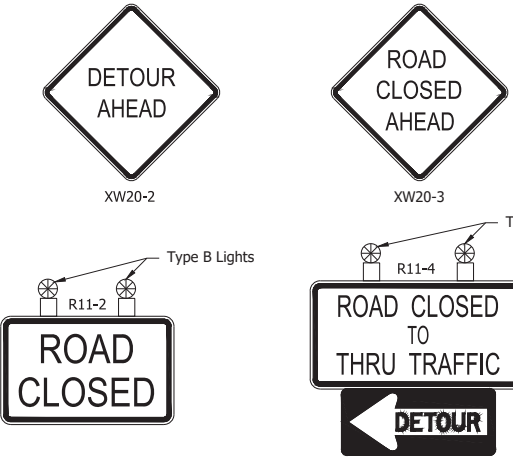
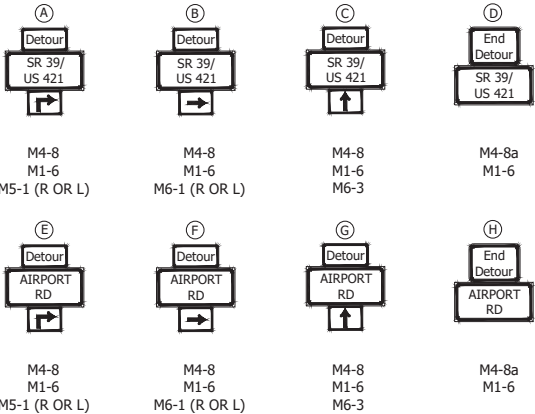
CONSTRUCTION SIGN SCHEDULE				
SIGN NO.	DESCRIPTION	SIZE (IN)	TYPE	EST. QTY.
XW20-2	"DETOUR AHEAD" SIGN	36 x 36	A	4
XW20-3	"ROAD CLOSED AHEAD" SIGN	36 x 36	A	1
			TOTAL TYPE "A" SIGNS	5

DETOUR ROUTE MARKER ASSEMBLIES: 36 EACH
ROAD CLOSURE SIGN ASSEMBLY: 2 EACH
TYPE III-A BARRICADES: 24 LFT.
TYPE III-B BARRICADES: 16 LFT.

- 1 Road Closure Sign Assembly with Type A Barricades (2 x 24') = 48 LFT., R11-2
- 2 Road Closure Sign Assembly with Type B Barricades (2 x 16') = 32 LFT., R11-4 and M4-10 (R) or (L)

LEGEND:

- ☼☼ TYPE B CONSTRUCTION WARNING LIGHTS
- ☼ CONSTRUCTION SIGN (WITH TYPE A CONSTRUCTION WARNING LIGHTS)
- TT CONSTRUCTION SIGN
- TT BARRICADE, TYPE III-A
- TT BARRICADE, TYPE III-B
- /// CONSTRUCTION ZONE
- XXX DETOUR ROUTE



ROAD CLOSURE SIGN ASSEMBLY 1

ROAD CLOSURE SIGN ASSEMBLY 2

NOTES

1. ALL MAINTENANCE OF TRAFFIC DEVICES, SIGNS AND PAVEMENT MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE INDIANA MUTCD.
2. SEE INDOT STD DWG 801-TCDT-01 FOR ADDITIONAL DETAILS.
3. SEE INDOT STD DWG 801-TCLG-01 FOR GENERAL NOTES.
4. ACCESS SHALL BE MAINTAINED FOR RESIDENTS AT ALL TIMES.

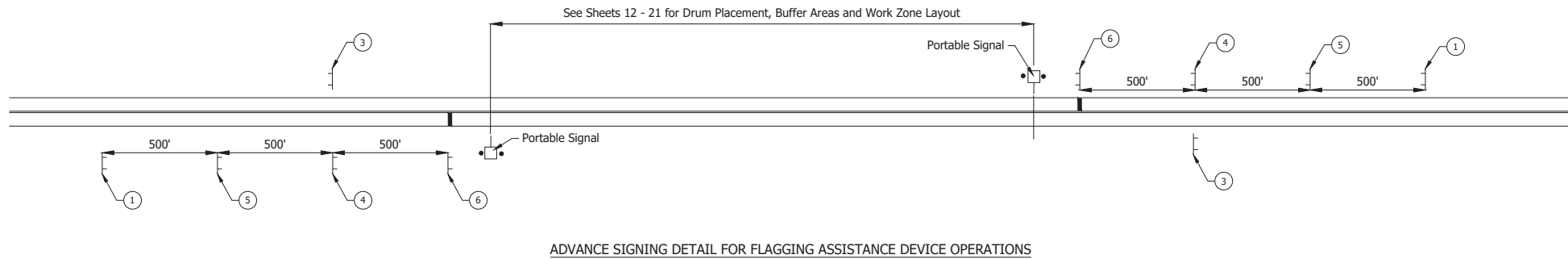
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE ____/____/____
DESIGNED: _____ SMF	DRAWN: _____ SMF	
CHECKED: _____ JNH	CHECKED: _____ JNH	

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
DETOUR ROUTE SR 39 & AIRPORT RD

HORIZONTAL SCALE 1"=500'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK	SHEETS 10 of 78
CONTRACT R-41180	PROJECT 1702835

Date: Sep 14, 2020, 9:03pm User Name: SFisch
File: S:_2018\218-026-Road(CAD)\INDOT Signage.dwg

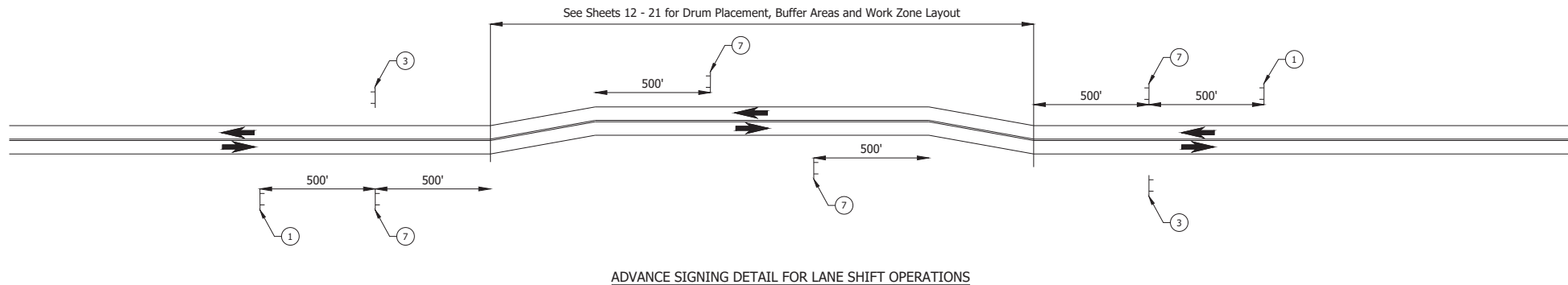


CONSTRUCTION SIGN LEGEND:

- ① ROAD CONSTRUCTION (AHEAD)/(1500 FEET) - (XW20-1)
- ③ END CONSTRUCTION - (XG20-2)
- ④ SIGNAL AHEAD - (XW3-3)
- ⑤ ONE LAND ROAD AHEAD - (XW20-4)
- ⑥ STOP HERE ON RED - (XR10-6)
- ⑦ TURN AND CURVE SIGNS - (XW1-4)
- └─ CONSTRUCTION SIGN

SUMMARY OF CONSTRUCTION SIGN SCHEDULE

SIGN NO.	DESCRIPTION	SIZE	TYPE	EST. QTY	UNIT
XW20-1	ROAD CONSTRUCTION AHEAD	36"X36"	A	4	EACH
XG20-2	END CONSTRUCTION	36"X18"	A	4	EACH
XW3-3	SIGNAL AHEAD	36"X36"	A	2	EACH
XW20-4	ONE LANE ROAD AHEAD	36"X36"	A	2	EACH
XR10-6	STOP HERE ON RED	24"X36"	A	2	EACH
XW1-4	TURN AND CURVE SIGNS	36"X36"	A	4	EACH
	TOTAL CONSTRUCTION SIGNS			18	EACH



GENERAL NOTES

- All materials, procedures, signs, markings, flashers and miscellaneous items shall conform to the requirements of the current INDOT Standard Specifications and the Indiana Manual on Uniform Traffic Control Devices.
- Each sign or barricade will be paid for only once regardless of how many times it is moved.
- Contractor shall conduct meetings with state police, fire department, ambulance service and local schools to coordinate roadway work.

RECOMMENDED
FOR APPROVAL _____
DESIGN ENGINEER DATE

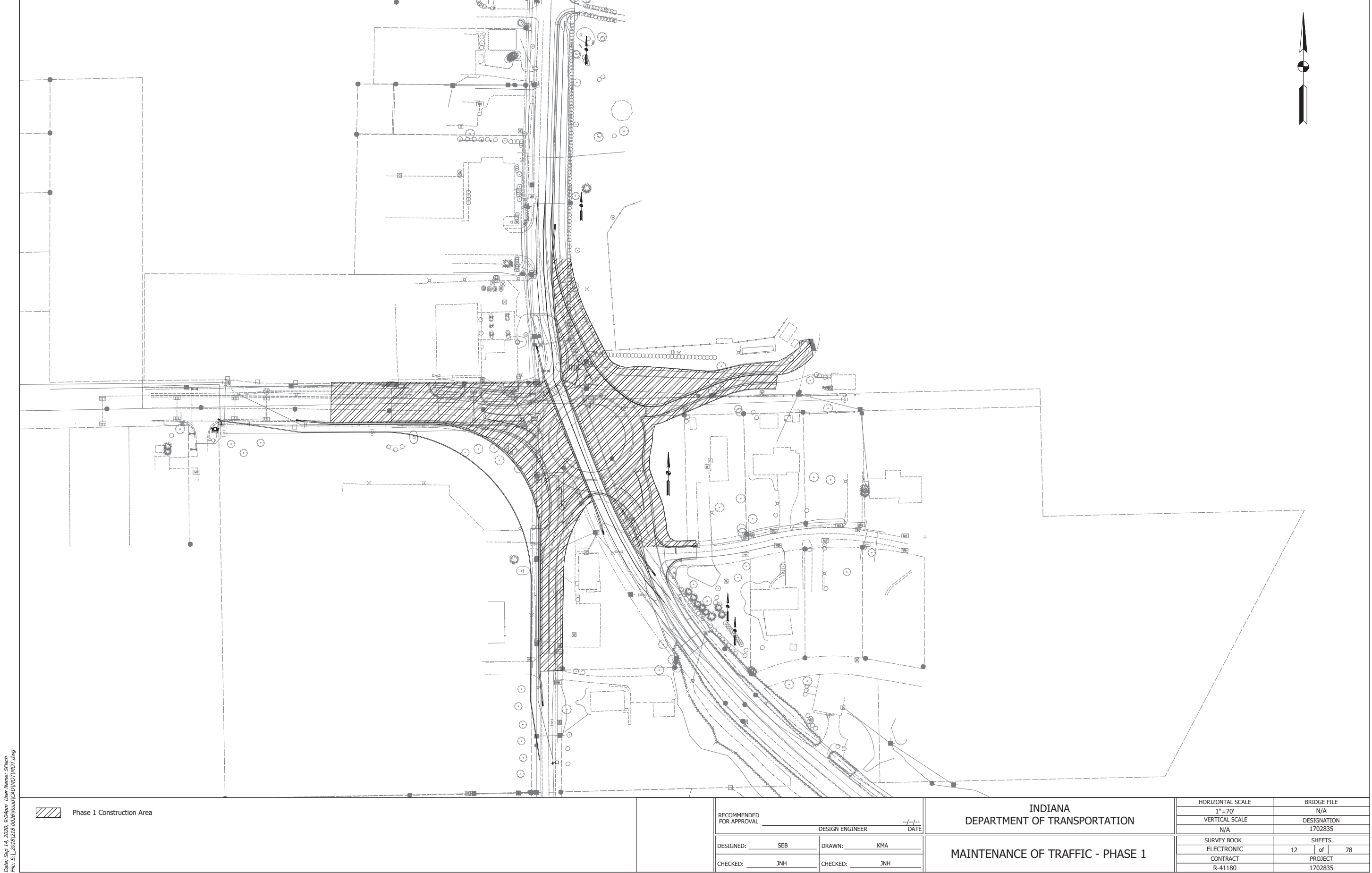
DESIGNED: _____ DRAWN: _____

CHECKED: _____ CHECKED: _____


INDIANA
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET

HORIZONTAL SCALE	BRIDGE FILE
NTS	N/A
VERTICAL SCALE	DESIGNATION
N/A	-
SURVEY BOOK	SHEETS
	11 of 11
CONTRACT	PROJECT
-	218-0062-07H



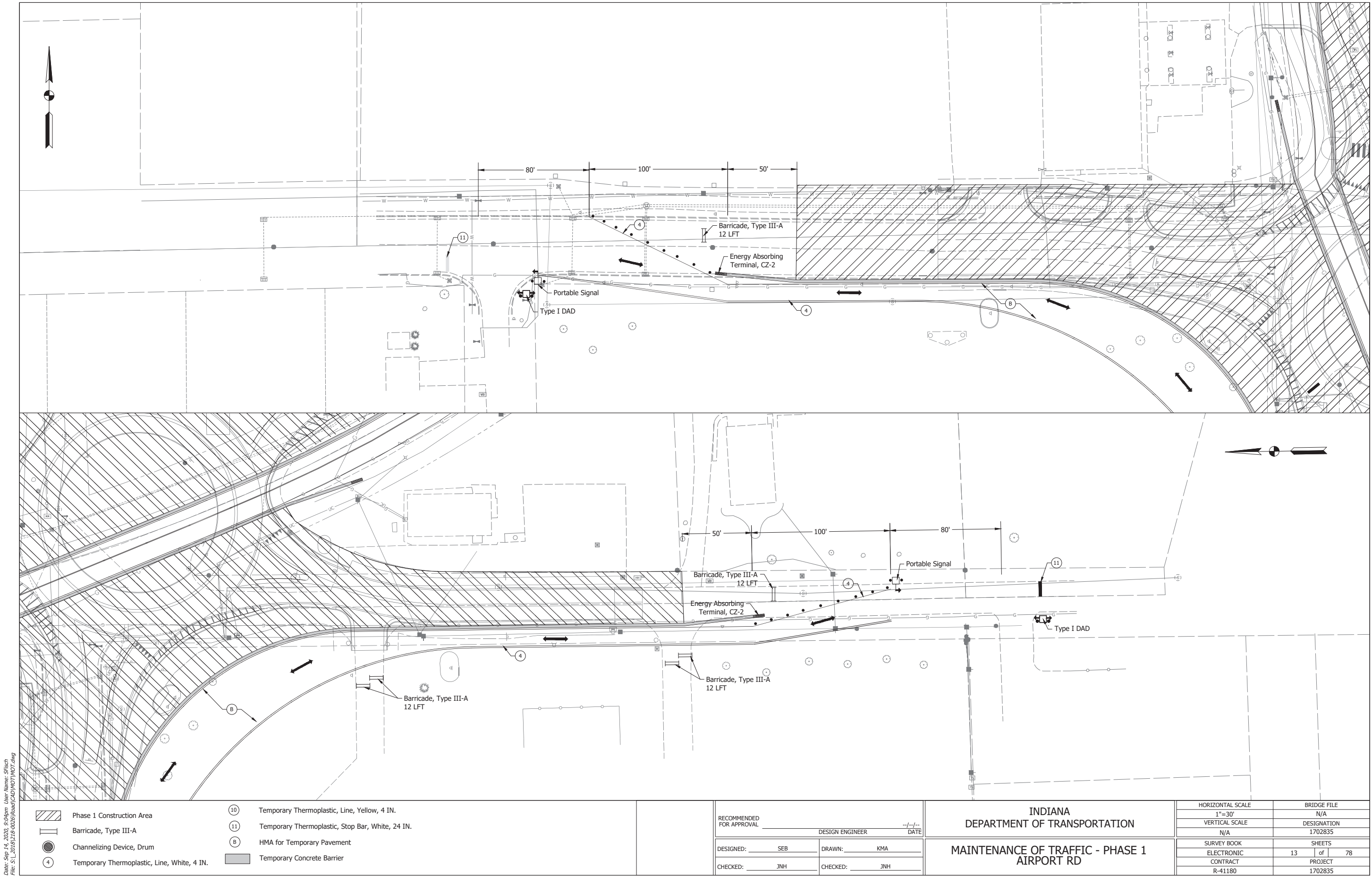
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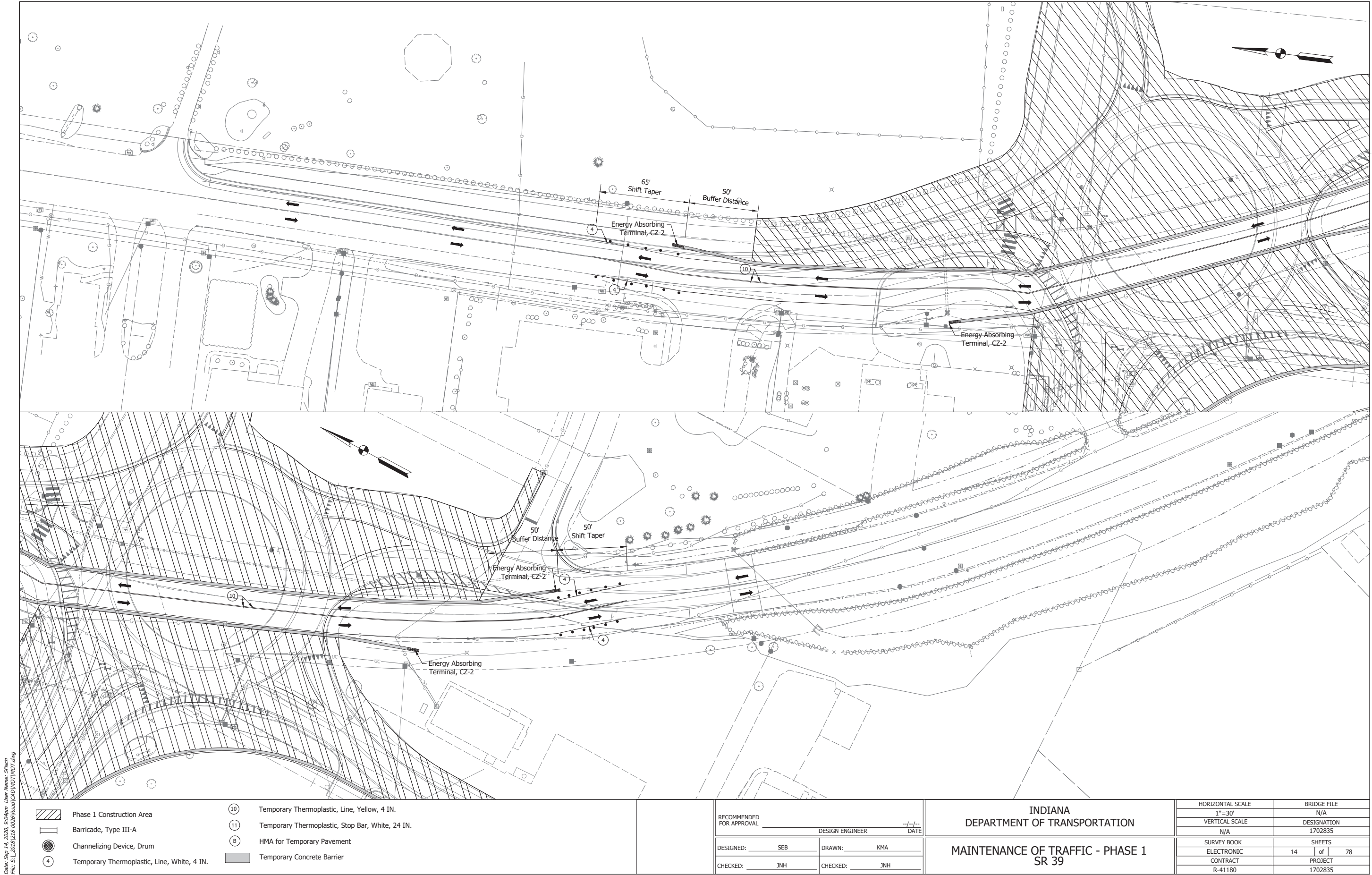
 Phase 1 Construction Area

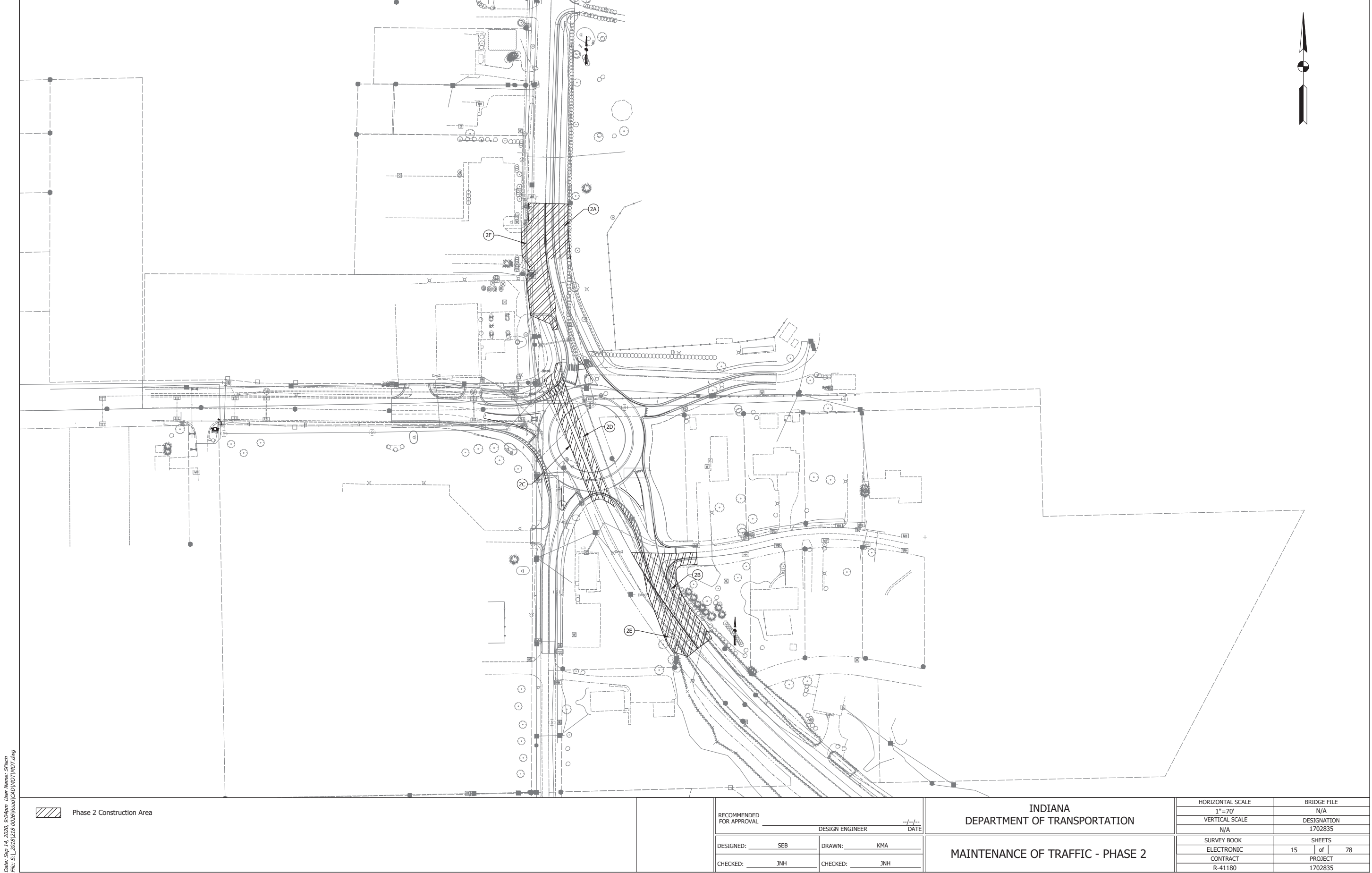
RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE ____/____/____
DESIGNED: _____	SEB	DRAWN: _____	KMA
CHECKED: _____	JNH	CHECKED: _____	JNH

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC - PHASE 1


HORIZONTAL SCALE 1"=70'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK ELECTRONIC CONTRACT R-41180	SHEETS 12 of 78 PROJECT 1702835







Date: Sep 14, 2020 9:44pm User Name: SFisch
File: S:\1702835\1702835Road\CD\1702835.dwg

 Phase 2 Construction Area

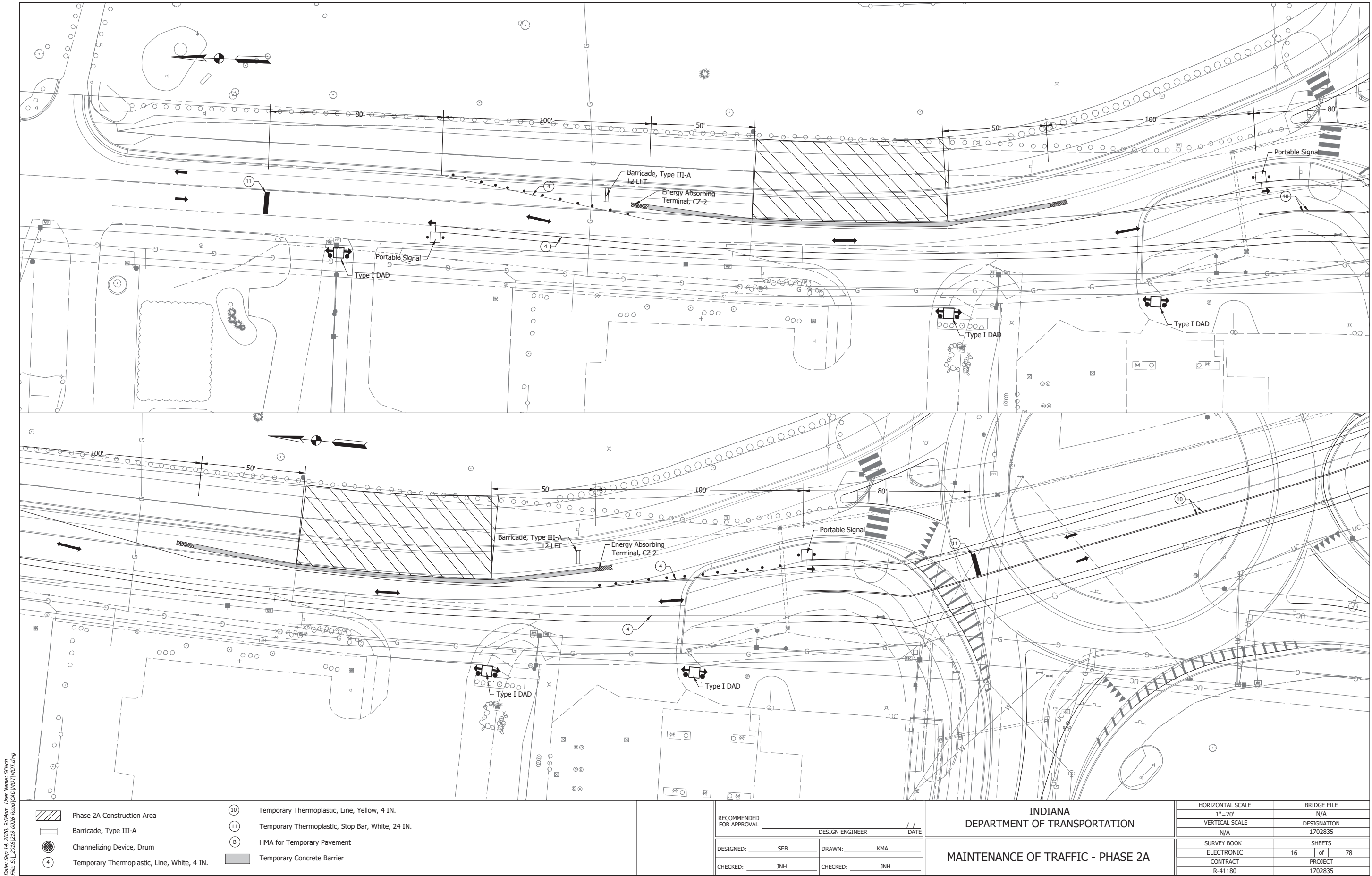
RECOMMENDED FOR APPROVAL _____
DESIGN ENGINEER _____ DATE _____

DESIGNED: _____ SEB DRAWN: _____ KMA
CHECKED: _____ JNH CHECKED: _____ JNH

INDIANA
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC - PHASE 2

HORIZONTAL SCALE	BRIDGE FILE	
1"=70'	N/A	
VERTICAL SCALE	DESIGNATION	
N/A	1702835	
SURVEY BOOK	SHEETS	
ELECTRONIC	15	of 78
CONTRACT	PROJECT	
R-41180	1702835	

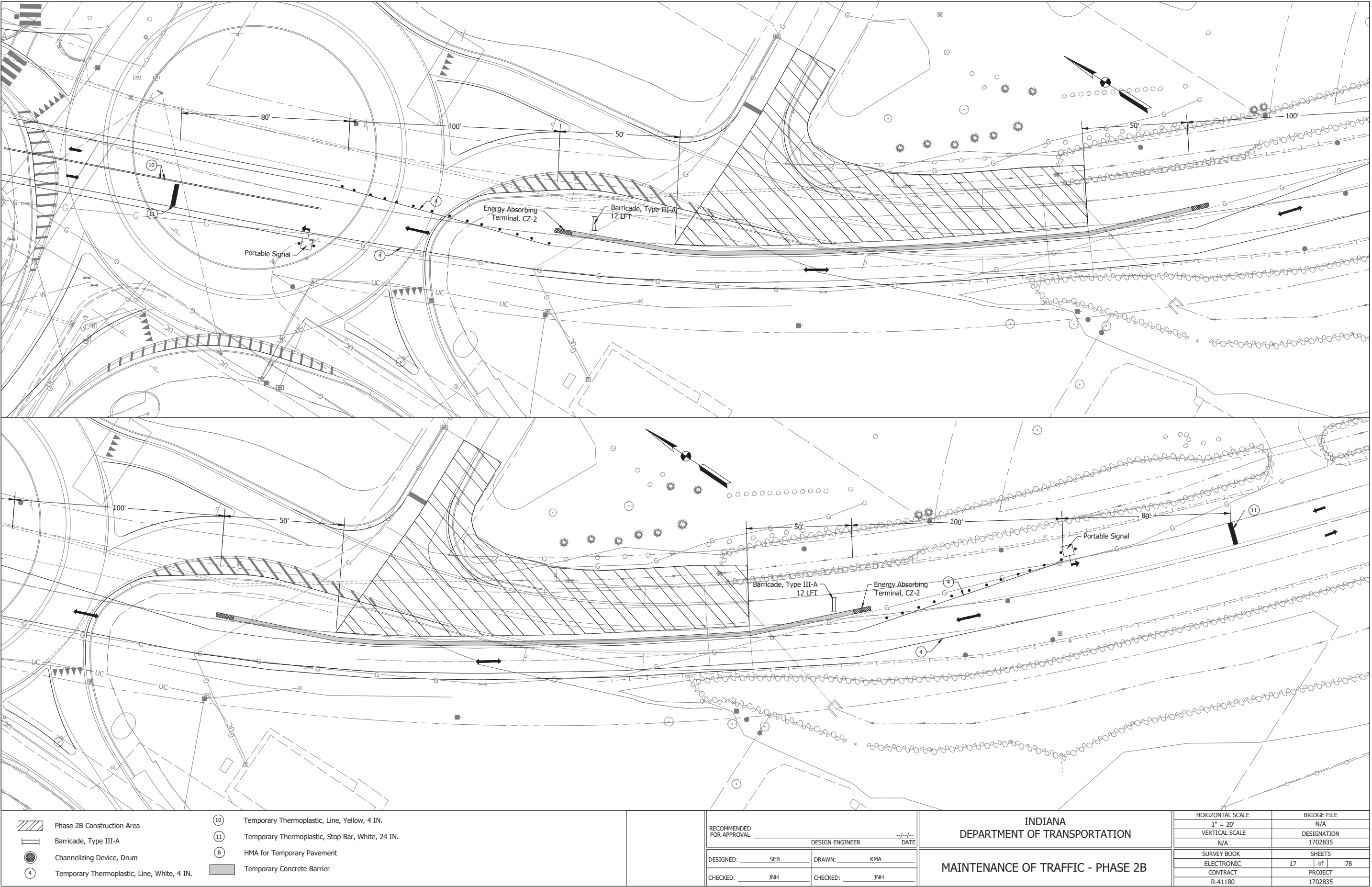


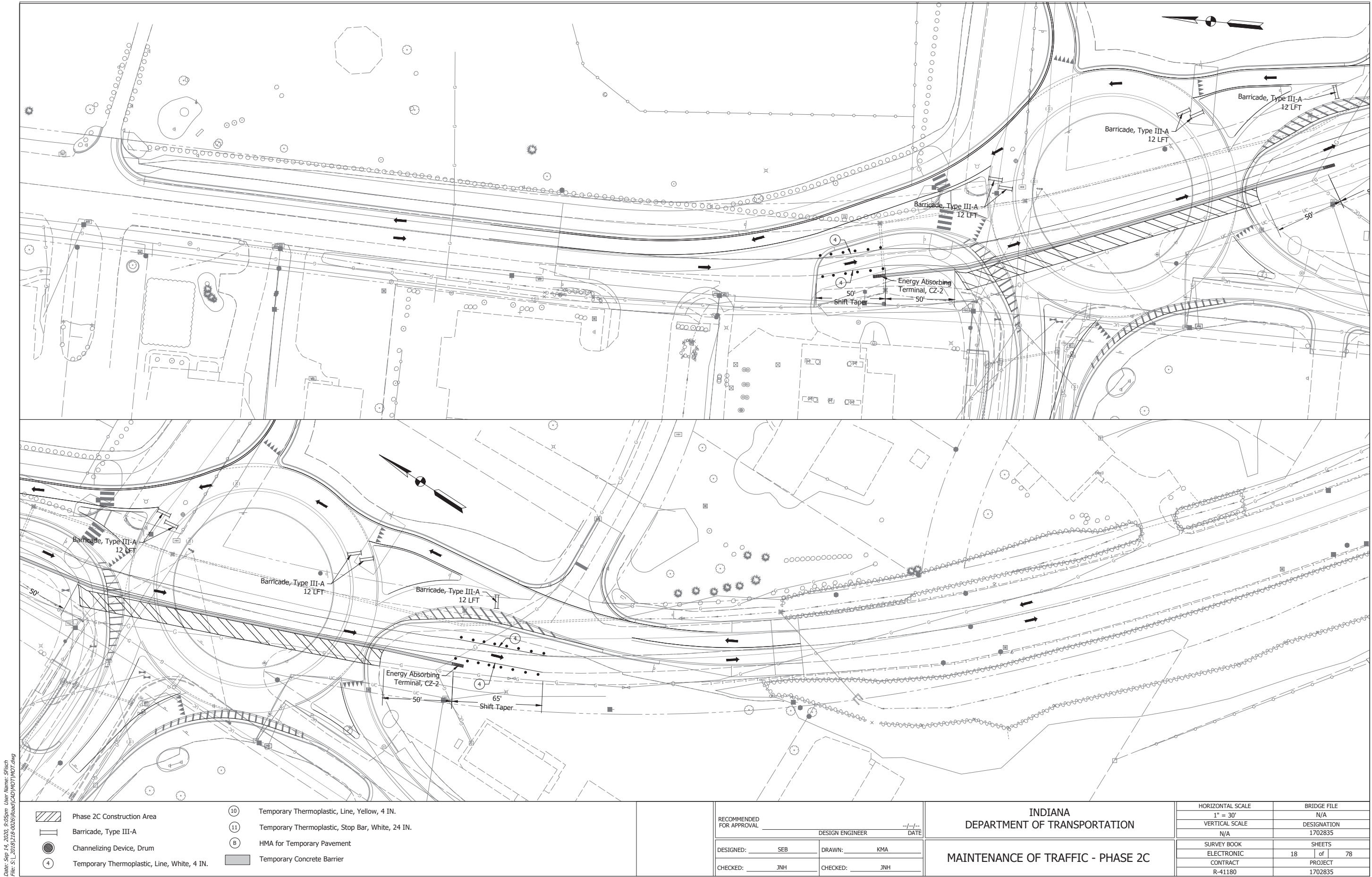
Date: Sep 14, 2020 9:04pm User Name: SPfisch
File: S:_2016\218-026\Road\CAD\MOI\MOT.dwg

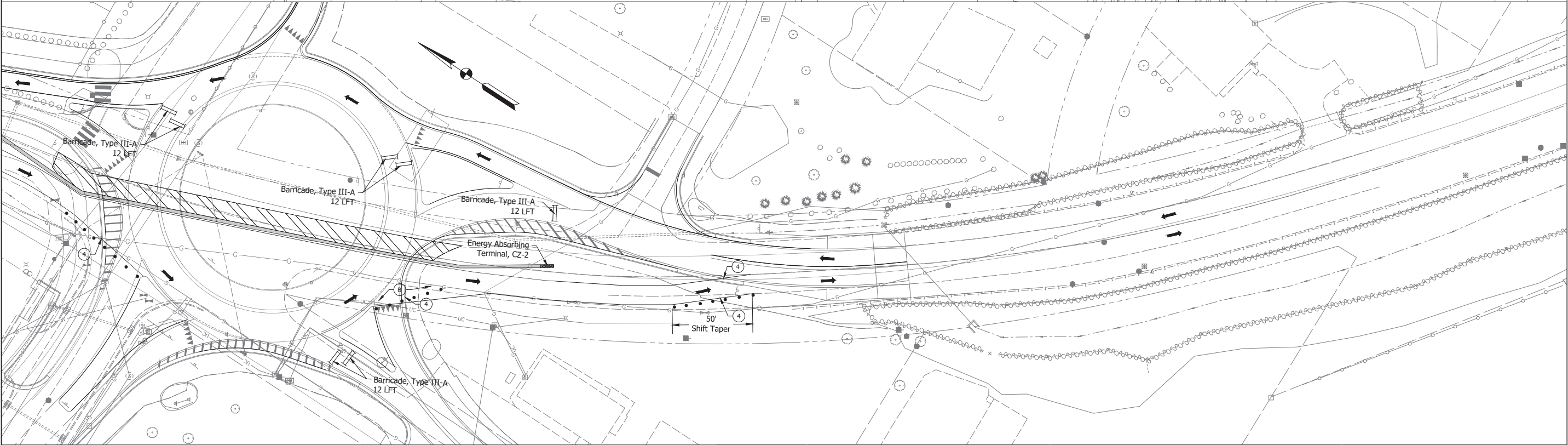
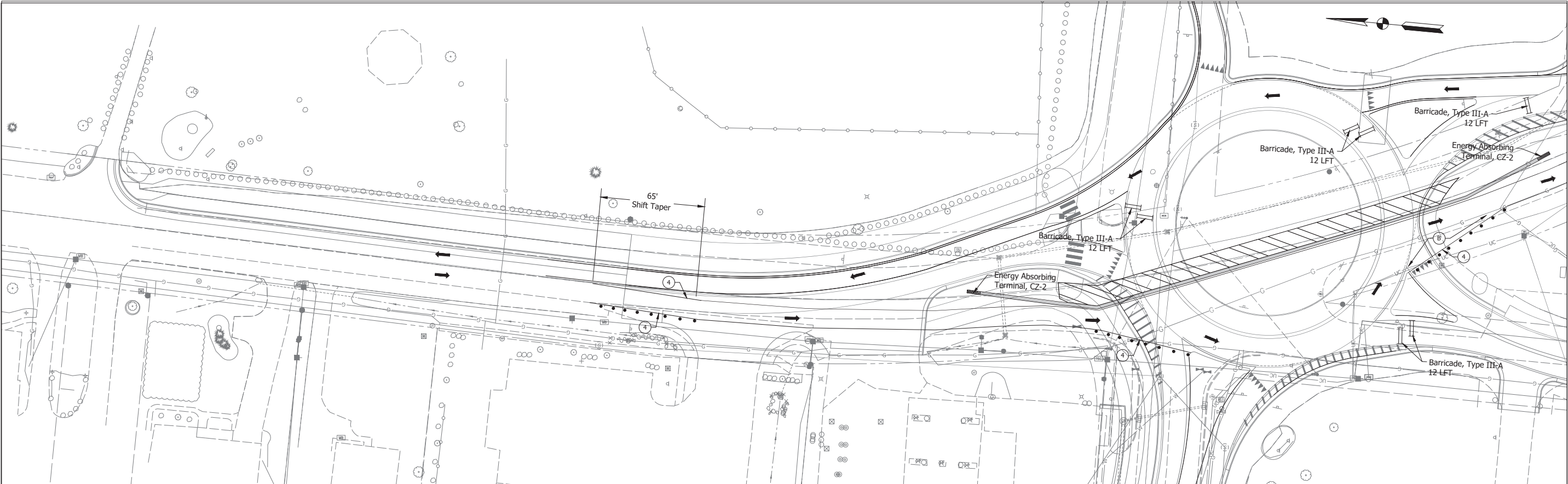
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: SEB		DRAWN: KMA			
CHECKED: JNH		CHECKED: JNH			

INDIANA DEPARTMENT OF TRANSPORTATION	
MAINTENANCE OF TRAFFIC - PHASE 2A	

HORIZONTAL SCALE 1"=20'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK ELECTRONIC CONTRACT R-41180	SHEETS 16 of 78 PROJECT 1702835







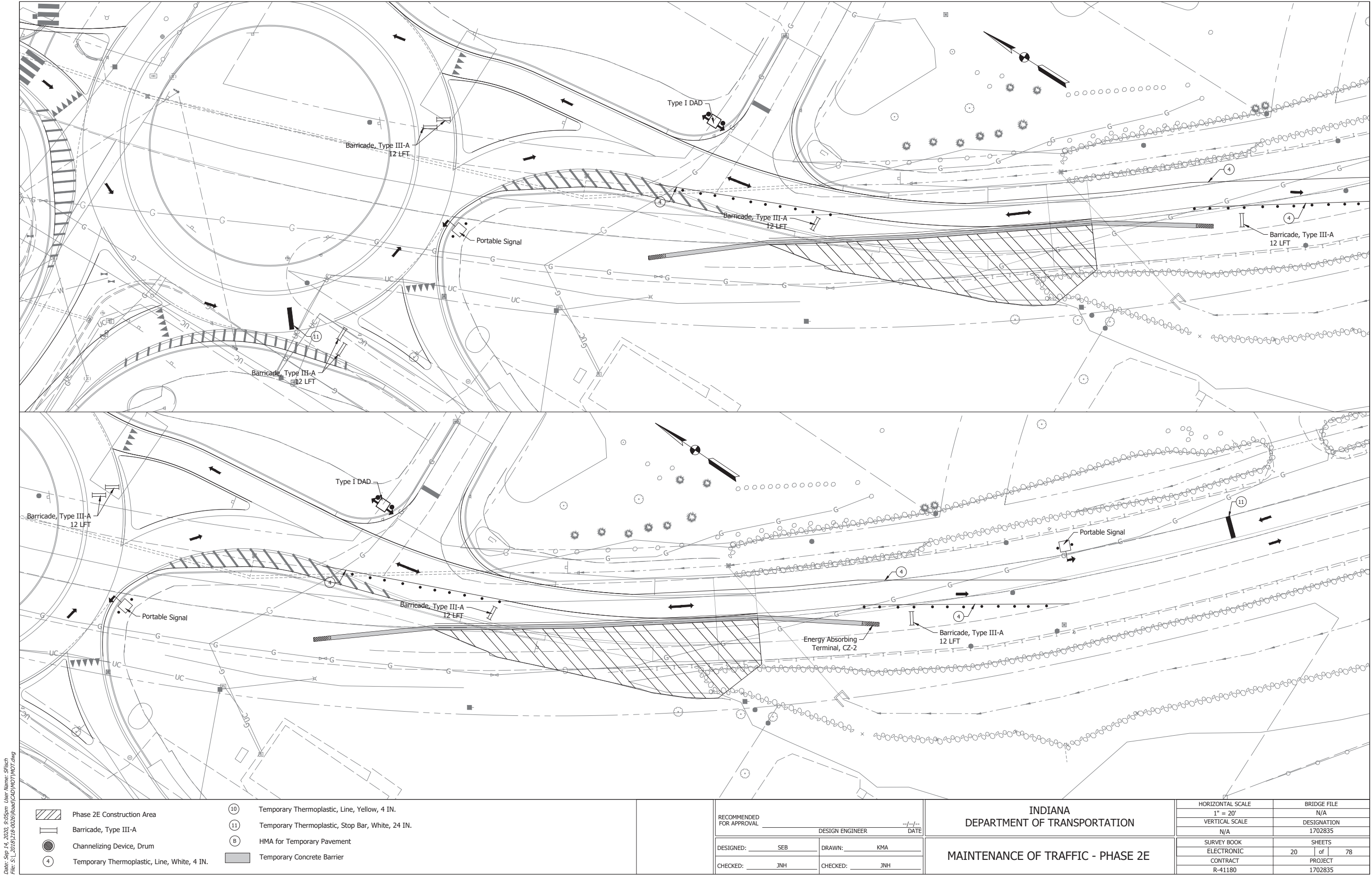
Date: Sep 14, 2020, 9:05pm User Name: SPfisch
File: S:\2016\216-026\Road\CAD\NOT\NOT.dwg

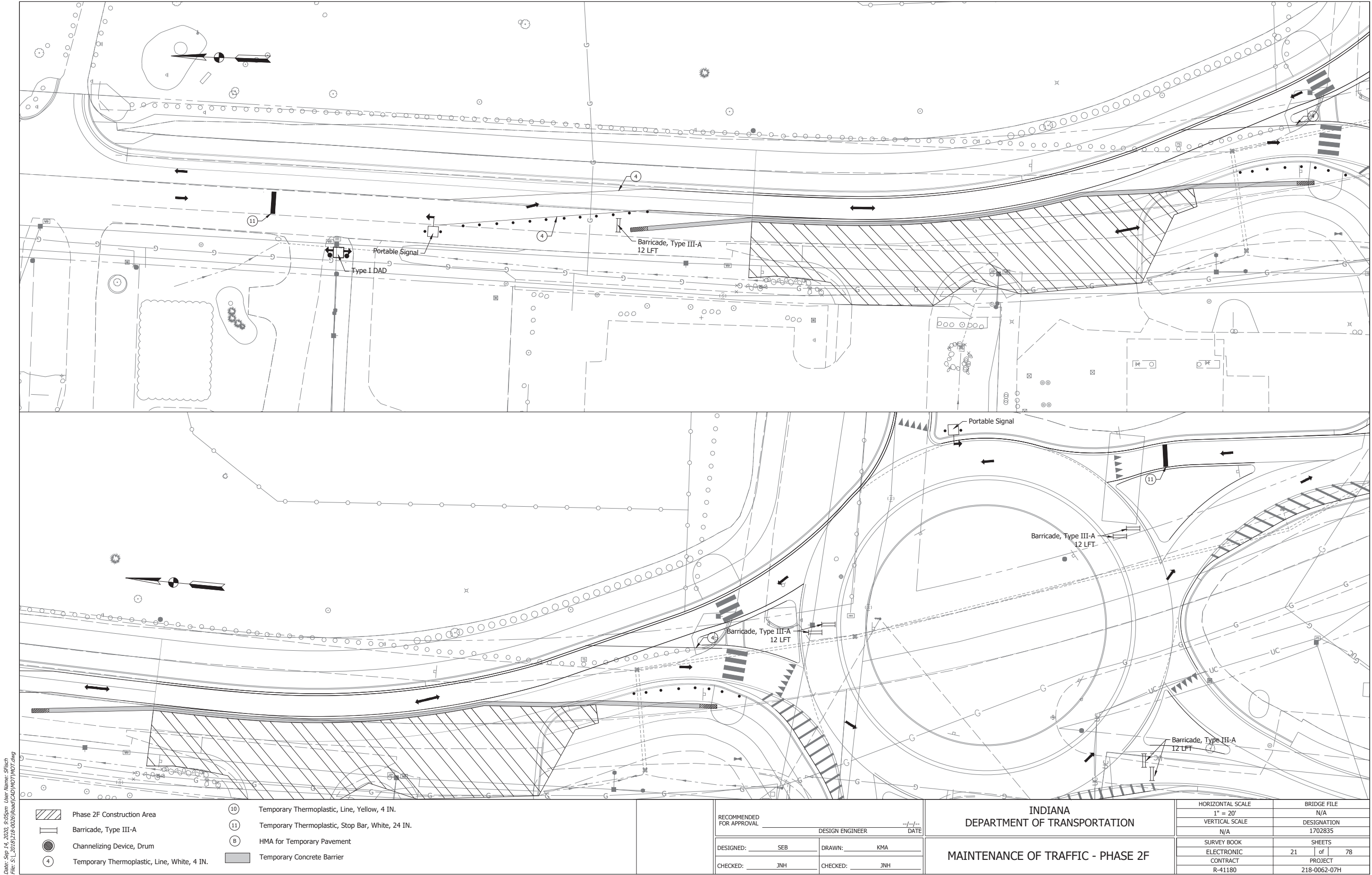
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	Barricade, Type III-A		Temporary Thermoplastic, Stop Bar, White, 24 IN.
	Channelizing Device, Drum		HMA for Temporary Pavement
	Temporary Thermoplastic, Line, White, 4 IN.		Temporary Concrete Barrier

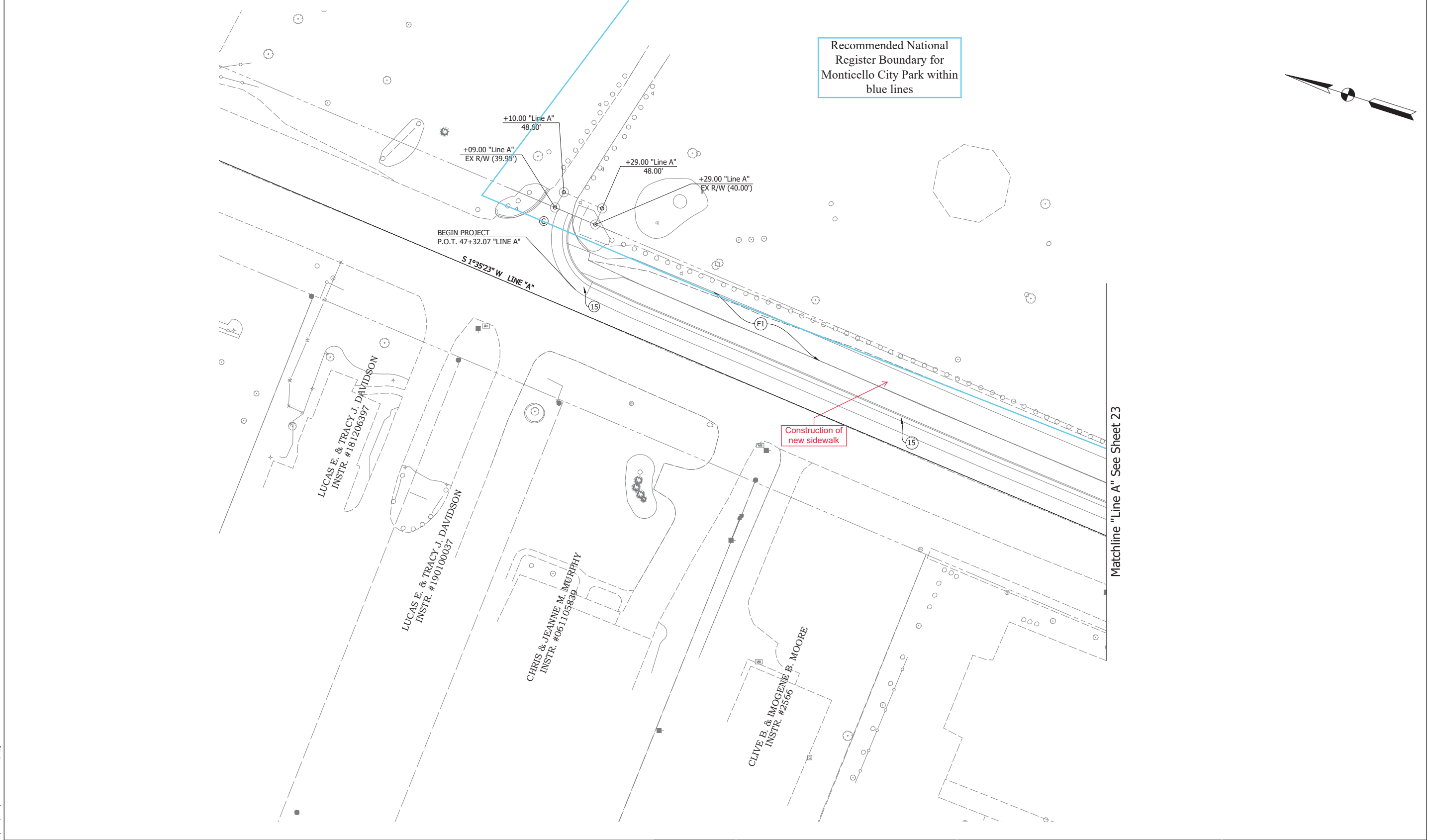
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: SEB	DRAWN: KMA	
CHECKED: JNH	CHECKED: JNH	

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC - PHASE 2D

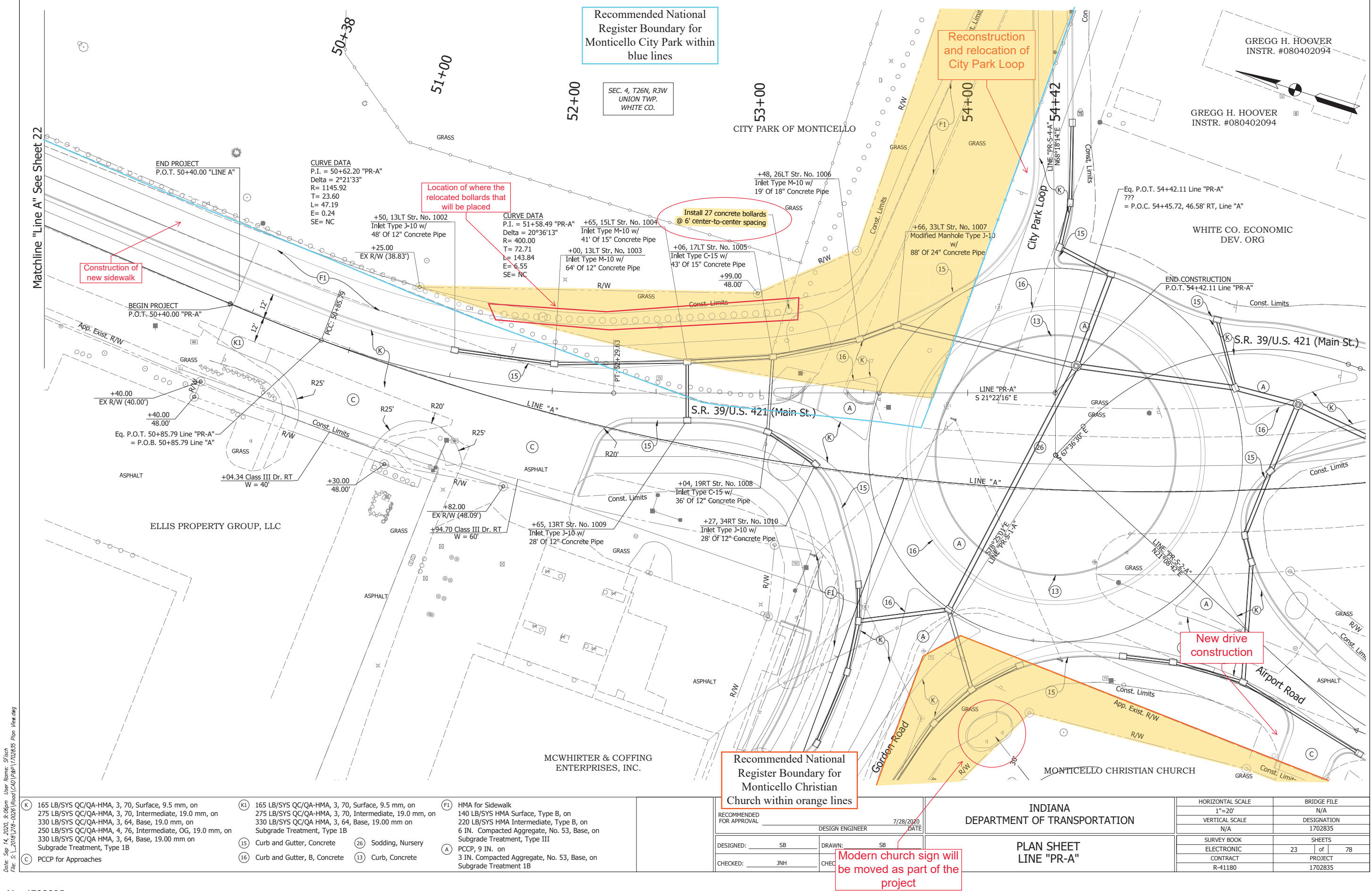
HORIZONTAL SCALE 1" = 30'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1702835
SURVEY BOOK ELECTRONIC CONTRACT R-41180	SHEETS 19 of 78 PROJECT 1702835







<div><div><div>K</div><div>165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B</div></div><div><div>C</div><div>PCCP for Approaches</div></div></div>	<div><div><div>K1</div><div>165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA HMA, 3, 64, Base, 19.00 mm on Subgrade Treatment, Type 1B</div></div><div><div>15</div><div>Curb and Gutter, Concrete</div></div><div><div>16</div><div>Curb and Gutter, B, Concrete</div></div></div>	<div><div><div>F1</div><div>HMA for Sidewalk 140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III</div></div><div><div>A</div><div>PCCP, 9 IN. on 3 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B</div></div></div>	<div><div>RECOMMENDED FOR APPROVAL</div><div>DESIGN ENGINEER</div><div>7/28/2020</div><div>DATE</div></div>	<div>INDIANA DEPARTMENT OF TRANSPORTATION</div> <div>PLAN SHEET LINE "A"</div>	<div><div>HORIZONTAL SCALE</div><div>1"=20'</div></div>	<div><div>BRIDGE FILE</div><div>N/A</div></div>
					<div><div>VERTICAL SCALE</div><div>N/A</div></div>	<div><div>DESIGNATION</div><div>1702835</div></div>
					<div><div>SURVEY BOOK</div><div>ELECTRONIC</div></div>	<div><div>SHEETS</div><div>23 of 78</div></div>
					<div><div>CONTRACT</div><div>R-41180</div></div>	<div><div>PROJECT</div><div>1702835</div></div>



Proposed Permanent ROW

Recommended National Register Boundary for Monticello City Park within blue lines

Recommended National Register Boundary for Monticello Christian Church within orange lines

Date: Sep 14, 2020 9:06am User Name: S.Fisch
File: S:\2018\1702835\1702835 Plan View.dwg

Modern church sign will be moved as part of the project

165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm, on 250 LB/SYS QC/QA-HMA, 4, 76, Intermediate, OG, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment, Type 1B

K1 165 LB/SYS QC/QA-HMA, 3, 70, Surface, 9.5 mm, on 275 LB/SYS QC/QA-HMA, 3, 70, Intermediate, 19.0 mm, on 330 LB/SYS QC/QA-HMA, 3, 64, Base, 19.0 mm on Subgrade Treatment, Type 1B
15 Curb and Gutter, Concrete
16 Curb and Gutter, B, Concrete
26 Sodding, Nursery
13 Curb, Concrete

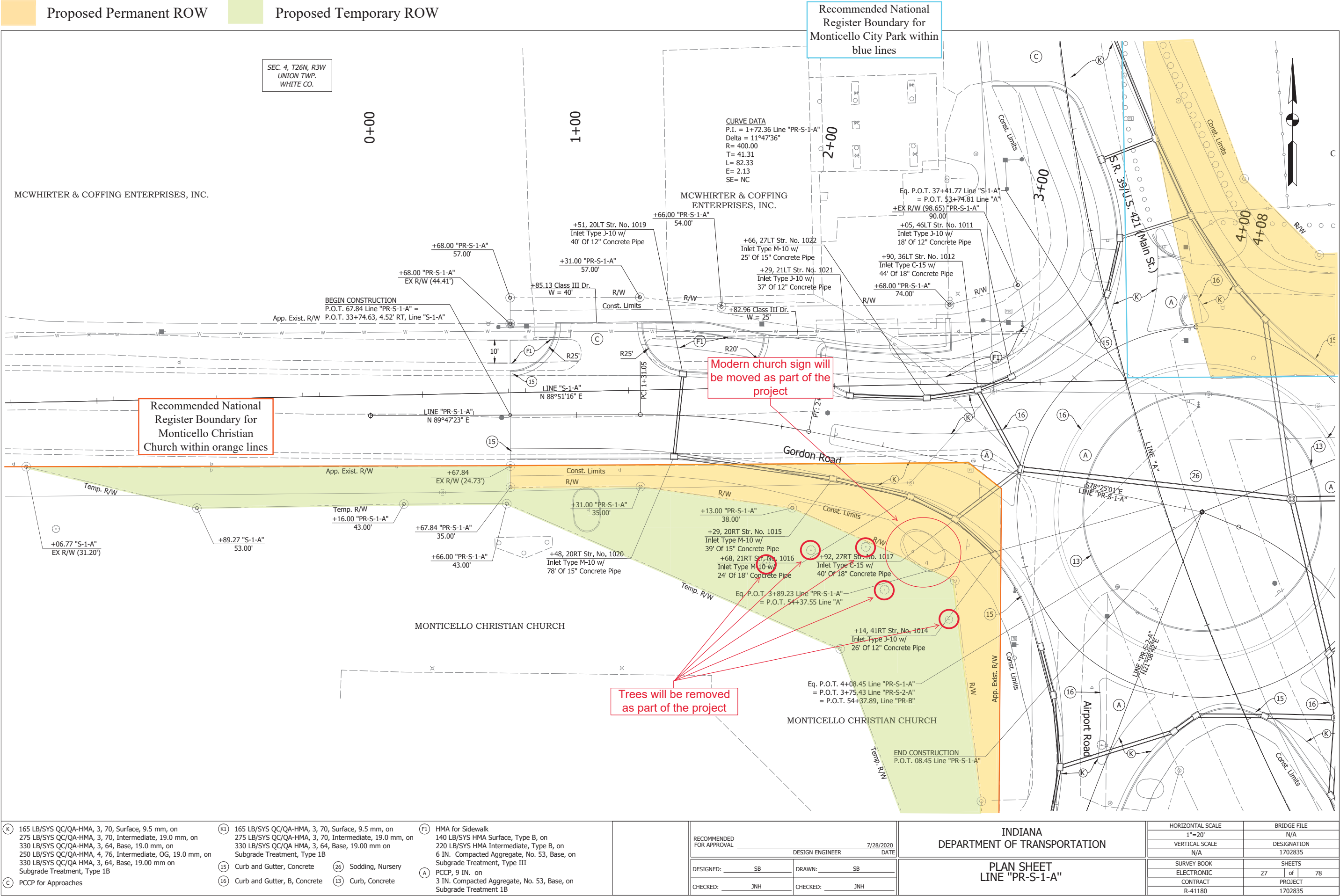
F1 HMA for Sidewalk
140 LB/SYS HMA Surface, Type B, on 220 LB/SYS HMA Intermediate, Type B, on 6 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment, Type III
PCCP, 9 IN. on 3 IN. Compacted Aggregate, No. 53, Base, on Subgrade Treatment 1B

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	7/28/2020	DATE
DESIGNED: SB	DRAWN: SB		
CHECKED: JNH	CHECKED: JNH		

INDIANA
DEPARTMENT OF TRANSPORTATION

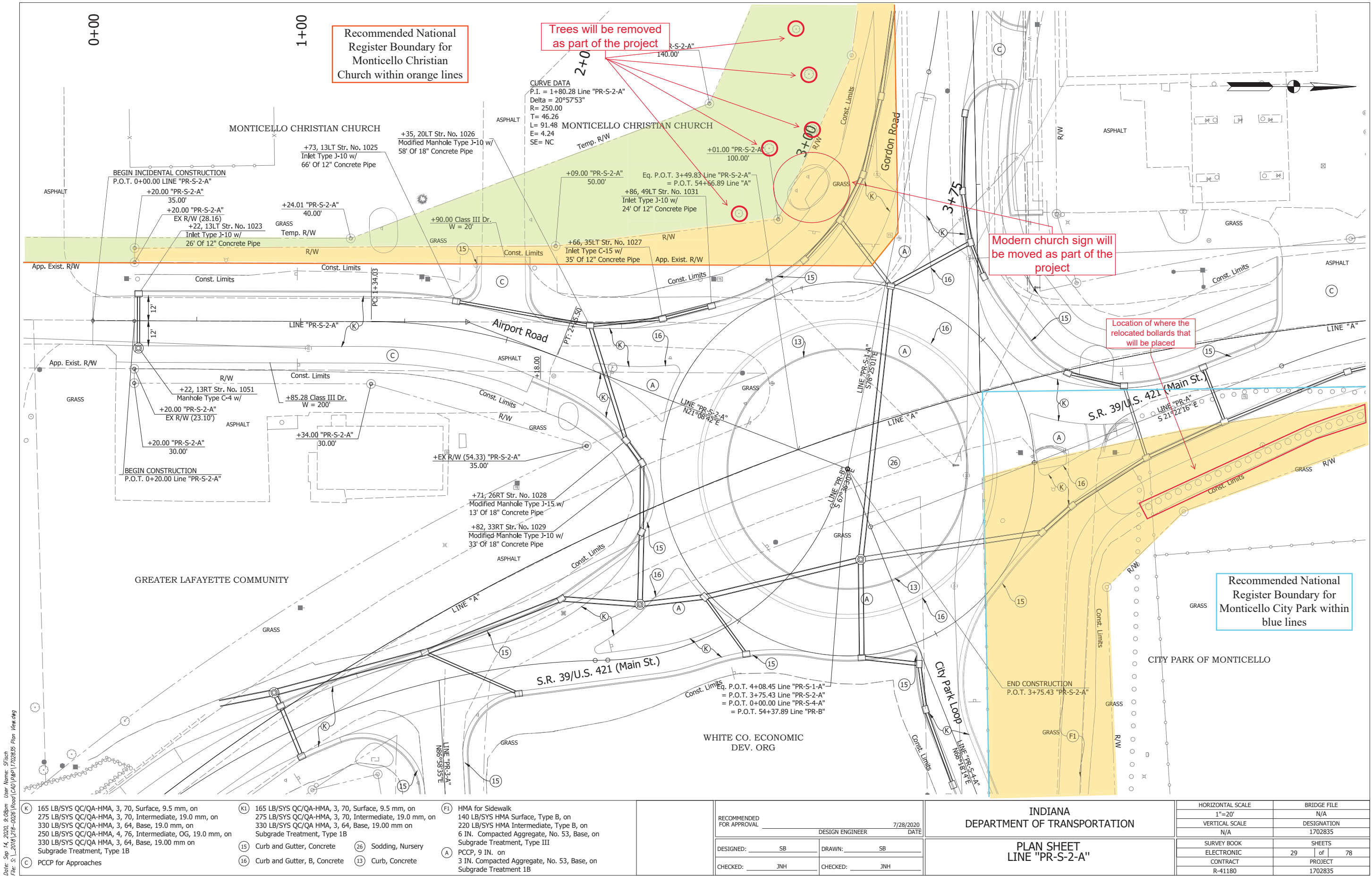
PLAN SHEET
LINE "PR-B"

HORIZONTAL SCALE	BRIDGE FILE
1"=20'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1702835
SURVEY BOOK	SHEETS
ELECTRONIC	25 of 78
CONTRACT	PROJECT
R-41180	1702835



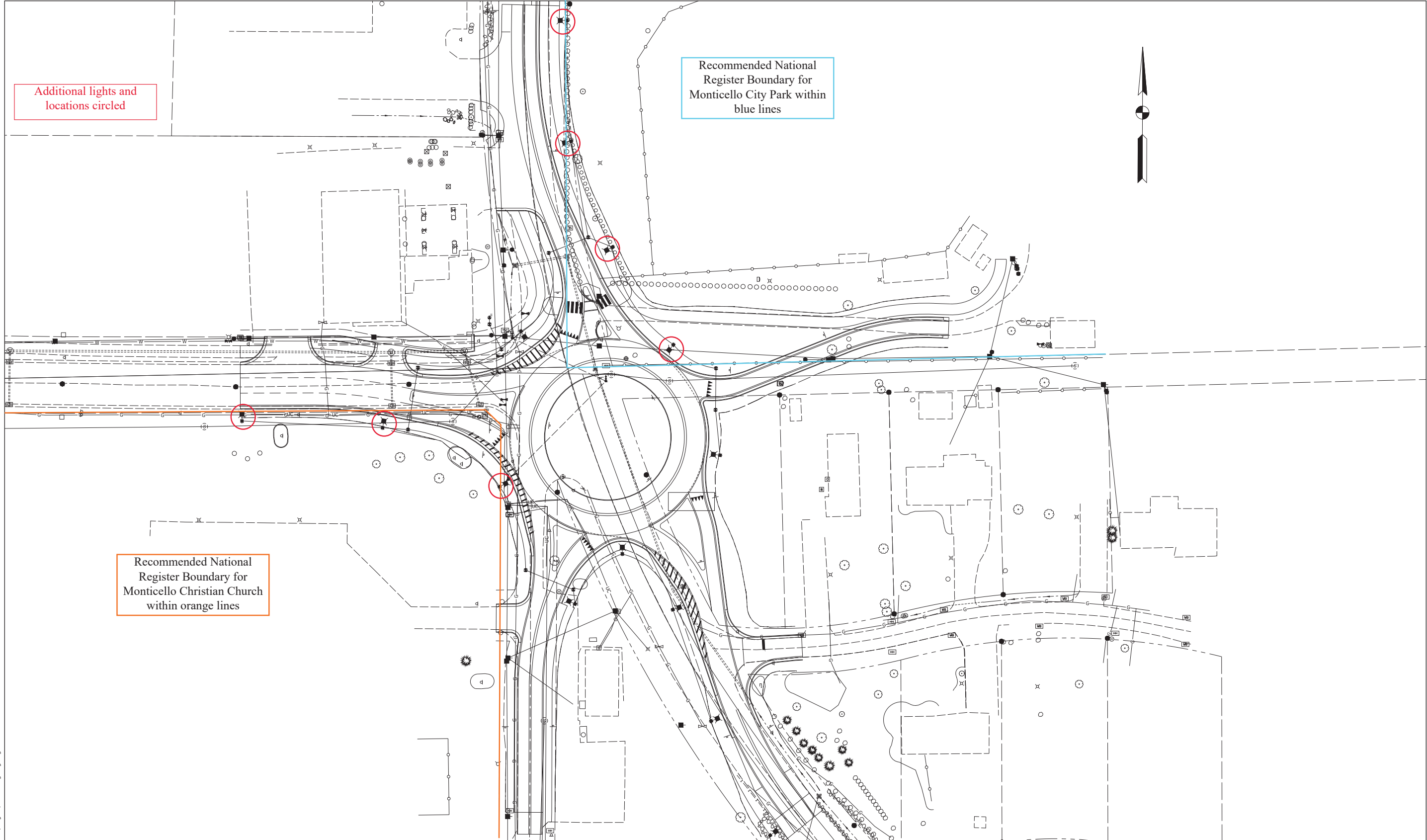
Proposed Permanent ROW

Proposed Temporary ROW



Proposed Temporary ROW





Existing Luminaire To Be Removed

Light Standard and Foundation

High Lumen Roadway Luminaire

40' Height - 15' Mast Arm on Anchor Base

Light Standard and Foundation

High Lumen Roadway Luminaire

40' Height - 15' Mast Arm on Breakaway Base

Luminaire, Underpass

High Mast Tower - 140' EMH
w/ 6 High Mast Asymmetric Directional
Luminaires

High Mast Tower - 140' EMH
w/ 6 High Mast Asymmetric Luminaires

High Mast Tower - 160' EMH
w/ 8 High Mast Symmetric Luminaires

Existing Right of Way
Service Point, Type II

Lighting Handhole

Cable Duct Marker

Lighting Controller

Conduit

Circuit Labels

LUMINAIRE IDENTIFICATION

82 - 69 - 41 - 01

RECOMMENDED FOR APPROVAL

DESIGNED: KMS

CHECKED: TWK

DESIGN ENGINEER

DRAWN: KMS

CHECKED: TWK

DATE

INDIANA
DEPARTMENT OF TRANSPORTATION

LIGHTING LAYOUT

HORIZONTAL SCALE	BRIDGE FILE
1"=40'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1702835
SURVEY BOOK	SHEETS
	53 of 98
CONTRACT	PROJECT
R-41180	1702835

Categorical Exclusion

Appendix C

Early Coordination



January 30, 2020

Sample Early
Coordination Letter

Re: Des. No.: 1702835
Intersection Improvement Project
Local Project
S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop
City of Monticello, White County, Indiana

Dear :

The Board of White County Commissioners, with funding from the Federal Highway Administration (FHWA) and oversight by the Indiana Department of Transportation (INDOT), intend to proceed with an intersection improvement project at the five-legged intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County, Indiana (Des. No. 1702835). This letter is part of the early coordination phase of the environmental review process. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above Des. No. and project description in your reply.** We will incorporate your comments into the formal environmental study. Your cooperation in this endeavor is appreciated.

Project Location and Existing Conditions

The proposed project is located at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. Specifically, the project is located in Section 4, Township 26 North, Range 3 West of Union Township as depicted on the Monticello South U.S. Geological Survey 1:24,000 scale quadrangle. Adjacent land use consists of residential development, commercial properties, as well as recreational and religious facilities. Please see attachments for maps and photographs of the proposed project area.

S. Main Street is a north-south roadway functionally classified as a minor arterial. The roadway typical cross-section consists of two, 12-foot wide asphalt travel lanes (one in each direction). The shoulder along the southbound lane is asphalt and is approximately 9 feet wide. Along the northbound lane, there is a 6-foot to 12-foot wide asphalt paved shoulder. Additionally, the southbound approach of S. Main Street widens near Gordon Road to provide for an 11-foot designated right-turn lane. North of the Gordon Road intersection, along the east paved shoulder, there is also an additional 2 to 12-foot wide aggregate pull-off that serves as public street parking for Monticello City Park. The posted speed limit is 35 miles per hour (mph).

3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
PHONE: 317.222.3878 • TOLL FREE: 800.423.7422

Airport Road is a north-south roadway functionally classified as a minor arterial. The roadway typical section consists two, 11-foot wide asphalt travel lanes (one in each direction) with no discernible shoulder. The posted speed limit is 25 mph.

Gordon Road is an east-west roadway functionally classified as a minor arterial. The roadway typical section consists of two, 11-foot wide asphalt travel lanes with asphalt shoulders of varying size, up to 2-foot wide. The posted speed limit is 35 mph. An existing 10-foot paved multi-use path runs along the north side of Gordon Road on the west side of the project area until it reaches the intersection with S. Main Street. Currently, there are Americans with Disabilities Act (ADA) compliant curb ramps at the northwest corner of Gordon Road and S. Main Street.

Oak Drive is an east-west roadway functionally classified as a local road. The roadway typical section consists of two, 10-foot asphalt travel lanes with no discernible shoulder. The posted speed limit is 25 mph.

City Park Loop is a one-way roadway that runs through Monticello City Park. The existing section consists of one, 10-foot wide travel lane and no discernible shoulder. City Park Loop serves as vehicular access to the park property that enters to the north of the subject intersection and exits at the intersection to be improved by this project. The posted speed limit is 10 mph.

In addition, the City of Monticello has a sidewalk construction project north of this proposed intersection improvement project. That project, which is being processed separately from this project, will add sidewalks along both sides of S. Main Street north to Tioga Road. These sidewalks should be constructed ahead of this project.

Purpose and Need

The need for this project stems from the poor level of service (LOS) for the five-legged intersection. There are six LOS levels ranging from LOS A (free flow) to LOS F (oversaturated). LOS C is considered acceptable. Currently, according to a traffic study performed by Lochmueller Group in January 2020, the LOS for the intersection overall is LOS C during both AM and PM peak hours. All approaches exhibit LOS C or better with the exception of the eastbound approach (Gordon Road) which exhibits LOS D during the AM peak hours and LOS E at PM peak hours. The design year (2042) LOS for the current signalized intersection is forecasted to be LOS F, which as stated above is oversaturated and considered unacceptable. A secondary need of the project is the lack of connectivity in pedestrian facilities in the area. The purpose of the proposed project is to improve the LOS of each of the approaches to a minimum of LOS C and improve the connectivity for pedestrian facilities.

Proposed Project

The proposed project will involve the construction of a roundabout at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. The roundabout will consist of one, 18-foot asphalt circulating travel lane and a 12-foot wide concrete truck apron with mountable concrete roll curb. The center of the roundabout will consist of a 115-foot diameter sodded area.

The approaches of Gordon Road, S. Main Street, Airport Road, and City Park Loop will all shift generally to the south or southeast and widened nearing the proposed roundabout. The shifting of the north S. Main Street approach could require the relocation of up to 14 concrete bollards currently used to delineate the boundary of the park property. Up to 200 feet of the existing alignment of City Park Loop at its exit to S. Main Street will be realigned to the south. The project will also include updates to the curb and curbside drainage along the affected roadways. Additionally, a section of Oak Drive will be reconstructed to full depth with new curbs and drainage. Oak Drive will maintain its current alignment and will not tie into the roundabout.

The proposed typical cross-section for Gordon Road will consist of two asphalt travel lanes that vary in width from 12 feet to 17 feet with 2-foot, 7-inch wide concrete curbs and gutter on both sides. Additionally, as the roadway approaches the new roundabout a 9-inch raised median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

The proposed typical cross-section for S. Main Street will consist of two asphalt travel lanes that vary in width from 12 feet to 16 feet and 11 inches with 2-foot, 7-inch concrete curb and gutter along both side of the roadway south of the intersection and along the west side of the roadway north of the intersection. Along the east side of the intersection, north of the new roundabout the existing widened shoulder used for public parking will remain. Additionally, as the roadway approaches the new roundabout a 9-inch raised median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

The proposed typical cross-section for Airport Road will consist of two asphalt travel lanes that vary in width from 12 feet to 16 feet and 11 inches with 2-foot, 7-inch concrete curb and gutter along both sides of the roadway. Additionally, as the roadway approaches the new roundabout a 9-inch raised concrete median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

The alignment of the existing Gordon Road Trail will also be shifted to the northeast to accommodate the proposed roundabout. This includes removing and replacing the existing ADA-compliant curb ramp at the northwest corner of the intersection and extending the path across S. Main Street to connect the Gordon Road Trail to Monticello City Park. With the project's realignment of City Park Loop, a portion of the existing roadway pavement of City Park Loop will be repurposed as trail extending into the park.

Within the project limits, excavation to a depth of approximately 2.5 feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection. This work will likely require excavation up to 10 feet below current ground surface.

Constructing a roundabout at this intersection is forecasted to bring the LOS of each approach at this intersection to a LOS A during peak hours.

The maintenance of traffic (MOT) for this project will occur in phases to ensure motorists have access through the intersection throughout construction. Access will be maintained for property owners during construction of the project. Standard INDOT signage, barricades, and flashing lights will be implemented. The MOT will be implemented per the *Indiana Design Manual* guidelines.

The estimated total length of the project, including all work along the approaches, is 1972 feet (0.37 mile). Construction is anticipated to begin in Fiscal Year (FY) 2023.

Right-of-Way (ROW)

The existing ROW along Gordon Road extends approximately 48 feet north and 40 feet south of the roadway centerline. The existing ROW along US 421/SR 39 extends from 22 to 40 feet east and from 45 to 68 feet west of the roadway centerline. The existing ROW along Airport Road extends 20 feet east and from 37 to 45 feet west of the roadway centerline. Additional ROW will be required for this project; however, the exact width and area of the proposed ROW is not known at this early stage of the project. It is anticipated that up to approximately 4 acres of permanent ROW could be necessary to complete the project. No temporary ROW is expected to be needed at this time. Development of the ROW will be refined as the project advances. Minor tree clearing may be required within the proposed construction limits.

Environmental Resources

A Red Flag Investigation (RFI) was performed for a 0.5-mile radius for the project area. Red Flags were identified within the 0.5-mile search radius; however, not all will impact, or be impacted by the proposed project. The RFI noted the following:

- One church, Monticello Christian Church, is located within the project area.
- The White County Airport is located 0.98 mile south of the project area.
- Monticello City Park, a section 4(f) resource managed by the Monticello Parks Board, is located within the project area.
- The White County/Monticello Trail is also located within the project area.
- One state cleanup site is located 0.26 mile north of the project and the plume of contamination was found to extend south toward the project area.
- One underground storage tank site, a Sunoco gas station, is located within the project area.

Appropriate coordination with the managers of the aforementioned resources will occur.

Lochmueller Group conducted a site visit on September 30, 2019 to identify any environmental constraints. No jurisdictional water resources were noted. However, the presence of Monticello Christian Church, Monticello City Park, and the Gordon Road Trail were all confirmed.

Section 106

The National Register of Historic Places (National Register) and the Indiana Register of Historic Sites and Structures (State Register) were reviewed using the State Historic Architectural and Archaeological Research Database (SHAARD) and SHAARD Geographic Information System (GIS) data published online. No above-ground historical resources on either list are within the project area. The 1994 *White County Interim Report: Indiana Historic Sites and Structures Inventory* (IHSSI) data was also examined; no surveyed resources from this inventory were located within the project area. Two contributing resources listed in the Interim Report are near the project area (Monticello City Park and a Cemetery). The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* by Mead & Hunt (2009) was reviewed. No bridges eligible for listing in the National Register are within the project area. One cemetery was noted within the vicinity of the project area. This project is anticipated to require full Section 106 due to the potentially eligible resources near the project area and the project's scope.

Range-wide Informal Programmatic Consultation

White County is within the range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened northern long-eared bat (*Myotis septentrionalis*). The U.S. Fish and Wildlife Service (USFWS) Range-wide Programmatic Informal Consultation for the Indiana bat and northern long-eared bat (NLEB) will be completed for this project. Completion of the appropriate determination key through the USFWS Information for Planning and Consultation (IPaC) portal will occur. If a determination of "Not Likely to Adversely Affect," or "Likely to Adversely Affect" is reached, then additional consultation with the USFWS will occur through INDOT.

Early Coordination

Should we not receive your response within **30 calendar days** from the date of this letter, it will be assumed that your agency feels that there will be no adverse effects incurred as a result of the proposed project. However, should you find that an extension to the response time is necessary, a reasonable amount may be granted upon request.

If you have any questions regarding this project, please feel free to contact me at (317) 222-3880 or at ckunkel@lochgroup.com. Additionally, should you want to contact the sponsor of this project, White County please contact the Highway Department Superintendent, Mike Kyburz at (219) 984-5851 or mkyburz@whitecountyindiana.us.

Thank you in advance for your input.

Sincerely,



Chris Kunkel
Environmental Specialist
Lochmueller Group, Inc.

Attachments:

- General Location Map
- USGS Monticello South Quadrangle Map
- Aerial Map (2018)
- Preliminary Concept Drawing
- RFI Maps
- Photographs

Removed to avoid duplication;
See Appendix B and Appendix E

Distribution List:

- USFWS, Bloomington Field Office (electronic submission)
- Natural Resources Conservation Service, Indianapolis Office (electronic submission)
- U.S. Army Corps of Engineers, Louisville District (electronic submission)
- U.S. Housing and Urban Development (electronic submission)
- National Park Service
- FHWA – Indiana Division (electronic submission)
- IDNR, Division of Fish and Wildlife (electronic submission)
- IDEM (electronic submission)
- INDOT, Office of Public Involvement (electronic submission)
- INDOT, Office of Aviation (electronic submission)
- INDOT, Environmental Services (electronic submission)
- INDOT, LaPorte District (electronic submission)
- Indiana Geological Survey (electronic submission)
- White County Board of Commissioners (electronic submission)
- White County Council
- White County Highway Department (electronic submission)
- White County Drainage Board
- White County Surveyor's Office
- White County Emergency Management Agency
- White County Floodplain Administrator (electronic submission)
- White County Sheriff's Department
- White County, Union Township Trustee
- White County Economic Development Organization (electronic submission)
- Monticello Street Department (electronic submission)
- Monticello Mayor's Office (electronic submission)
- Monticello City Council
- Monticello Police Department (electronic submission)
- Twin Lakes Community School Corporation (electronic submission)
- Monticello Fire Department (electronic submission)
- Monticello Christian Church (electronic submission)
- Monticello Parks Board (electronic submission)



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.in.gov

White County Highway Department
Mike Kyburz
PO Box 67
Reynolds, IN 47980

Lochmueller Group
Chris Kunkel
3502 Woodview Trace
Suite 150
Indianapolis, IN 46268

Date

To Engineers and Consultants Proposing Roadway Construction Projects:

RE: The Board of White County Commissioners, with funding from the Federal Highway Administration (FHWA) and oversight by the Indiana Department of Transportation (INDOT), intend to proceed with an Intersection Improvement project at the five-legged intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County, Indiana (Des. No. 1702835). The proposed project is located at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. Specifically, the project is located in Section 4, Township 26 North, Range 3 West of Union Township as depicted on the Monticello South U.S. Geological Survey 1:24,000 scale quadrangle. Adjacent land use consists of residential development, commercial properties, as well as recreational and religious facilities. The proposed project will involve the construction of a roundabout at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. The roundabout will consist of one, 18-foot asphalt circulating travel lane and a 12-foot wide concrete truck apron with mountable concrete roll curb. The center of the roundabout will consist of a 115-foot diameter sodded area. The approaches of Gordon Road, S. Main Street, Airport Road, and City Park Loop will all shift generally to the south or southeast and widened nearing the proposed roundabout. The shifting of the north S. Main Street approach could require the relocation of up to 14 concrete bollards currently used to delineate the boundary of the park property. Up to 200 feet of the existing alignment of City Park Loop at its exit to S. Main Street will be realigned to the south. The project will also include updates to the curb and curbside drainage along the affected roadways. Additionally, a section of Oak Drive will be reconstructed to full depth with new curbs and drainage. Oak Drive will maintain its current alignment and will not tie into the roundabout. Within the project limits, excavation to a depth of approximately 2.5 feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection. This work will likely require excavation up to 10 feet below current ground surface. Constructing a roundabout at this intersection is forecasted to bring the LOS of each approach at this intersection to a LOS A during peak hours. The maintenance of traffic (MOT) for this project will occur in phases to ensure motorists have access through the intersection throughout construction. Access will be maintained for property owners during construction of the project. Standard INDOT signage, barricades, and flashing lights will be implemented. The MOT will be implemented per the Indiana Design Manual guidelines. The estimated total length of the project, including all work along the approaches, is 1972 feet (0.37 mile). Construction is anticipated to begin in Fiscal Year (FY) 2023. The existing ROW along Gordon Road extends approximately 48 feet north and 40 feet south of the roadway centerline. The existing ROW along US 421/SR 39 extends from 22 to 40 feet east and from 45 to 68 feet west of the roadway centerline. The existing ROW along Airport Road extends 20 feet east and from 37 to 45 feet west of the roadway centerline. Additional ROW will be required for this project; however, the exact width and area of the proposed ROW is not known at this early stage of the project. It is anticipated that up to approximately 4 acres of permanent ROW could be necessary to complete the project. No temporary ROW is expected to be needed at this time. Development of the ROW will be refined as the project advances. Minor tree clearing may be required within the proposed construction limits.

This letter from the Indiana Department of Environmental Management (IDEM) serves as a standardized response to inquiries inviting IDEM comments on roadway construction, reconstruction, or other improvement projects within existing roadway corridors when the proposed scope of the project is beneath the threshold requiring a formal National Environmental Policy Act-mandated Environmental Assessment or Environmental Impact Statement. As the letter attempts to address all roadway-related environmental topics of potential concern, it is possible that not every topic addressed in the letter will be applicable to your particular roadway project.

For additional information on specific roadway-related topics of interest, please visit the appropriate Web pages cited below, many of which provide contact information for persons within the various program areas who can answer questions not fully addressed in this letter. Also please be mindful that some environmental requirements may be subject to change and so each person intending to include a copy of this letter in their project documentation packet is advised to download the most recently revised version of the letter; found at: <http://www.in.gov/ide/5283.htm> (<http://www.in.gov/ide/5283.htm>).

To ensure that all environmentally-related issues are adequately addressed, IDEM recommends that you read this letter in its entirety, and consider each of the following issues as you move forward with the planning of your proposed roadway construction, reconstruction, or improvement project:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management.

A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/ldem/4396.htm> (<http://www.in.gov/ldem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality Wetlands Program. To learn more about the Wetlands Program, visit: <http://www.in.gov/ldem/4384.htm> (<http://www.in.gov/ldem/4384.htm>).
3. If the USACE determines that a wetland or other water body is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A State Isolated Wetland permit from IDEM's Office of Water Quality (OWQ) is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the OWQ Wetlands Program at 317-233-8488.
4. If your project will involve over a 0.5 acre of wetland impact, stream relocation, or other large-scale alterations to water bodies such as the creation of a dam or a water diversion, you should seek additional input from the OWQ Wetlands Program staff. Consult the Web at: <http://www.in.gov/ldem/4384.htm> (<http://www.in.gov/ldem/4384.htm>) for the appropriate staff contact to further discuss your project.
5. Work within the one-hundred year floodway of a given water body is regulated by the Department of Natural Resources, Division of Water. The Division issues permits for activities regulated under the following statutes:
 - IC 14-26-2 Lakes Preservation Act 312 IAC 11
 - IC 14-26-5 Lowering of Ten Acre Lakes Act No related code
 - IC 14-28-1 Flood Control Act 310 IAC 6-1
 - IC 14-29-1 Navigable Waterways Act 312 IAC 6
 - IC 14-29-3 Sand and Gravel Permits Act 312 IAC 6
 - IC 14-29-4 Construction of Channels Act No related code

For information on these Indiana (statutory) Code and Indiana Administrative Code citations, see the DNR Web site at: <http://www.in.gov/dnr/water/9451.htm> (<http://www.in.gov/dnr/water/9451.htm>). Contact the DNR Division of Water at 317-232-4160 for further information.

The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.

6. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/ldem/4902.htm> (<http://www.in.gov/ldem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/ldem/4917.htm#constreq> (<http://www.in.gov/ldem/4917.htm#constreq>)), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/lac/T03270/A00150>) [PDF] (<http://www.in.gov/legislative/lac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isd/sol/contacts/map.html>) (<http://www.in.gov/isd/sol/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the Implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/ldem/4900.htm> (<http://www.in.gov/ldem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

7. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317/232-4080) for additional project input.
8. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
9. For projects involving effluent discharges to waters of the State of Indiana, contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
10. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed (<http://www.in.gov/dem/4148.htm> (<http://www.in.gov/dem/4148.htm>)) under specific conditions. You also can seek an open burning variance from IDEM.

However, IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on site (you must register with IDEM if more than 2,000 pounds is to be composted; contact 317/232-0088). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) onsite, although burying large quantities of such material can lead to subsidence problems, later on.

Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

Additionally, if construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for 3-5 years precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for 3-5 years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at (317) 233-7272.

2. The U.S. EPA and the Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. (For a county-by-county map of predicted radon levels in Indiana, visit: <http://www.in.gov/dem/4145.htm> (<http://www.in.gov/dem/4145.htm>).)

The U.S. EPA further recommends that all homes (and apartments within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L, or higher, EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L, or higher, EPA recommends the installation of radon-reduction measures. (For a list of qualified radon testers and radon mitigation (or reduction) specialists visit: http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf)). It also is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure visit: <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/dem/4145.htm> (<http://www.in.gov/dem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

3. With respect to asbestos removal: all facilities slated for renovation or demolition (except residential buildings that have (4) four or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

However, in all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at <http://www.in.gov/icpr/webfile/formsdch/44593.pdf> (<http://www.in.gov/icpr/webfile/formsdch/44593.pdf>).

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. All notification remitters will be billed on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/Idem/4983.htm> (<http://www.in.gov/Idem/4983.htm>).

4. With respect to lead-based paint removal: IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal visit: <http://www.in.gov/Idem/19131.htm> (<http://www.in.gov/Idem/19131.htm>).
5. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.al.org/legislative/lac/T03260/A00080.PDF>) (<http://www.al.org/legislative/lac/T03260/A00080.PDF>).
6. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (View at: www.al.org/legislative/lac/T03260/a00020.pdf (<http://www.al.org/legislative/lac/T03260/a00020.pdf>)). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
7. For more information on air permits visit: <http://www.in.gov/Idem/4223.htm> (<http://www.in.gov/Idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or OAMPROD@idem.state.in.us.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/Idem/4998.htm> (<http://www.in.gov/Idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If PCBs are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes (Asbestos removal is addressed above, under Air Quality).
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317/308-3039. See: <http://www.in.gov/Idem/4999.htm> (<http://www.in.gov/Idem/4999.htm>).

FINAL REMARKS

Should you need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that you notify all adjoining property owners and/or occupants within ten days your submittal of each permit application. However, if you are seeking multiple permits, you can still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Should the scope of the proposed project be expanded to the extent that a National Environmental Policy Act Environmental Assessment (EA) or Environmental Impact Statement (EIS) is required, IDEM will actively participate in any early interagency coordination review of the project.

Meanwhile, please note that this letter does not constitute a permit, license, endorsement or any other form of approval on the part of the Indiana Department of Environmental Management regarding any project for which a copy of this letter is used. Also note that it is the responsibility of the project engineer or consultant using this letter to ensure that the most current draft of this document, which is located at <http://www.in.gov/Idem/5284.htm> (<http://www.in.gov/Idem/5284.htm>), is used.

Signature(s) of the Applicant

I acknowledge that the following proposed roadway project will be financed in part, or in whole, by public monies.

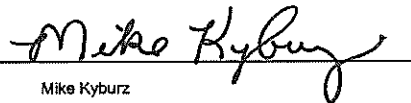
Project Description

The Board of White County Commissioners, with funding from the Federal Highway Administration (FHWA) and oversight by the Indiana Department of Transportation (INDOT), intend to proceed with an intersection improvement project at the five-legged intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County, Indiana (Des. No. 1702835). The proposed project is located at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. Specifically, the project is located in Section 4, Township 26 North, Range 3 West of Union Township as depicted on the Monticello South U.S. Geological Survey 1:24,000 scale quadrangle. Adjacent land use consists of residential development, commercial properties, as well as recreational and religious facilities. The proposed project will involve the construction of a roundabout at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. The roundabout will consist of one, 18-foot asphalt circulating travel lane and a 12-foot wide concrete truck apron with mountable concrete roll curb. The center of the roundabout will consist of a 115-foot diameter sodded area. The approaches of Gordon Road, S. Main Street, Airport Road, and City Park Loop will all shift generally to the south or southeast and widened nearing the proposed roundabout. The shifting of the north S. Main Street approach could require the relocation of up to 14 concrete bollards currently used to delineate the boundary of the park property. Up to 200 feet of the existing alignment of City Park Loop at its exit to S. Main Street will be realigned to the south. The project will also include updates to the curb and curbside drainage along the affected roadways. Additionally, a section of Oak Drive will be reconstructed to full depth with new curbs and drainage. Oak Drive will maintain its current alignment and will not tie into the roundabout. Within the project limits, excavation to a depth of approximately 2.5 feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection. This work will likely require excavation up to 10 feet below current ground surface. Constructing a roundabout at this intersection is forecasted to bring the LOS of each approach at this intersection to a LOS A during peak hours. The maintenance of traffic (MOT) for this project will occur in phases to ensure motorists have access through the intersection throughout construction. Access will be maintained for property owners during construction of the project. Standard INDOT signage, barricades, and flashing lights will be implemented. The MOT will be implemented per the Indiana Design Manual guidelines. The estimated total length of the project, including all work along the approaches, is 1972 feet (0.37 mile). Construction is anticipated to begin in Fiscal Year (FY) 2023. The existing ROW along Gordon Road extends approximately 48 feet north and 40 feet south of the roadway centerline. The existing ROW along US 421/SR 39 extends from 22 to 40 feet east and from 45 to 68 feet west of the roadway centerline. The existing ROW along Airport Road extends 20 feet east and from 37 to 45 feet west of the roadway centerline. Additional ROW will be required for this project; however, the exact width and area of the proposed ROW is not known at this early stage of the project. It is anticipated that up to approximately 4 acres of permanent ROW could be necessary to complete the project. No temporary ROW is expected to be needed at this time. Development of the ROW will be refined as the project advances. Minor tree clearing may be required within the proposed construction limits.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environment that appears directly above. In addition, I understand that in order to complete that project in which I am interested, with a minimum of impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

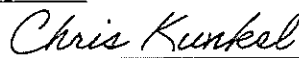
Date: 1-30-2020

Signature of the INDOT
Project Engineer or Other Responsible Agent


Mike Kyburz

Date: 01/30/2020

Signature of the
For Hire Consultant



Chris Kunkel



Organization and Project Information

Project ID:
Des. ID: 1702835
Project Title: Gordon Road Intersection Improvement
Name of Organization: Lochmueller Group
Requested by: Chris Kunkel

Environmental Assessment Report

1. Geological Hazards:

- High liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- Petroleum Exploration Wells

*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

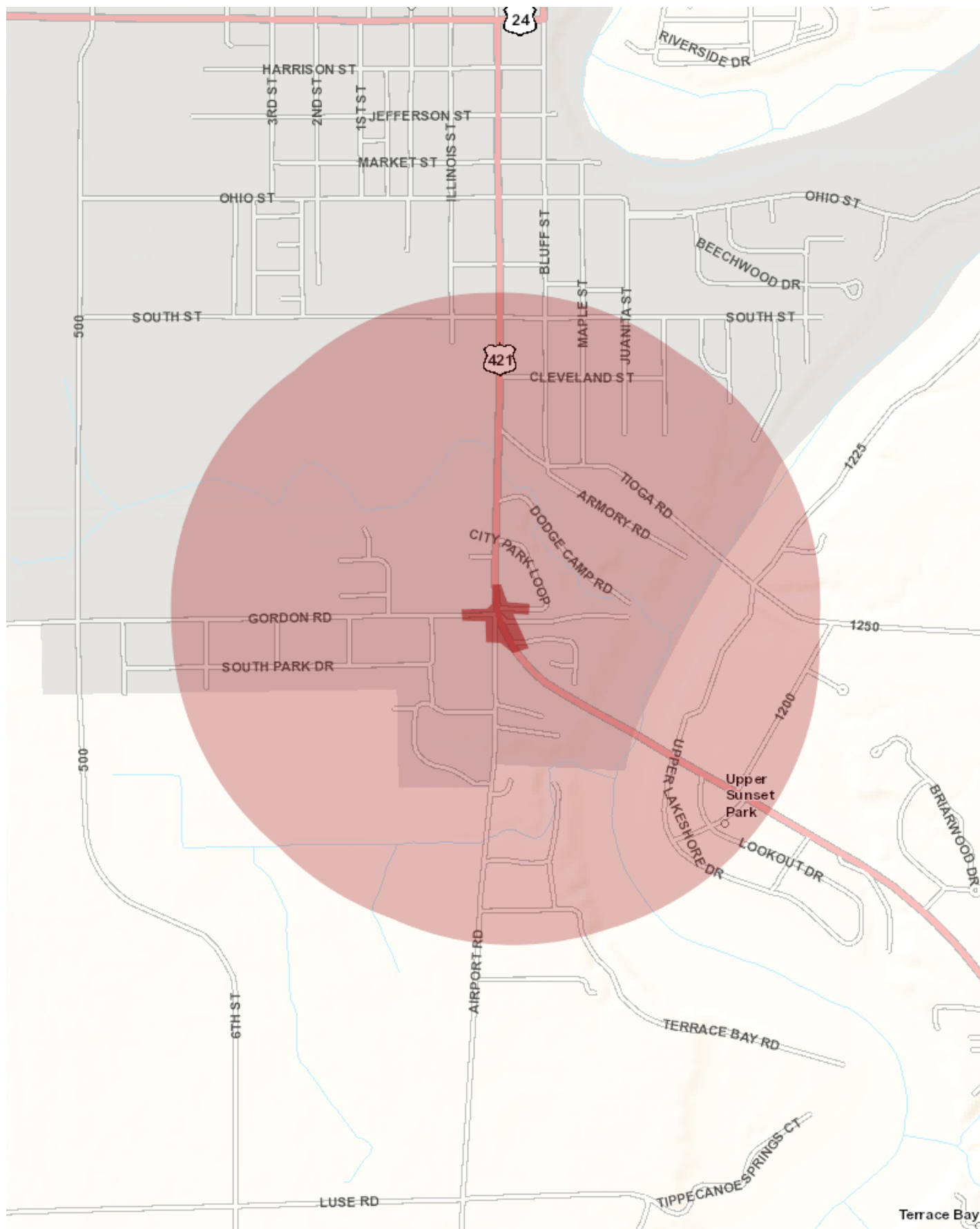
This information was furnished by Indiana Geological Survey

Address: 420 N. Walnut St., Bloomington, IN 47404

Email: IGSEnvir@indiana.edu

Phone: 812 855-7428

Date: January 30, 2020



Chris Kunkel

From: City Fire <fire@monticelloin.gov>
Sent: Monday, February 3, 2020 4:30 PM
To: Kunkel, Chris
Cc: City Mayor; Frank Arthur
Subject: Re: Gordon Road Intersection Improvement (Des. No. 1702835) Early Coordination Letter

Chris,

The largest truck on the Monticello Fire Department requires a minimum of a 75 foot turn radius for tires and a minimum 85 foot turn radius for the body.

Galen Logan, Chief
Monticello Fire Department
911 West South Street
Monticello, IN 47960
574-583-3151-office
574-583-6401-station
glogan@monticelloin.gov

On Thu, Jan 30, 2020 at 1:22 PM Kunkel, Chris <CKunkel@lochgroup.com> wrote:

Good afternoon,

Please see the attached early coordination letter and associated attachments for the intersection improvement project in White County, Indiana.

Please contact myself or Ruth Hook (rhook@lochgroup.com) should you have any questions or comments regarding this project.

Thank you for your time and have a great day,

Chris Kunkel

Environmental Biologist

Lochmueller Group

3502 Woodview Trace, Suite 150, Indianapolis, IN 46268

317.334.6818 (direct) | 317.677.5132 (mobile)

CKunkel@lochgroup.com

WHITE COUNTY AREA PLAN COMMISSION

**Joseph Rogers
Executive Director**

**P.O. Box 851
Monticello, IN 47960**

**Phone: 574/583-7355
Fax: 574/583-1593**

February 4, 2020

**Lochmueller Group
Attn: Chris Kunkel
3502 Woodview Trace, Suite 150
Indianapolis, IN 46268**

**Ref: INDOT Des No.: 1702835; Intersection Improvement Project
Chris Kunkel correspondence dated Jan 30, 2020**

Dear Chris,

I am in receipt of your request for feedback concerning the planned construction activities related to the above referenced project. I have reviewed the documents and have only two concerns related to the proposed road improvement.

- 1) Any right-of-way taking requires submission to our office of an administrative subdivision application for each taking which must include a survey or acceptable engineering drawing providing the legal boundaries of the tract of land to be incorporated into the right-of-way.**
- 2) Based on the right-of-way taking, certain structures such as signs, buildings or other common structures may be made out of compliance by such taking. Should that occur, we would like to have those situations identified so that we may notify the applicable property owners of the creation of these non-conforming circumstances.**

Other than those two issues, I do not see these activities causing a concern for the White County Area Plan Department.

Sincerely,

**Joseph W. Rogers
Executive Director
White County Area Plan**



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (855) 463-6848

Eric Holcomb, Governor
Joe McGuinness, Commissioner

Chris Kunkel
Lochmueller Group, Inc.
(317) 222-3880
ckunkel@lochgroup.com

February 6, 2020

Re: Early Coordination Review, Des. 1702835

S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop – Intersection Improvement Project, Monticello, White County, Indiana

Dear Mr. Kunkel:

The Indiana Department of Transportation (INDOT) Environmental Services Division (ESD) appreciates the opportunity to assist you on the project referenced above. Pursuant to your early coordination request for an environmental review, we have performed a preliminary search of the project area.

There appears to be at least **one** active project you should be aware of that is adjacent to the project area for Des. 1702835. A summary of this project is provided below.

DES: 1600938 – Monticello Sidewalks Project along S. Main Street (US 421) from Tioga Road intersection to approximately 0.08 miles north of the Gordon Road intersection, White County, Indiana

Project Sponsor: Town of Monticello

Project Manager (PM): Jami Erdmann, jerdmann@indot.in.gov, 219-325-7484

Consultant: RQAW Corporation, contact Cameron Fraser, cfraser@rqaw.com, 317-588-1768

Timeline: Letting scheduled for FY 2021

The above referenced project, Des. 1600938, proposes to add sidewalks along S. Main Street (US 421) from the locations stated above. This project states that the new sidewalks along its southern termini will tie into sidewalks built for the roundabout project. However, there is no mention of sidewalks for Des. 1702835 and the draft plans provided did not appear to show sidewalks as part of this project. You mention this sidewalk project in your early coordination letter, but there is no discussion of how it ties in to your project and it is not shown on the graphics provide. INDOT recommends coordination with the Project Sponsor, INDOT PM, or Consulting Firm to correct deficiencies with both projects.

If your project will require the use of state right-of-way please contact the In-House Services Manager at the INDOT LaPorte District Office.

As always, be sure to follow all applicable processes as well as federal and state laws and local requirements. Thank you for the opportunity to assist you with your project. If you have any questions, please contact a member of my staff, Meghan Hinkle: 317-232-1490 or MHinkle@indot.IN.gov.

Sincerely,

Ron Bales
Environmental Policy Manager,
Environmental Services Division

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February 10, 2020

Chris Kunkel
Lochmueller Group, Inc.
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Dear Mr. Kunkel:

The proposed project to make intersection improvements at South Main Street, Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County, Indiana, (Des No 1702835), as referred to in your letter received January 30, 2020, will not cause a conversion of prime farmland.

If you need additional information, please contact John Allen at 317-295-5859.

Sincerely,

JILL REINHART Digitally signed by JILL
REINHART
Date: 2020.02.13 11:46:51 -05'00'

Acting For

JERRY RAYNOR
State Conservationist

Helping People Help the Land.



USDA is an equal opportunity provider, employer and lender.

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-22189

Request Received: January 30, 2020

Requestor: Lochmueller Group Inc
Chris Kunkel
3502 Woodview Trace, Suite 150
Indianapolis, IN 46268

Project: South Main Street (US 421/SR 39) and Airport Road, Gordon Road, and City Park Loop intersection roundabout construction, and Oak Drive roadway reconstruction, Monticello; Des #1702835

County/Site info: White

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of the Tippecanoe River or the unnamed tributary. Please submit more detailed plans to the Division of Water's Technical Services Section if you are unsure whether or not a permit will be required.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: The measures below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas that are not currently mowed and maintained with a mixture of grasses, sedges, and wildflowers native to Central Indiana as soon as possible upon completion; turf-type grasses (including low-endophyte, friendly endophyte, and endophyte free tall fescue but excluding all other varieties of tall fescue) may be used in currently mowed areas only.
2. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
3. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.
4. Seed and protect all disturbed streambanks and slopes not protected by other methods that are 3:1 or steeper with erosion control blankets that are heavy-duty, biodegradable, and net free or that use loose-woven / Leno-woven netting to minimize the entrapment and snaring of small-bodied wildlife such as snakes and turtles (follow manufacturer's recommendations for selection and installation); seed and apply mulch on all other disturbed areas.
5. Plant five trees, at least 2 inches in diameter-at-breast height, for each tree which is removed that is ten inches or greater in diameter-at-breast height.

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife

Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: February 28, 2020

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

June 08, 2021

Consultation Code: 03E12000-2021-SLI-0369

Event Code: 03E12000-2021-E-06655

Project Name: Gordon Road Intersection Improvement Project - Des. No. 1702835

Subject: Updated list of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service’s Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street
Bloomington, IN 47403-2121
(812) 334-4261

Project Summary

Consultation Code: 03E12000-2021-SLI-0369

Event Code: 03E12000-2021-E-06655

Project Name: Gordon Road Intersection Improvement Project - Des. No. 1702835

Project Type: TRANSPORTATION

Project Description: The Board of White County Commissioners, with funding from the Federal Highway Administration (FHWA) and oversight by the Indiana Department of Transportation (INDOT), intend to proceed with an intersection improvement project at the five-legged intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County, Indiana (Des. No. 1702835). The proposed project will involve the construction of a roundabout at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. The roundabout will consist of one, 18-foot asphalt circulating travel lane and a 12-foot wide concrete truck apron with mountable concrete roll curb. The center of the roundabout will consist of a 115-foot diameter sodded area.

The approaches of Gordon Road, S. Main Street, Airport Road, and City Park Loop will all shift generally to the south or southeast and widened nearing the proposed roundabout. The shifting of the north S. Main Street approach could require the relocation of up to 14 concrete bollards currently used to delineate the boundary of the park property. Up to 200 feet of the existing alignment of City Park Loop at its exit to S. Main Street will be realigned to the south. The project will also include updates to the curb and curbside drainage along the affected roadways. Additionally, a section of Oak Drive will be reconstructed to full depth with new curbs and drainage. Oak Drive will maintain its current alignment and will not tie into the roundabout.

Along the east side of the intersection, north of the new roundabout the existing widened shoulder used for public parking will remain. Additionally, as the roadway approaches the new roundabout a 9-inch raised median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

As Airport Road approaches the new roundabout a 9-inch raised concrete median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

The alignment of the existing Gordon Road Trail will also be shifted to the northeast to accommodate the proposed roundabout. This includes removing and replacing the existing ADA-compliant curb ramp at the northwest corner of the intersection and extending the path across S. Main

Street to connect the Gordon Road Trail to Monticello City Park. With the project's realignment of City Park Loop, a portion of the existing roadway pavement of City Park Loop will be repurposed as trail extending into the park.

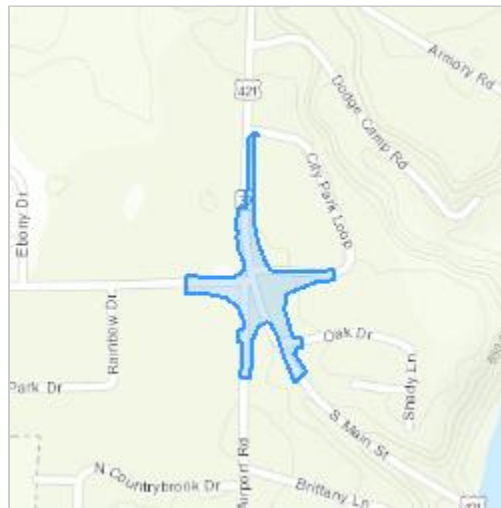
The project will involve the addition of permanent lighting and temporary lighting, although not likely, may be used if night work is required.

Suitable summer bat habitat exists near the project area. It is anticipated that 0.29 acre of suitable habitat within 100 feet of the existing roadway will be cleared as part of this project. The dominant species to be removed include red maple (*Acer rubra*) and black locust (*Robinia pseudoacacia*). Tree clearing is planned to occur in the early spring, outside of the bat active season. Construction is expected to begin in the spring of 2023.

The INDOT LaPorte District reviewed the USFWS Database on March 9, 2021 for the presence of endangered bats or their hibernacula within 0.5 mile of the project area. No bats or their hibernacula were found.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@40.73050539091291,-86.76184798967472,14z>



Counties: White County, Indiana

Endangered Species Act Species

There is a total of 2 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

-
1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/5949	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> ▪ Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045	Threatened

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior

FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

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Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>



In Reply Refer To:

March 17, 2021

Consultation code: 03E12000-2021-I-0369

Event Code: 03E12000-2021-E-04538

Project Name: Gordon Road Intersection Improvement Project - Des. No. 1702835

Subject: Concurrence verification letter for the 'Gordon Road Intersection Improvement Project - Des. No. 1702835' project under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request to verify that the **Gordon Road Intersection Improvement Project - Des. No. 1702835** (Proposed Action) may rely on the concurrence provided in the February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended; 16 U.S.C 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action is within the scope and adheres to the criteria of the PBO, including the adoption of applicable avoidance and minimization measures, and may affect, but is not likely to adversely affect (NLAA) the endangered Indiana bat (*Myotis sodalis*) and/or the threatened Northern long-eared bat (*Myotis septentrionalis*).

The Service has 14 calendar days to notify the lead Federal action agency or designated non-federal representative if we determine that the Proposed Action does not meet the criteria for a NLAA determination under the PBO. If we do not notify the lead Federal action agency or designated non-federal representative within that timeframe, you may proceed with the Proposed Action under the terms of the NLAA concurrence provided in the PBO. This verification period allows Service Field Offices to apply local knowledge to implementation of the PBO, as we may identify a small subset of actions having impacts that were unanticipated. In such instances, Service Field Offices may request additional information that is necessary to verify inclusion of the proposed action under the PBO.

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action is modified, or new information reveals that it may affect the Indiana bat and/or Northern long-eared bat in a manner or to an extent not considered in the PBO, further review to conclude the requirements of ESA Section 7(a)(2) may be required. If the Proposed Action may affect any other federally-listed or proposed species, and/or any designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act may also be required. In either of these circumstances, please contact this Service Office.

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Gordon Road Intersection Improvement Project - Des. No. 1702835

Description

The Board of White County Commissioners, with funding from the Federal Highway Administration (FHWA) and oversight by the Indiana Department of Transportation (INDOT), intend to proceed with an intersection improvement project at the five-legged intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County, Indiana (Des. No. 1702835). The proposed project will involve the construction of a roundabout at the intersection of S. Main Street, Airport Road, Gordon Road, and City Park Loop. The roundabout will consist of one, 18-foot asphalt circulating travel lane and a 12-foot wide concrete truck apron with mountable concrete roll curb. The center of the roundabout will consist of a 115-foot diameter sodded area.

The approaches of Gordon Road, S. Main Street, Airport Road, and City Park Loop will all shift generally to the south or southeast and widened nearing the proposed roundabout. The shifting of the north S. Main Street approach could require the relocation of up to 14 concrete bollards currently used to delineate the boundary of the park property. Up to 200 feet of the existing alignment of City Park Loop at its exit to S. Main Street will be realigned to the south. The project will also include updates to the curb and curbside drainage along the affected roadways. Additionally, a section of Oak Drive will be reconstructed to full depth with new curbs and drainage. Oak Drive will maintain its current alignment and will not tie into the roundabout.

Along the east side of the intersection, north of the new roundabout the existing widened shoulder used for public parking will remain. Additionally, as the roadway approaches the new roundabout a 9-inch raised median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

As Airport Road approaches the new roundabout a 9-inch raised concrete median with a roll curb will be placed between the travel lanes, with a width from 2 feet to 30 feet.

The alignment of the existing Gordon Road Trail will also be shifted to the northeast to accommodate the proposed roundabout. This includes removing and replacing the existing ADA-compliant curb ramp at the northwest corner of the intersection and extending the path across S. Main Street to connect the Gordon Road Trail to Monticello City Park. With the project's realignment of City Park Loop, a portion of the existing roadway pavement of City Park Loop will be repurposed as trail extending into the park.

The project will involve the addition of permanent lighting and temporary lighting, although not likely, may be used if night work is required.

Suitable summer bat habitat exists near the project area. It is anticipated that 0.29 acre of suitable habitat within 100 feet of the existing roadway will be cleared as part of this project. The dominant species to be removed include red maple (*Acer rubra*) and black locust (*Robinia pseudoacacia*). Tree clearing is planned to occur in the early spring, outside of the bat active season. Construction is expected to begin in the spring of 2023.

The INDOT LaPorte District reviewed the USFWS Database on March 9, 2021 for the presence of endangered bats or their hibernacula within 0.5 mile of the project area. No bats or their hibernacula were found.

Determination Key Result

Based on your answers provided, this project(s) may affect, but is not likely to adversely affect the endangered Indiana bat and/or the threatened Northern long-eared bat, therefore, consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required. However, also based on your answers provided, this project may rely on the concurrence provided in the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of a known Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

10. Will the project clear more than 20 acres of suitable habitat per 5-mile section of road/rail?

No

11. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

12. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors?

Yes

14. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented Indiana bat** roosting/foraging habitat or travel corridors occur^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

B) During the inactive season

15. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

16. Will the removal or trimming of habitat or trees occur **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors?

Yes

17. What time of year will the removal or trimming of habitat or trees **within** suitable but **undocumented NLEB** roosting/foraging habitat or travel corridors occur?

B) During the inactive season

18. Will *any* tree trimming or removal occur **within** 100 feet of existing road/rail surfaces?

Yes

19. Will *any* tree trimming or removal occur **between** 100-300 feet of existing road/rail surfaces?

No

20. Are *all* trees that are being removed clearly demarcated?
Yes
21. Will the removal of habitat or the removal/trimming of trees include installing new or replacing existing **permanent** lighting?
No
22. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?
No
23. Does the project include slash pile burning?
No
24. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?
No
25. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)
No
26. Will the project involve the use of **temporary** lighting *during* the active season?
Yes
27. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **temporary** lighting will be used?
Yes
28. Will the project install new or replace existing **permanent** lighting?
Yes
29. Is there *any* suitable habitat **within** 1,000 feet of the location(s) where **permanent** lighting will be installed or replaced?
Yes
30. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?
Yes
31. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

32. Will *any* activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the inactive season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

Yes

33. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge and/or structure activities, temporary or permanent lighting, or use of percussives, limited to actions that DO NOT cause any additional stressors to the bat species?

Examples: lining roadways, unlighted signage , rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

Yes

34. Will the project raise the road profile **above the tree canopy**?

No

35. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the active season within undocumented habitat.

36. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season

37. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the Indiana bat's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

38. Is the habitat removal portion of this project consistent with a Not Likely to Adversely Affect determination in this key?

Automatically answered

Yes, because the tree removal/trimming that occurs outside of the NLEB's active season occurs greater than 0.5 miles from the nearest hibernaculum, is less than 100 feet from the existing road/rail surface, includes clear demarcation of the trees that are to be removed, and does not alter documented roosts and/or surrounding summer habitat within 0.25 miles of a documented roost.

39. **General AMM 1**

Will the project ensure *all* operators, employees, and contractors working in areas of known or presumed bat habitat are aware of *all* FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable Avoidance and Minimization Measures?

Yes

40. **Tree Removal AMM 1**

Can *all* phases/aspects of the project (e.g., temporary work areas, alignments) be modified, to the extent practicable, to avoid tree removal^[1] in excess of what is required to implement the project safely?

Note: Tree Removal AMM 1 is a minimization measure, the full implementation of which may not always be practicable. Projects may still be NLAA as long as Tree Removal AMMs 2, 3, and 4 are implemented and LAA as long as Tree Removal AMMs 3, 5, 6, and 7 are implemented.

[1] The word "trees" as used in the AMMs refers to trees that are suitable habitat for each species within their range. See the USFWS' current summer survey guidance for our latest definitions of suitable habitat.

Yes

41. **Tree Removal AMM 3**

Can tree removal be limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits)?

Yes

42. **Tree Removal AMM 4**

Can the project avoid cutting down/removal of *all* (1) **documented**^[1] Indiana bat or NLEB roosts^[2] (that are still suitable for roosting), (2) trees **within** 0.25 miles of roosts, and (3) documented foraging habitat any time of year?

[1] The word documented means habitat where bats have actually been captured and/or tracked.

[2] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

Yes

43. **Lighting AMM 1**

Will *all* **temporary** lighting be directed away from suitable habitat during the active season?

Yes

44. **Lighting AMM 2**

Does the lead agency use the BUG (Backlight, Uplight, and Glare) system developed by the Illuminating Engineering Society^{[1][2]} to rate the amount of light emitted in unwanted directions?

[1] Refer to [Fundamentals of Lighting - BUG Ratings](#)

[2] Refer to [The BUG System—A New Way To Control Stray Light](#)

Yes

45. **Lighting AMM 2**

Will the **permanent** lighting be designed to be as close to 0 for all three BUG ratings as possible, with a priority of "uplight" of 0 and "backlight" as low as practicable?

Yes

Project Questionnaire

1. Have you made a No Effect determination for *all* other species indicated on the FWS IPaC generated species list?

N/A

2. Have you made a May Affect determination for *any* other species on the FWS IPaC generated species list?

N/A

3. How many acres^[1] of trees are proposed for removal between 0-100 feet of the existing road/rail surface?

[1] If described as number of trees, multiply by 0.09 to convert to acreage and enter that number.

0.29

Avoidance And Minimization Measures (AMMs)

This determination key result includes the commitment to implement the following Avoidance and Minimization Measures (AMMs):

TREE REMOVAL AMM 1

Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal.

LIGHTING AMM 1

Direct temporary lighting away from suitable habitat during the active season.

TREE REMOVAL AMM 2

Apply time of year restrictions for tree removal when bats are not likely to be present, or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and **outside of documented** roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed.

LIGHTING AMM 2

When installing new or replacing existing permanent lights, use downward-facing, full cut-off lens lights (with same intensity or less for replacement lighting); or for those transportation agencies using the BUG system developed by the Illuminating Engineering Society, be as close to 0 for all three ratings with a priority of "uplight" of 0 and "backlight" as low as practicable.

TREE REMOVAL AMM 3

Ensure tree removal is limited to that specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits).

TREE REMOVAL AMM 4

Do not remove **documented** Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or **documented** foraging habitat any time of year.

GENERAL AMM 1

Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs.

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on December 29, 2020. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which may require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

Chris Kunkel

From: Onochie, Stephen <sonochie@idem.IN.gov>
Sent: Wednesday, March 31, 2021 1:37 PM
To: Chris Kunkel
Subject: RE: Potential project impacts from Poncie's Water Sports (AI# 55012)

Chris,

Based on the map you provided showing the project location and data collected during the IDEM investigations, you will likely not encounter any issues with either petroleum or chlorinated solvents from the Poncie Water sports site. Your project location is outside our exposure control area. Please let me know if you have any questions about this email. I am also attaching a link to IDEM document that shows maps and concentration levels for your review. VFC # [82774047](#)

Thanks



Stephen Onochie
Senior Environmental Manager | Petroleum
Remediation Section
Petroleum Branch | Office of Land Quality
Indiana Department of Environmental
Management

(317) 234-3306 | sonochie@idem.IN.gov



From: Chris Kunkel <CKunkel@lochgroup.com>
Sent: Wednesday, March 31, 2021 1:11 PM
To: Onochie, Stephen <sonochie@idem.IN.gov>
Cc: Ruth Hook <RHook@lochgroup.com>
Subject: Potential project impacts from Poncie's Water Sports (AI# 55012)

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Hi Steven,

Like I mentioned in our phone call, we are constructing a roundabout at the five-legged intersection of S Main Street, Gordon Road, Airport Road, and City Park Loop in Monticello (Des. No. 1702835). The project involves roundabout construction, storm sewer updates, and multi-use path construction. The maximum depth of excavation will be 10 feet for the storm sewer updates. I've attached a map to give you an idea where the site is in relation to Poncie's Water Sports.

We are seeking your opinion on whether you think that this hazardous material site will pose any issues to our site from the chlorinated solvent plume that looks to extend toward the project area. If you need any additional information, please let us know.

Chris Kunkel

From: Courtade, Julian <JCourtade@indot.IN.gov>
Sent: Thursday, January 30, 2020 2:16 PM
To: Kunkel, Chris
Subject: RE: Gordon Road Intersection Improvement (Des. No. 1702835) Early Coordination Letter

Chris –

After reviewing the Early Coordination Letter, I have determined that if any object, obstruction, or equipment will exceed 70 ft. in height, further coordination will be required with our office. This is due to the close proximity of White County Airport and the need for any obstructions within 5 miles to meet a 100:1 glideslope to the nearest runway. Please let me know if you have any questions!

Best,

Julian L. Courtade
Chief Airport Inspector
INDOT, Office of Aviation
IGCN Room N955
100 North Senate Avenue
Indianapolis, IN 46204
Office: (317) 232-1477
Email: jcourtade@indot.in.gov



From: Kunkel, Chris [mailto:CKunkel@lochgroup.com]
Sent: Thursday, January 30, 2020 1:11 PM
To: Courtade, Julian <JCourtade@indot.IN.gov>
Subject: Gordon Road Intersection Improvement (Des. No. 1702835) Early Coordination Letter

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Good afternoon,

Please see the attached early coordination letter and associated attachments for the intersection improvement project in White County, Indiana.

Please contact myself or Ruth Hook (rhook@lochgroup.com) should you have any questions or comments regarding this project.

Thank you for your time and have a great day,

Categorical Exclusion

Appendix D

**Section 106 of the National Historic
Preservation Act (NHPA)**

Gordon Road Intersection Project:

Intersection of US 421/SR 39, Airport Road, Gordon Road, and

City Park Loop

City of Monticello, White County, Indiana

Des. No. 1702835

Section 106

800.11(e) Documentation

February 17, 2021



**White County
Federal Highway Administration**



Hannah Blad

Hannah Blad

**FEDERAL HIGHWAY ADMINISTRATION'S
SECTION 4(F) COMPLIANCE REQUIREMENTS (FOR HISTORIC PROPERTIES) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECTS
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
GORDON ROAD INTERSECTION PROJECT
INTERSECTION OF US 421/SR39, AIRPORT ROAD, GORDON ROAD, AND CITY PARK
LOOP
CITY OF MONTICELLO, WHITE COUNTY, INDIANA
DES. NO.: 1702835**

**AREA OF POTENTIAL EFFECTS
(Pursuant to 36 CFR Section 800.4(a)(1))**

The APE for this project encompasses all resources immediately adjacent to the project area and those that may not be immediately adjacent but that have a proximate viewshed of the project area. The project area encompasses the area required to support the purpose and need of the project. On Airport Road the APE extends 550-feet south of the project area, including resources one property back from the road along both sides. On Gordon Road the APE extends approximately 600-feet west of the project area and encompasses resources one property back from the road along both sides. On US 421/SR 39 the APE extends approximately 760-feet north of the project area and encompasses resources one property back from the road along both sides. On US 421/SR 39, the APE extends approximately 450-feet southeast of the project area, stopping just shy of the Tippecanoe River. The eastern side of the APE includes City Loop Drive and Oak Drive, extending between 200-feet and 260-feet to capture all resources within the viewshed. The APE includes all areas where ground disturbance may occur or where auditory or visual impacts might be expected. Please see the APE map in Appendix A, page 3.

**ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR Section 800.4(c)(2))**

There are no properties currently listed in the National Register of Historic Places (National Register) within the APE.

There are two properties recommended eligible for listing in the National Register within the APE:

Monticello City Park (Indiana Historic Sites and Structures Inventory [IHSSI] #181-426-29118). Monticello City Park is located in southern Monticello and has multiple structures and features associated with Works Progress Administration (WPA) construction. Between 1935 and 1938 workers for the WPA built several structures in Monticello City Park in the WPA Rustic architecture style. The Monticello City Park is eligible for listing in the National Register under Criterion A as a local example of a WPA-associated property and Criterion C for its architectural significance.

Monticello Christian Church (Lochmueller #8). The Monticello Christian Church, located at 105 Gordon Road in Monticello, was constructed in 1968 in the Contemporary style. Sparsely ornamented, the structure is dominated by three steeply pitched gable roofs and freestanding tower. The exterior is clad in brick veneer and there are concrete sills on all the lower windows. The central gable features a large, multi-light, rectangular stained-glass window on the north elevation, rising from the lower hipped roof to the gable roof peak. On the south elevation, the central gable has a tall window in the shape of a thin Latin cross. The sanctuary seats face the cross window. The reinforced concrete tower, constructed in a narrow, cruciform

plan, rises in front of the central gable and ends high above the roof peak, where it is crowned by three metal Latin crosses. The Monticello Christian Church is eligible for listing in the National Register under Criterion C for its architectural significance.

EFFECT FINDING

Monticello City Park (IHSSI #181-426-29118) – No Adverse Effect Monticello Christian Church (Lochmueller #8) – No Adverse Effect

INDOT, acting on FHWA’s behalf, has determined a “No Adverse Effect” finding is appropriate for this undertaking.

INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of effect.

SECTION 4(F) COMPLIANCE REQUIREMENTS (for historic properties)

Monticello City Park (IHSSI #181-426-29118) - This undertaking will convert property from the Monticello City Park (IHSSI #181-426-29118), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf has determined the appropriate Section 106 finding is “No Adverse Effect”; therefore FHWA hereby intends to issue a “de minimis” finding for the Monticello City Park, pursuant to SAFETEA-LU, thereby satisfying FHWA’s responsibilities under Section 4(f) for this historic property.

Monticello Christian Church (Lochmueller #8) - This undertaking will convert property from the Monticello Christian Church (Lochmueller #8), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA’s behalf has determined the appropriate Section 106 finding is “No Adverse Effect”; therefore FHWA hereby intends to issue a “de minimis” finding for the Monticello Christian Church, pursuant to SAFETEA-LU, thereby satisfying FHWA’s responsibilities under Section 4(f) for this historic property.

Comments will be accepted for thirty (30) days upon receipt of the findings.

Anuradha V. Kumar

Anuradha V. Kumar, for FHWA
Manager
INDOT Cultural Resources

03/01/2021

Approval Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR 800.5(c)
GORDON ROAD INTERSECTION PROJECT
INTERSECTION OF US 421/SR39, AIRPORT ROAD, GORDON ROAD, AND
CITY PARK LOOP
CITY OF MONTICELLO, WHITE COUNTY, INDIANA
DES. NO.: 1702835**

1. DESCRIPTION OF THE UNDERTAKING

White County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with an intersection project, Des. No. 1702835. The FHWA is providing funding and is the lead federal agency for this Section 106 undertaking. The proposed undertaking is on US 421 at the intersection of Gordon Road in White County, Indiana. It is within Union Township, Monticello South USGS Topographic Quadrangle, in Section 4, Township 26 North, Range 3 West. The entire project is located within the city limits of Monticello. Adjacent land use consists of residential development and commercial properties, as well as recreational and religious facilities. Please see maps and photographs of the project area in Appendices A and B.

The need for the project stems from the poor level of service at the current five road approaches converging at the intersection, the high amount of summer traffic attributed to Indiana Beach, Terrace Bay, Lake Freeman, and Lake Shafer, and the lack of connectivity in pedestrian facilities in that area. The purpose of the project is to improve the safety of the intersection for vehicular traffic, as well as for pedestrians.

The proposed project will involve the construction of a roundabout at the intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multi-use trail east to Monticello City Park and a sidewalk will extend from the south entrance of Monticello City Park north along the east side of US 421/Main Street to the north entrance of the park. The alignment of the trail will be shifted to accommodate the new roundabout. The project will also include updates to the curb and curbside drainage along the roadways, and the addition of Americans with Disabilities Act (ADA) compliant curb ramps at the intersection. Also at the intersection, approximately fourteen 40-foot-high mast lights will be added around the roundabout and the realigned roads entering the roundabout. The depth of excavation for light poles is anticipated to be 8 feet. Excavation to a depth of approximately 2.5-feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection which would involve excavation up to 10-feet below current ground surface. Wayfinding signage will also be included. This project will require the acquisition of approximately 1.61 acres of permanent right-of-way (ROW) and 0.46 acre of temporary ROW.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. The federal involvement is funding from the FHWA. The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties.

The aboveground APE for this project encompasses all resources immediately adjacent to the project area and those that may not be immediately adjacent but that have a proximate viewshed of the project area. The project area encompasses the area required to support the purpose and need of the project. On Airport Road the APE extends 550-feet south of the project area, including resources one property back from the road along both sides. On Gordon Road the APE extends approximately 600-feet west of the project area and encompasses resources one property back from the road along both sides. On US 421/SR 39 the APE extends approximately 760-feet north of the project area and encompasses resources one property back from the road along both sides. On US 421/SR 39, the APE extends approximately 450-feet southeast of the project area, stopping just shy of the Tippecanoe River. The eastern side of the APE includes City Loop Drive and Oak Drive, extending between 200-feet and 260-feet to capture all resources within the viewshed. The APE includes all areas where ground disturbance may occur or where auditory or visual impacts might be expected. Please see Appendix A, page 3, for a map of the aboveground APE.

The archaeological survey area for this project lies entirely within the aboveground APE and is limited to areas where ground disturbance may occur from roundabout, trail, sidewalk, lighting, storm sewer, and related drainage construction. Please see Appendix E, pages 3-5 for a summary of the archaeological report from this investigation.

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The National Register, Indiana Register of Historic Sites and Structures (State Register), the Indiana Historic Sites and Structures Inventory (IHSSI), the State Historic Architectural and Archaeological Research Database (SHAARD), and the Indiana Historic Buildings, Bridges, and Cemeteries Map (IHBBCM) were consulted. No resources within the APE were identified as being listed in the National Register.

The *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (February 2009) by Mead & Hunt was reviewed. No bridges eligible for listing in the NRHP are located within the project area.

Gary Francis Quigg, a Lochmueller Group historian who meets the Secretary of the Interior's Professional Qualification Standards, performed a site inspection of the project area on September 30, 2019 and documented resources that will be at least 50 years of age at the time of the project letting within the APE. The APE was investigated for the existence of any historic properties, structures, objects or districts listed in or eligible for listing in the National Register. The historian walked the APE, taking photographs of all resources meriting a Contributing or higher rating. Non-Contributing resources or those that did not meet the age requirements were noted but not documented other than in general view photographs. Two Contributing previously surveyed resources were documented and twenty-two newly identified aboveground resources with a recommended rating of Contributing or higher were recorded within the APE. An additional five newly identified resources were located outside of the APE but within a subdivision determined ineligible for listing in the National Register. As a result of this field survey, two properties are recommended eligible for the National Register within the APE: Monticello City Park (IHSSI #181-426-29118) and Monticello Christian Church (Lochmueller #8).

Early coordination was initiated on September 12, 2019, with an email to consulting parties. The email asked consulting parties to review the early coordination letter attached to the email and via IN SCOPE, which is INDOT's Section 106 document website <http://erms.indot.in.gov/Section106Documents/>. A hard copy of these materials was mailed to the SHPO on September 12, 2019. Those who were invited to become consulting parties at that time are shown below, with those accepting consulting party status shown in bold below and in Appendix C, pages 1-3.

State Historic Preservation Officer (automatic consulting party)

Kankakee-Iroquois Regional Planning Commission

White County Commissioners

White County Highway Department

Town of Monticello Mayor

Town of Monticello Street Department

White County Historian

White County Historical Society

Indiana Landmarks - Northern Regional Office

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

In a letter dated September 25, 2019, the Pokagon Band of Potawatomi Indians responded to the early coordination letter stating that the project would have, “No Historic Properties in the Area of Potential Effects (APE) significant to the Pokagon Band of Potawatomi Indians.” See Appendix D, page 6 for a copy of the communication.

In a letter dated October 8, 2019, the SHPO staff responded to the early coordination letter stating they did not know of any other parties that should be invited to participate in the Section 106 consultation process. In that same letter, the SHPO staff asked (1) that property owners be invited as soon as possible if right-of-way is planned to be taken from adjacent historic properties and (2) if the entire project will be within the city limits. See Appendix D, page 7-8 for a copy of the communication.

Following the SHPO October 8, 2019 letter, the Monticello Christian Church and the Monticello Parks Department were sent early coordination letters on October 10, 2019. No response was received from either party. See Appendix D, page 9 for a copy of the communication.

No additional comments were received after the distribution of the early coordination materials.

A Phase Ia archaeological reconnaissance survey was conducted by 106 Consulting, LLC (106) in September and October of 2019. The field reconnaissance yielded no newly documented archaeological sites. As such, no further archaeological work was recommended. Please see Appendix E, pages 3-5, for a summary of the Phase Ia archaeological reconnaissance survey.

A Historic Property Report (HPR), based on the results of the September 30, 2019 aboveground field survey, was completed (Blad, June 4, 2020) which recommended the Monticello City Park (IHSSI #181-426-29118) and the Monticello Christian Church (Lochmueller #8) as eligible for listing in the National Register. Please see Appendix E, page 2, for a summary of the HPR.

The HPR and Phase Ia archaeology report were uploaded to IN SCOPE, and an email was sent to consulting parties notifying them of the availability of the reports online on June 4, 2020. Hard copies of these materials were also mailed to the SHPO on that same day. The current owners of the Monticello City Park and the Monticello Christian Church were invited to become consulting parties again at this time as well.

In a letter dated June 4, 2020, the Miami Tribe of Oklahoma responded to the HPR accepting consulting party status and offered “no objection” to the project. Please see Appendix D, page 18 for a copy of the communication.

The SHPO responded to the HPR on June 29, 2020, in a letter concurring with the conclusions in the HPR including the eligibility of Monticello City Park and the Monticello Christian Church, noting that “[t]he proposed historic property boundaries for those two properties seem plausible.”

In a follow up letter regarding the eligibility of the Oak Park Subdivision on July 9, 2020, the SHPO staff clarified the response in their June 29, 2020 letter, stating that they agreed that the subdivision was not eligible for listing in the National Register. Please see Appendix D, pages 19-22 for a copy of the communications.

The owners of the historic properties (Monticello Parks Department and the Monticello Christian Church) have not responded to their consulting party invitations.

No additional comments were received from consulting parties regarding the identification of historic properties.

3. DESCRIBE AFFECTED HISTORIC PROPERTIES

Monticello City Park (IHSSI #181-426-29118). Monticello City Park is located in southern Monticello and has multiple structures and features associated with Works Progress Administration (WPA) construction. Between 1935 and 1938 workers for the WPA built several structures in Monticello City Park in the WPA Rustic architecture style. The Monticello City Park is eligible for listing in the National Register under Criterion A as a local example of a WPA-associated property and Criterion C for its architectural significance.

Monticello Christian Church (Lochmueller #8). The Monticello Christian Church, located at 105 Gordon Road in Monticello, was constructed in 1968 in the Contemporary style. Sparsely ornamented, the structure is dominated by three steeply pitched gable roofs and freestanding tower. The exterior is clad in brick veneer and there are concrete sills on all the lower windows. The central gable features a large, multi-light, rectangular stained-glass window on the north elevation, rising from the lower hipped roof to the gable roof peak. On the south elevation, the central gable has a tall window in the shape of a thin Latin cross. The sanctuary seats face the cross window. The reinforced concrete tower, constructed in a narrow, cruciform plan, rises in front of the central gable and ends high above the roof peak, where it is crowned by three metal Latin crosses. The Monticello Christian Church is eligible for listing in the National Register under Criterion C for its architectural significance.

4. DESCRIBE THE UNDERTAKING’S EFFECT ON HISTORIC PROPERTIES

Monticello City Park (IHSSI #181-426-29118) – No Adverse Effect

The proposed undertaking will encroach upon the recommended National Register boundary for the Monticello City Park. A portion of a roundabout will be constructed in the southwestern corner of the recommended National Register boundary. As a result of the construction of the roundabout, City Park Loop will be reconstructed and realigned within the recommended boundary as well. The new southern end of City Park Loop will meet the roundabout directly south of the recommended National Register boundary within city-owned ROW. From the roundabout, the realignment of City Park Loop will curve to the northeast and connect with the existing loop drive just southeast of the baseball diamond. In addition to the roundabout and road reconstruction, a sidewalk will be constructed between the south fence-line of the baseball diamond and the new alignment of City Park Loop and along the east side of S. Main Street (US 421/SR 39). This sidewalk will extend from a terminus just southeast of the baseball diamond to the north entrance of City Park Loop from S. Main Street. As a result of the new roundabout, twenty-seven concrete,

pyramidal markers located along the perimeter of the park along S. Main Street will be relocated along the inside (park-side) edge of the new proposed sidewalk. The markers have been attributed to the WPA-era of the park anecdotally, since the bulk of park's improvements occurred during that period. However, no documentation has been found to establish their construction by the WPA or when the markers were added to the park boundary. Further, no information was obtained regarding whether or not the current location of the markers is where they were placed originally or has any historic importance. The markers have been replaced throughout the years by the parks department, when they have been damaged by vehicular traffic, using a mold designed to replicate the design. Recently, seventeen markers have been removed by the parks department in the location of the intersection with US 421/Main Street and Gordon Road, in anticipation of the project. The same markers will be placed in new locations near the entrance to the park as part of the project. The markers and greenspace in front of the park are the only features within the boundary for the park that will be impacted by the project. The baseball diamond will not be impacted by the project. See Appendix F, page 4 for the location of where the markers were previously located and where they will be placed as part of the project.

Also, within the west and southwest edges of the recommended National Register boundary, four 40-foot-high-mast lights will be added adjacent to the roundabout. Please see Appendix F, page 16 for the locations of the mast lights. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello City Park in a manner that would diminish its historic integrity.

Monticello Christian Church (Lochmueller #8) – No Adverse Effect

The proposed undertaking will encroach upon the recommended National Register boundary of the Monticello Christian Church. The existing ROW along Gordon Road extends approximately 48 feet north and 40 feet south of the roadway centerline. The recommended National Register boundary ends at the ROW line which extends 17 feet from the edge of pavement. A portion of the construction of the roundabout and the realignment of the Gordon Road approach into the roundabout will take place within the recommended National Register boundary for the Monticello Christian Church. In total, 0.21 acre of permanent ROW and 0.46 acre of temporary ROW will be acquired from the Monticello Christian Church. Traffic on Airport Road will have a one-way signal during construction, and traffic heading north will have to travel west on Gordon Road to access downtown Monticello and SR 39. The temporary ROW is required as part of the Maintenance of Traffic (MOT) plan and provides the necessary clearance to provide a temporary access road between Gordon Road and Airport Road while the roundabout is constructed. Two of the three modern church signs located along Gordon Road, S. Main Street, and Airport Road will not be affected by this project. The sign located at the southwest corner of the intersection of US 421/Main Street and Gordon Road will be relocated as part of the project. This sign is a modern addition to the church property and does not contribute to the architectural significance of the church. This church sign will be relocated within the temporary ROW. In addition, five trees adjacent to this sign will be removed as part of the project. Four out of the five trees to be removed have a diameter at breast height (DBH) of 10 inches or greater, while the fifth tree has a DBH of less than 10 inches. No other existing features within the National Register boundary will be impacted by the encroachment. In addition, three 40-foot high-mast lights will be added near the new roundabout within northwest corner of the recommended National Register boundary. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello Christian Church in a manner that would diminish its historic integrity.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT – INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

According to 36 CFR 800.5(a)(1) "An adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the

National Register in a manner that would diminish the integrity of the property's location, design, setting, materials, workmanship, feeling or association." The following discusses potential effects to the Monticello City Park and Monticello Christian Church. Please see maps and photographs of these resources in Appendices A and B.

Monticello City Park (IHSSI #181-426-29118) -- According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply. The undertaking will alter the existing setting within the property beyond its present condition, but it will not alter Monticello City Park in a manner that would diminish its historic integrity. The addition of a sidewalk along S. Main Street and City Park Loop, the realignment of a portion of City Park Loop, the construction of part of the roundabout and storm sewers, the addition of four 40-foot-high-mast lights, and the relocation of markers will not adversely alter the setting of the property.

Per 800.5(a)(2)(i), the undertaking will not result in the "Physical destruction of or damage to all or part of the property." Although alterations (the relocation of markers, the construction of a sidewalk, storm sewer, lighting, road, and roundabout) will occur within the recommended National Register boundary of the property along S. Main Street, the property will not be destroyed or damaged by this project.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will not cause "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines." The addition of a sidewalk along S. Main Street and City Park Loop, the realignment of a portion of City Park Loop, the relocation of markers, the addition of high-mast lighting, the construction of part of the roundabout and storm sewer installation will alter the property within the recommended National Register boundary of the property along S. Main Street and City Park Loop. However, these alterations will not diminish the integrity of the property. It is anticipated that 0.68 acre (29,768 square feet) of permanent ROW will be acquired from this property for the construction of the sidewalk, roundabout, and City Park Loop.

Per 36 CFR 800.5(a)(2)(iii), the undertaking will not result in the "Removal of the property from its historic location."

Per 36 CFR 800.5(a)(2)(iv), the undertaking will not result in a "Change of the character of the property's uses or of physical features within the property setting that contribute to its historic significance." Changes in the viewshed/streetscape, such as the addition of a sidewalk, lighting, storm sewer and roundabout, the realignment of a portion of City Park Loop, and the relocation of markers, will not change the character of the property's use or the physical features that contribute to its historic significance in this area such that the overall integrity of the property will be diminished. The existing viewshed/streetscape includes modern sidewalks, signage, curbing, ramps, pavement markings, and lighting which were installed outside the period of significance.

Per 36 CFR 800.5(a)(2)(v), the undertaking will not cause the "Introduction of visual, atmospheric or audible elements that diminish the integrity of the property's significant historic features." Changes in the viewshed/streetscape, such as the addition of a sidewalk along S. Main Street and City Park Loop, the realignment of a portion of City Park Loop, the relocation of markers, the addition of high-mast lighting, and the construction of part of the roundabout, will introduce visual elements to the property. The construction of the sidewalk will bring more pedestrian and bicycle traffic by the park. However, these elements will be very similar to existing elements within the existing viewshed, will not be highly visible, and will not diminish the integrity of the significant historic features of the property.

Per 36 CFR 800.5(a)(2)(vi), the undertaking will not result in the "Neglect of a property which causes its deterioration..."

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” Ownership of the property will not change as a result of this project.

Monticello Christian Church (Lochmueller #8) -- According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply. The undertaking will alter the existing setting of the resource beyond its present condition, but it will not alter the Monticello Christian Church in a manner that would diminish its historic integrity. The construction of small portions of the roundabout, and the Gordon Road approach into the roundabout at the northeast corner of the recommended National Register boundary for the church will not adversely alter the setting of the property.

Per 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” Although alterations (high-mast lights, sign and tree removal, construction of a small portion of the roundabout and a small portion of the approach of Gordon Road) will occur within the recommended National Register boundary of the property along Gordon Road and US 421/S. Main Street, the property will not be destroyed or damaged by this project.

Per 36 CFR 800.5(a)2(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.” The addition of lights, the removal of the sign and trees, the construction of small portion of the roundabout and a small portion of the approach of Gordon Road will alter the property within the recommended National Register boundary along Gordon Road and US 421/S. Main Street. However, these alterations will not diminish the integrity of the property. These improvements will require 0.21 acre of permanent ROW and 0.46 acre of temporary ROW be acquired from the Monticello Christian Church.

Per 36 CFR 800.5(a)2(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in a “Change of the character of the property’s uses or of physical features within the property setting that contribute to the historic significance.” Changes in the viewshed/streetscape, including the addition of lights, the removal of the sign and trees, the construction of small portions of the roundabout and the Gordon Road approach will not change the character of the property’s use or the physical features that contribute to its historic significance in this area such that the overall integrity of the property will be diminished. The existing viewshed/streetscape includes modern sidewalks, signage, curbing, ramps, pavement markings, and lighting which were installed outside the period of significance.

Per 36 CFR 800.5(a)2(v), the undertaking will not cause the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Changes in the viewshed/streetscape including the addition of lights, the removal of the sign and trees, the partial construction of a roundabout and approach of Gordon Road within the recommended National Register boundary will introduce visual elements to the property. However, these elements will be very similar to existing elements, will not be highly visible, and will not diminish the integrity of the significant historic features of the property.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)(2)(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” Ownership of the historic resource will not change as a result of this project.

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

As noted above, early coordination was initiated on September 12, 2019. All consulting parties received the early coordination materials via email and in addition, the SHPO was mailed a hard copy of the materials. Following the SHPO October 8, 2019 letter, the Monticello Christian Church and the Monticello Parks Department were sent early coordination letters on October 10, 2019. Monticello Christian Church and Monticello Parks Department were invited to become consulting parties again with the distribution of the HPR. The complete list of those who agreed to be consulting parties is shown in bold below and in Appendix C, pages 1-3.

- **State Historic Preservation Officer (automatic consulting party)**
- Kankakee-Iroquois Regional Planning Commission
- White County Commissioners
- White County Highway Department
- Town of Monticello Mayor
- Town of Monticello Street Department
- White County Historian
- White County Historical Society
- Indiana Landmarks - Northern Regional Office
- Monticello City Park
- Monticello Christian Church
- Absentee Shawnee Tribe of Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- **Miami Tribe of Oklahoma**
- Peoria Tribe of Indians of Oklahoma
- **Pokagon Band of Potawatomi Indians**

The following is a summary of the comments of the consulting parties following the distribution of the early coordination materials and HPR (September 2019 through November 2020). These comments have been previously presented in detail above in “Section 2. Efforts to Identify Historic Properties” and the correspondence may be viewed in Appendix D, pages 1-22:

- September 25, 2019: A letter from the Pokagon Band of Potawatomi Indians responded to the early coordination letter accepting consulting party status.
- October 8, 2019: A letter from SHPO stated that they were unaware of any additional consulting parties that should be invited to participate in the Section 106 process but that if ROW is to be taken from the historic properties their owners should be invited as soon as possible.
- October 10, 2019: The Monticello Parks Department and the Monticello Christian Church were invited to be consulting parties.
- June 4, 2020: The Miami Tribe of Oklahoma responded to the HPR, offered no objection to the project, and accepted consulting party status.

- June 29, 2020: A letter from SHPO concurred with the conclusions from the HPR including the APE and eligibility of the Monticello City Park and the Monticello Christian Church. In the same letter, the SHPO concurred with the findings from the Phase 1a.
- July 9, 2020: A letter from SHPO clarifies that the Oak Park Subdivision is not eligible for the National Register and the only two eligible properties within the APE are the Monticello City Park and the Monticello Christian Church.

On December 17, 2020, a preliminary effects letter recommending a finding of “No Adverse Effect” was uploaded to IN SCOPE and an email was sent to consulting parties notifying them of the letter. A hard copy of the letter was mailed to SHPO on that same day. Please see Appendix D, pages 23-28 for a copy of the letter.

On January 19, 2021, the SHPO responded to the preliminary effects letter stating, “[w]e agree with the December 17 effects analysis letter that the Monticello City Park (IHSSI #181-426-29118) which is eligible for inclusion in National Register of Historic Places (“NRHP”) under Criterion A, and the Monticello Christian Church (Lochmueller #8) which is eligible for inclusion in the NRHP under Criterion C do not appear to be adversely affected by this project. We do not believe the characteristics that qualify the identified historic properties for inclusion in the NRHP will be diminished as a result of this project.” See Appendix D, pages 29-30 for a copy of the correspondence.

No other consulting party comments were received. No consulting parties expressed an interest in participating in a consulting party meeting.

A public notice will be published in the *Monticello Herald Journal* newspaper seeking the views of the public regarding the effects of the proposed project on the historic elements within the APE. Comments from the public will be accepted for 30 days following the publication of the notice. If any substantive comments are received during this period, this document will be revised to include them.

APPENDICES

A – Maps	Some removed to avoid duplication
B – General Photographs	
C – Consulting Parties List	
D – Consulting Parties Correspondence	
E – Historic Property Report Summary and Archaeological Report Summary	
F – Stage 2 Plans	Removed to avoid duplication

Section 106 800.11(e)

Appendix A

Maps

Location Map and Topo Map
removed to avoid duplication;
see Appendix B



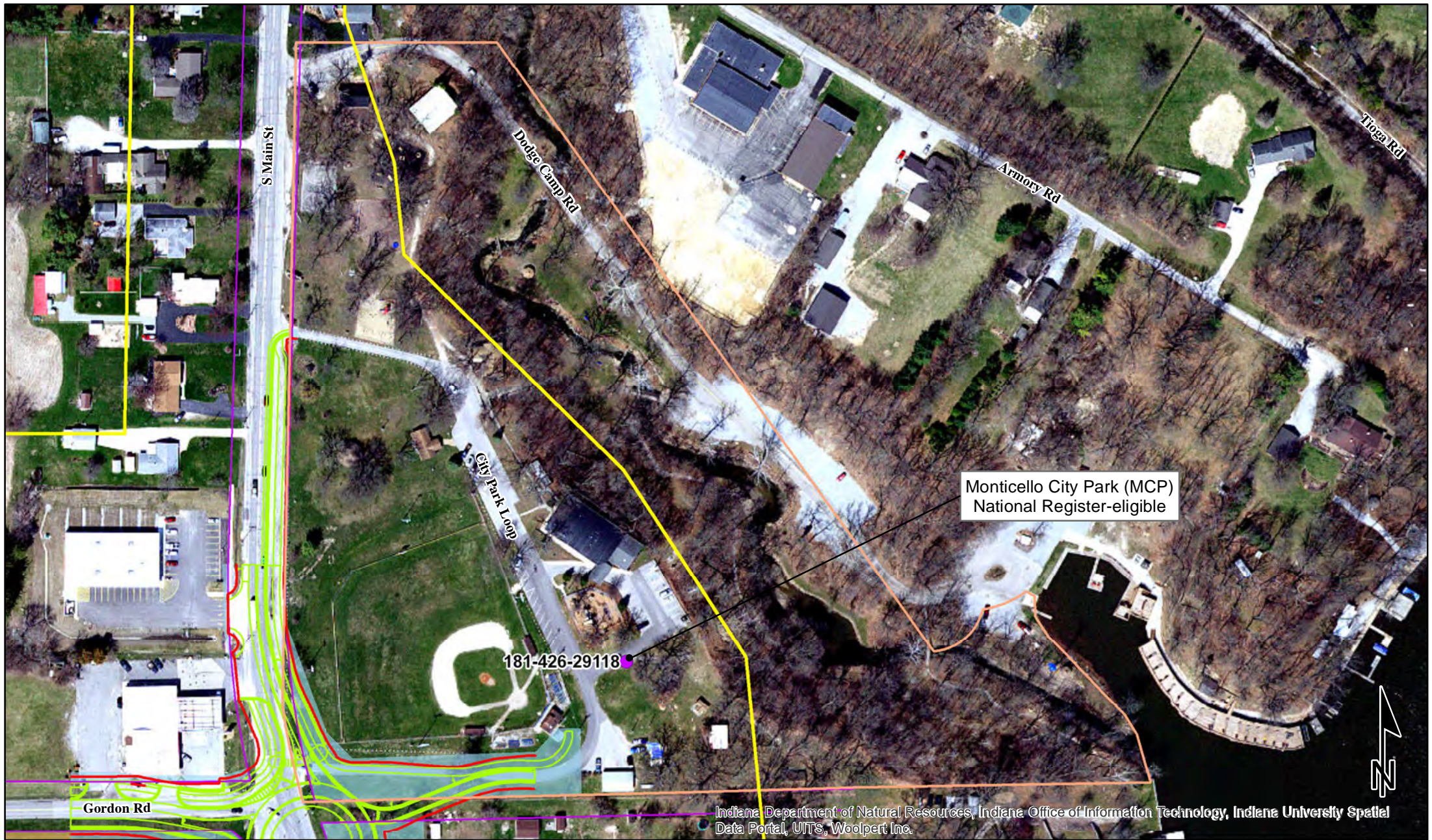
Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Des. No. 1702835
 Gordon Road Intersection Project
 Intersection of US 421/SR 39, Airport Road, Gordon Road,
 and City Park Loop
 City of Monticello, White County, Indiana

Area of Potential Effects Map

 APE	★ National Register Sites	County Survey Sites	Historic Bridges
 MCC Recommended Boundary	 Historic Districts	RATING	RATING
 MCP Recommended Boundary	 Cemeteries	● Outstanding	■ Outstanding
 Construction Limits		● Notable	■ Notable
 Existing ROW		● Contributing	■ Contributing
 Proposed Design		● Non-Contributing	■ Non-Contributing
 Proposed Temporary ROW		● Demolished	■ Demolished
 Proposed Permanent ROW		● Unknown	■ Unknown



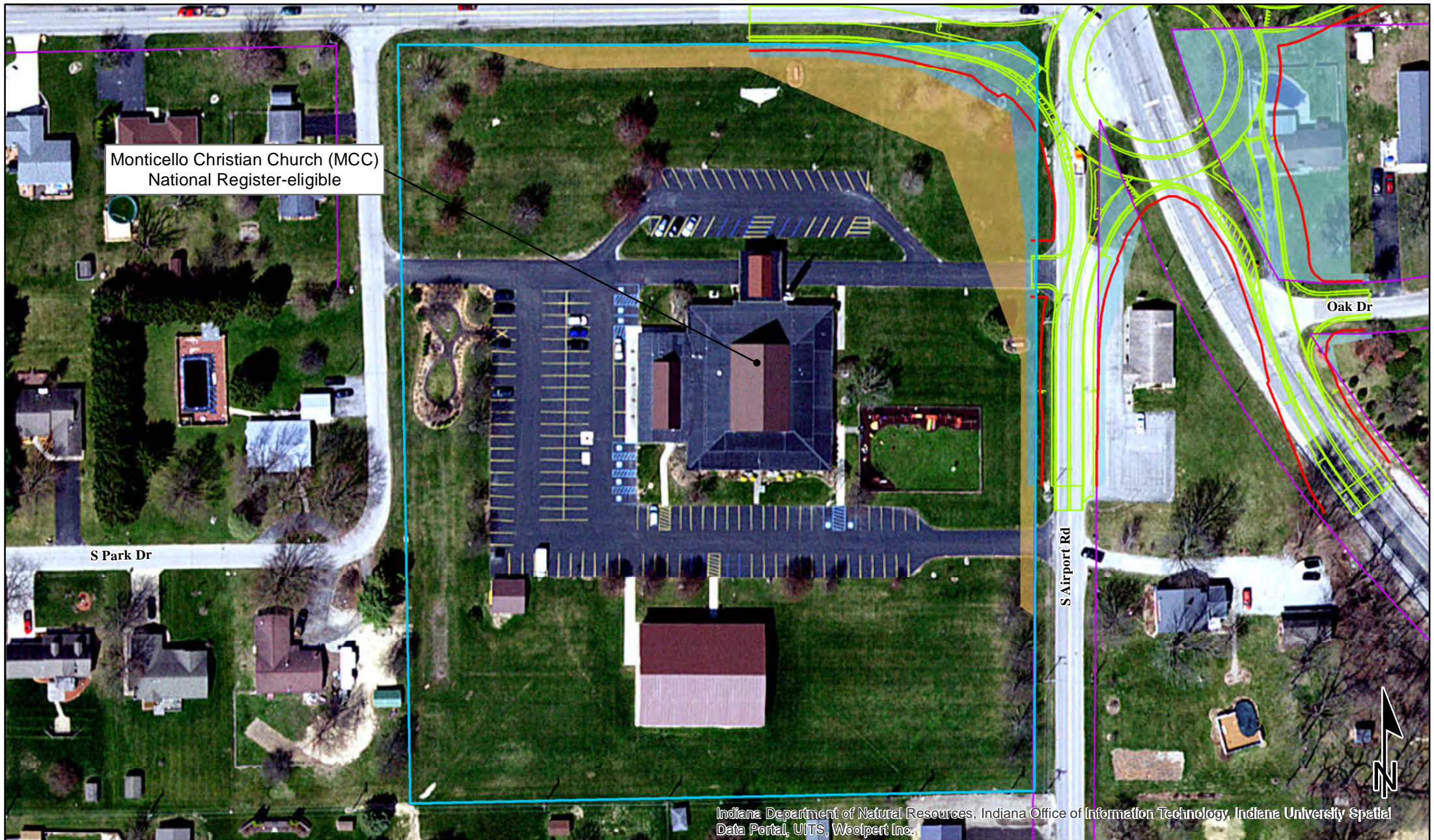
Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Des. No. 1702835
 Gordon Road Intersection Project
 Intersection of US 421/SR 39, Airport Road, Gordon Road,
 and City Park Loop
 City of Monticello, White County, Indiana

**Monticello City Park
Aerial Map**

APE	National Register Sites	County Survey Sites	Historic Bridges
MCP Recommended Boundary	Historic Districts	RATING	RATING
Construction Limits	Cemeteries	Outstanding	Outstanding
Existing ROW		Notable	Notable
Proposed Design		Contributing	Contributing
Proposed Temporary ROW		Non-Contributing	Non-Contributing
Proposed Permanent ROW		Demolished	Demolished
		Unknown	Unknown



Sources:
Non Orthophotography
 Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Des. No. 1702835
 Gordon Road Intersection Project
 Intersection of US 421/SR 39, Airport Road, Gordon Road,
 and City Park Loop
 City of Monticello, White County, Indiana

**Monticello Christian Church
 Aerial Map**

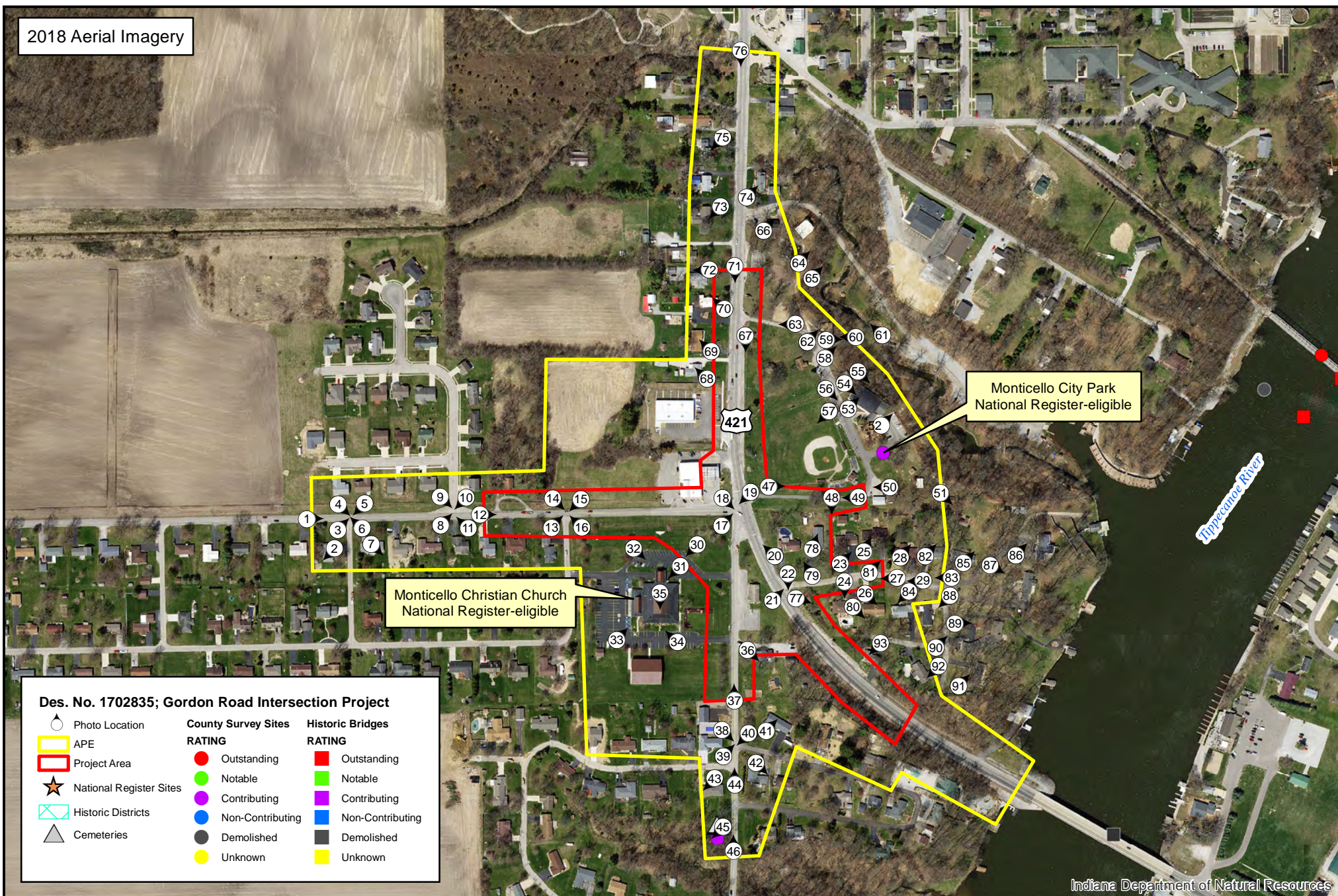
MCC Recommended Boundary	National Register Sites	County Survey Sites	Historic Bridges
Construction Limits	Historic Districts	RATING	RATING
Existing ROW	Cemeteries	Outstanding	Outstanding
Proposed Design		Notable	Notable
Proposed Temporary ROW		Contributing	Contributing
Proposed Permanent ROW		Non-Contributing	Non-Contributing
		Demolished	Demolished
		Unknown	Unknown

Section 106 800.11(e)

Appendix B

General Photographs

2018 Aerial Imagery



Indiana Department of Natural Resources

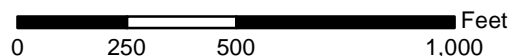
Des. No. 1702835; Gordon Road Intersection Project

- | | | |
|-------------------------|----------------------------|-------------------------|
| Photo Location | County Survey Sites | Historic Bridges |
| APE | RATING | RATING |
| Project Area | Outstanding | Outstanding |
| National Register Sites | Notable | Notable |
| Historic Districts | Contributing | Contributing |
| Cemeteries | Non-Contributing | Non-Contributing |
| | Demolished | Demolished |
| | Unknown | Unknown |



3502 Woodview Trace, Suite 150
Indianapolis, IN 46268
Phone: (317) 222-3880
Fax: (317) 222-3881

Photo Location Map



Location: US 421
County: White
Township: Union
State: Indiana



Date: 3/20/2020



1. Looking E on Gordon Rd from W edge of APE between Royalwood Lane and Spring Drive



2. Looking SW at Lochmueller #1, Contributing, 1000 Spring Drive



3. Looking NE at the intersection of Gordon Road and Spring Drive



4. Looking SE at the intersection of Gordon Road and Spring Drive



5. Looking SW at the intersection of Gordon Road and Spring Drive



6. Looking NW at the intersection of Gordon Road and Spring Drive



7. Looking SSE at Lochmueller #2, Contributing, 213 Gordon Road



8. Looking NE at the intersection of Gordon Road and Ebony Drive



9. Looking SE at the intersection of Gordon Road and Ebony Drive



10. Looking SW at the intersection of Gordon Road and Ebony Drive



11. Looking NW at the intersection of Gordon Road and Ebony Drive



12. Looking E at W project limits on Gordon Road



13. Looking NE at the intersection of Gordon Road and Rainbow Drive



14. Looking SE at the intersection of Gordon Road and Rainbow Drive



15. Looking SW at the intersection of Gordon Road and Rainbow Drive



16. Looking NW at the intersection of Gordon Road and Rainbow Drive



17. Looking NE at the intersection of Gordon Road, Airport Road, and US 421/SR 39



18. Looking SE at the intersection of Gordon Road, Airport Road, and US 421/SR 39



19. Looking SW at the intersection of Gordon Road, Airport Road, and US 421/SR 39



20. Looking NW at the intersection of Gordon Road, Airport Road, and US 421/SR 39



21. Looking NE at Oak Park Subdivision at the intersection of US 421/SR 39 and Oak Drive



22. Looking S at the intersection of US 421/SR 39 and Oak Drive



23. Looking NNE in Oak Park Subdivision at Lochmueller #3, Contributing, 104 Oak Drive



24. Looking S in Oak Park Subdivision at Lochmueller #4, Contributing, 105 Oak Drive



25. Looking NNE in Oak Park Subdivision at Lochmueller #5, Contributing, 200 Oak Drive



26. Looking SE in Oak Park Subdivision at Lochmueller #6, Contributing, 201 Oak Drive



27. Looking W from E end of project limits on Oak Drive



28. Looking NNE in Oak Park Subdivision at Lochmueller #7, Contributing, 202 Oak Drive



29. Looking W in Oak Park Subdivision from E end of APE on Oak Drive



30. Looking SW at Lochmueller #8, Monticello Christian Church, Outstanding,
105 Gordon Road, National Register-eligible



31. Looking SW at Lochmueller #8, Monticello Christian Church, Outstanding,
105 Gordon Road, National Register-eligible



32. Looking SE at Lochmueller #8, Monticello Christian Church, Outstanding,
105 Gordon Road, National Register-eligible



33. Looking NE at Lochmueller #8, Monticello Christian Church, Outstanding,
105 Gordon Road, National Register-eligible



34. Looking NNW at Lochmueller #8, Monticello Christian Church, Outstanding,
105 Gordon Road, National Register-eligible



35. Looking S inside sanctuary of at Lochmueller #8, Monticello Christian Church, Outstanding, 105 Gordon Road, National Register-eligible



36. Looking SSE at Lochmueller #9, Contributing, 1105 S. Airport Road



37. Looking N from S end of project limits on S. Airport Road



38. Looking SE at the intersection of Airport Road and Brittany Lane



39. Looking NE at the intersection of Airport Road and Brittany Lane



40. Looking SW at the intersection of Airport Road and Brittany Lane and the intersection of Airport Rd and Countrybrook Drive



41. Looking NE at Lochmueller #10, Contributing, 102 Brittany Lane



42. Looking SE at Lochmueller #11, Contributing, 107 Brittany Lane



43. Looking SW at Lochmueller #12, Contributing, 102 Countrybrook Drive



44. Looking N at the intersection of S. Airport Road and Countrybrook Drive and the intersection of S. Airport Road and Brittany Lane



45. Looking SW at IHSSI #181-426-25030/CR-91-70, Rothrock II/Britton Cemetery, Contributing



46. Looking N at S end of APE on S. Airport Road



47. Looking E at row of concrete pyramidal roadway markers/bollards at S boundary of Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



48. Looking S at project limit line that extends into Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



49. Looking W from E edge of project limits in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



50. Looking W from E edge of APE in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



51. Looking SSE at WPA-built shelter in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



52. Looking NE at WPA-built activity center, Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



53. Looking NE at W elevation window on the WPA-built activity center in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



54. Looking SE at carved limestone plaque on WPA-built activity center in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



55. Looking SE at louvered, arched vents on WPA-built activity center in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



56. Looking SE at WPA-built activities center in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



57. Looking SW from the E edge of APE at WPA-built baseball diamond in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



58. Looking NE at WPA-built balustrade, piers, and retaining wall at entrance to stone stairway in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



59. Looking E at WPA-built stone stairway in Monticello City Park Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



60. Looking W at WPA-built stone stairway in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



61. Looking NW at WPA-built concrete and stone dams on small creek in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



62. Looking NE at WPA built central shelter in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



63. Looking W along City Park Loop showing concrete pyramidal roadway markers/bollards in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



64. Looking NE at WPA-built stone drinking water fountain in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



65. Looking NE at WPA-built overlook structure in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



66. Looking NE at WPA-built shelter in Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



67. Looking S along W boundary of Monticello City Park showing concrete pyramidal roadway markers/ bollards, Monticello City Park, IHSSI #181-426-29118, Contributing, National Register-eligible



68. Looking NW at Lochmueller #13, Contributing, 906 Gordon Road



69. Looking NW at Lochmueller #14, Contributing, 904 S. Main Street



70. Looking NW at Lochmueller #15, Contributing, 902 S. Main Street



71. Looking S from N project limits on US 421/SR 39



72. Looking W at Lochmueller #16, Contributing, 900 S. Main Street



73. Looking SW at Lochmueller #17, Contributing, 810 S. Main Street



74. Looking NE at Lochmueller #18, Contributing, 809 S. Main Street



75. Looking SW at Lochmueller #19, Contributing, 806 S. Main Street



76. Looking S from N end of the APE on US 421/SR 39



77. Looking ESE at Oak Park Subdivision sign



78. Looking NNE in Oak Park Subdivision at 102 Oak Drive, Non-Contributing



79. Looking NNW in Oak Park Subdivision at location of c. 1954 house demolition



80. Looking SE in Oak Park Subdivision at Lochmueller #4, Contributing, 105 Oak Drive



81. Looking NE in Oak Park Subdivision at limestone mailbox post at 200 Oak Drive



82. Looking NE in Oak Park Subdivision at Lochmueller #20, Contributing, 204 Oak Drive



83. Looking W in Oak Park Subdivision, streetscape view along Oak Drive



84. Looking SW in Oak Park Subdivision at Lochmueller #21, Contributing, 203 Oak Drive



85. Looking NE in Oak Park Subdivision at Lochmueller #22, Contributing, 206 Oak Drive; outside APE



86. Looking NE in Oak Park Subdivision at Lochmueller #23, Contributing, 304 Oak Drive; outside APE



87. Looking ESE in Oak Park Subdivision at Lochmueller #24, Contributing, 303 Oak Drive; outside APE



88. Looking SW in Oak Park Subdivision at Lochmueller #25, Contributing, 1006 Shady Lane; outside APE



89. Looking NE in Oak Park Subdivision at 1011 Shady Lane, Non-Contributing; outside APE



90. Looking NE in Oak Park Subdivision, streetscape showing large setbacks on E side of Shady Lane



91. Looking NE in Oak Park Subdivision at Lochmueller #26, Contributing, 1013 Shady Lane; outside APE



92. Looking SW in Oak Park Subdivision at 205 Shady Hill Court, Non-Contributing



93. Looking SW in Oak Park Subdivision at Lochmueller #27, Contributing, 203 Shady Hill Court

Section 106 800.11(e)

Appendix C

Consulting Parties List

Consulting Party List
Des. No. 1702835
Gordon Road Intersection Improvement Project
Intersection of US 421 and CR 1000 W
City of Monticello, White County, Indiana

Automatic Section 106 Consulting Party:

**Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology
Chad Slider, State Historic Preservation Officer
402 W. Washington St., Room W274
Indianapolis, Indiana 46204
cslider@dnr.in.gov**

Invited Consulting Parties:

Kankakee-Iroquois Regional Planning Commission
115 East Fourth Street
P.O. Box 127
Monon, IN 47959-0127
elb@kirpc.net

White County Commissioner
Steve Burton
P.O. Box 260
110 North Main Street
Monticello, IN 47960
wcsteveburton@gmail.com

White County Commissioner
John Heimlich
P.O. Box 260
110 North Main Street
Monticello, IN 47960
wcjohnheimlich@gmail.com

White County Commissioner
David Diener
P.O. Box 260
110 North Main Street
Monticello, IN 47960
dadiener@hotmail.com

White County Highway Department
Mike Kyburz, Superintendent
P.O. Box 67
Reynolds, IN 47980
mkyburz@whitecountyindiana.us

*Participating Consulting Parties in **BOLD**

Consulting Party List
Des. No. 1702835
Gordon Road Intersection Improvement Project
Intersection of US 421 and CR 1000 W
City of Monticello, White County, Indiana

Invited Consulting Parties (Continued):

Town of Monticello Mayor
Ken Houston
City Hall
227 N. Main Street
Monticello, IN 47960
mayor@monticelloin.gov

Town of Monticello Street Department
Frank Arthur, Superintendent
515 N. Railroad Street
Monticello, Indiana 47960
streets@monticelloin.gov

White County Historian
Judith C. Baker-Elliott
706 E. Ohio Street
Monticello, IN 47960-2367
judybaker42@gmail.com

White County Historical Society
Judith C. Baker, Director
101 S. Bluff Street
Monticello, IN 47960-2308
museum@lightstreamin.com

Indiana Landmarks – Northern Regional Office
Todd Zeiger, Director
801 West Washington Street
South Bend, IN 46601
tzeiger@indianalandmarks.org

Monticello Christian Church
105 Gordon Road
Monticello, IN 47960
deb@mccwired.com & amy@mccwired.com

Monticello Parks Department
904 City Park Loop
Monticello, IN 47960
parks@monticelloin.gov

*Participating Consulting Parties in **BOLD**

Consulting Party List

Des. No. 1702835

Gordon Road Intersection Improvement Project

Intersection of US 421 and CR 1000 W

City of Monticello, White County, Indiana

Invited Consulting Parties (Continued):

Absentee Shawnee Tribe of Oklahoma

Eastern Shawnee Tribe of Oklahoma

Miami Tribe of Oklahoma

Peoria Tribe of Indians of Oklahoma

Pokagon Band of Potawatomi Indians

*Participating Consulting Parties in **BOLD**

Section 106 800.11(e)

Appendix D

Consulting Parties Correspondence

Blad, Hannah

From: Blad, Hannah
Sent: Thursday, September 12, 2019 8:53 AM
To: cslider@dnr.in.gov; elb@kirpc.met; wcsteveburton@gmail.com; wcjohnnheimlich@gmail.com; dadiener@hotmail.com; mkyburz@whitecountyindiana.us; mayor@monticelloin.gov; streets@monticello.gov; judybaker42@gmail.com; wcmuseum@centurylink.net; tzeiger@indianalandmarks.org
Cc: Ross, Anthony; Miller, Shaun (INDOT); SBranigin (SBranigin@indot.IN.gov); Costa, Chad; Quigg, Gary; Hawley, Jessica; Fleming, Steven; jerdman@indot.in.gov
Subject: FHWA Project: Des. No. 1702835; Gordon Road Intersection Improvement Project, White County, Indiana
Attachments: Gordon Road Intersection Project_Des1702835_Early Coordination_2019-09-12.pdf

Des. No.: 1702835

Project Description: Intersection improvement

Location: Intersection of US 421/SR 39 and Gordon Road, Union Township, Town of Monticello, Indiana

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project (Des. No. 1702835).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer, Kankakee-Iroquois Regional Planning Commission, White County Commissioners, White County Highway Department, Town of Monticello Mayor, Town of Monticello Street Department, White County Historian, White County Historical Society, Indiana Landmarks – Northern Regional Office, Absentee Shawnee Tribe of Oklahoma, Eastern Shawnee Tribe of Oklahoma, Miami Tribe of Oklahoma, Peoria Tribe of Indians of Oklahoma, Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Blad, Hannah

From: Ross, Anthony <ARoss3@indot.IN.gov>
Sent: Thursday, September 12, 2019 9:42 AM
To: 'dhunter@miamination.com'; thpo@estoo.net; lpappenfort@peoriatribes.com; Matthew Bussler (Matthew.Bussler@pokagonband-nsn.gov); snease@astribes.com
Cc: Blad, Hannah; Miller, Shaun (INDOT); Branigin, Susan; Costa, Chad; Quigg, Gary; Allen, Michelle (FHWA)
Subject: FHWA Project: Des. No. 1702835; Gordon Road Intersection Improvement Project, White County, Indiana
Attachments: We found suspicious links

Des. No.: 1702835

Project Description: Intersection improvement

Location: Intersection of US 421/SR 39 and Gordon Road, Union Township, Town of Monticello, Indiana

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project (Des. No. 1702835).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer, Kankakee-Iroquois Regional Planning Commission, White County Commissioners, White County Highway Department, Town of Monticello Mayor, Town of Monticello Street Department, White County Historian, White County Historical Society, Indiana Landmarks – Northern Regional Office, Absentee Shawnee Tribe of Oklahoma, Eastern Shawnee Tribe of Oklahoma, Miami Tribe of Oklahoma, Peoria Tribe of Indians of Oklahoma, Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

September 12, 2019

This letter was sent to the listed parties.

RE: Gordon Road Intersection Project
Intersection of US 421/SR 39, Airport Road, and Gordon Road
Town of Monticello, White County, Indiana
Des. No. 1702835

Dear Consulting Party (see attached list),

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with an intersection project (Des. No. 1702835). Lochmueller Group is under contract with White County to advance the preliminary engineering and environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on US 421 at the intersection of Gordon Road in White County, Indiana. It is within Union Township, USGS Topographic Quadrangle, in Section 4, Township 26 North, Range 3 West.

The need for the project stems from the poor level of service at the current five road approaches converging at the intersection, the high amount of summer traffic attributed to Indiana Beach, Terrace Bay, Lake Freeman, and Lake Shader, and the lack of connectivity in pedestrian facilities in that area. The purpose of the project is to improve the safety of the intersection for vehicular traffic, as well as for pedestrians.

The project will involve the construction of a roundabout at the intersection of Gordon Road, US 421/SR 39, Airport Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Also, the project will build another portion of the Gordon Road multiuse trail that will extend from near the intersection of Ebony Drive and Gordon Road to Monticello City Park. The project will require the acquisition of approximately 4 acres of additional permanent right-of-way (ROW). Excavation will be required to replace the existing pavement which will be to a depth of approximately 2.5 feet. Additionally, storm sewer work may also be required. The excavation required for this work may go to a depth of up to 10 feet. Wayfinding signage will also be included with this project. Design is still in the early stages and more details will be made available as design progresses.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

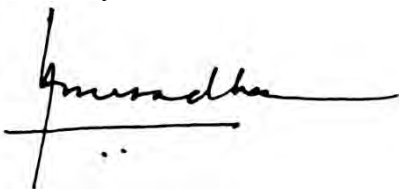
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Hannah Blad of Lochmueller Group at 317.334.6826 or hblad@lochgroup.com. All future responses regarding the proposed project should be forwarded to Lochmueller Group at the following address:

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, IN, 46268
hblad@lochgroup.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office

Environmental Services

Enclosures:

- USGS Topographic Map

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Kankakee-Iroquois Regional Planning Commission
- White County Commissioners
- White County Highway Department
- Town of Monticello Mayor
- Town of Monticello Street Department
- White County Historian
- White County Historical Society
- Indiana Landmarks - Northern Regional Office
- Absentee Shawnee Tribe of Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians



Pokégnek Bodéwadmik • Pokagon Band of Potawatomi
Department of Language and Culture

59291 Indian Lake Road • Dowagiac, MI 49047 • www.PokagonBand-nsn.gov
(269) 462-4316 • (269) 782-2499 fax

9/25/2019

Shaun Miller
INDOT
Phone: 317-233-6795
Email: SMiller@indot.in.gov

**FHWA Project: Des. No. 1702835; Gordon Road Intersection Improvement
Project, White County, Indiana**

Dear Responsible Party:

Migweth for contacting me regarding these projects. As THPO, I am responsible for handling Section 106 Consultations on behalf of the tribe. I am writing to inform you that after reviewing the details for the project referenced above, I have made the determination that there will be **No Historic Properties in Area of Potential Effects (APE)** significant to the Pokagon Band of Potawatomi Indians. However, if any archaeological resources are uncovered during this undertaking, please stop work and contact me immediately. Should you have any other questions, please don't hesitate to contact me at your earliest convenience.

Sincerely,

Matthew J.N. Bussler
Tribal Historic Preservation Officer
Pokagon Band of Potawatomi Indians
Office: (269) 462-4316
Cell: (269) 519-0838
Matthew.Bussler@Pokagonband-nsn.gov

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October 8, 2019

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Early coordination letter for the Gordon Road Intersection Project, involving construction of a roundabout at the intersection of Gordon Road, US 421/SR 39, Airport Road, and City Park Loop, as well as construction of a portion of the Gordon Road multi-use trail, within or near the City of Monticello, Union Township, White County, Indiana (Des. No. 1702835; DHPA No. 24381)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed INDOT's September 12, 2019, early coordination letter, which we received on September 16.

We are not aware of any parties who should be invited to participate in the Section 106 consultation on this federal undertaking, beyond those whom INDOT already has invited. However, if right-of-way is likely to be taken from a potentially historic property, it might be advisable to invite the owner of that property as soon as possible.

We look forward to reviewing the proposed area of potential effects ("APE") and the reports on investigations of above-ground cultural resources and of archaeological resources that INDOT indicated will be forthcoming. Because it is anticipated that approximately four acres of additional, permanent right-of-way will be needed, even though design work is in the early stages, we recommend that the APEs for both above-ground resources and for archaeological resources be drawn broadly enough to take into consideration possible effects of any conceivable location and configuration of the roundabout and the multi-use trail. We have observed that in some projects, the Section 106 review is completed quickly, but the review of the project later has to be revisited when an overlooked aspect of the scope of work or design is added.

Also, in the next correspondence on this project, please clarify whether or not the entire project area will be within the Monticello city limits.

The Indiana SHPO staff's archaeological reviewer for this project is Beth K. McCord, and the structures reviewer is John Carr. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

Hannah Blad
October 8, 2019
Page 2

In all future correspondence about the Gordon Road Intersection Project at Monticello, White County (Des. No. 1702835), please refer to DHPA No. 24381.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:JLC:jlc

emc: Anuradha Kumar, INDOT
Shaun Miller, INDOT
Anthony Ross, Ph.D., INDOT
Susani Branigin, INDOT
Shirley Clark, INDOT
Chad Costa, Lochmueller Group
Hannah Blad, Lochmueller Group
Wade T. Tharp, INDNR-DHPA
John Carr, INDNR-DHPA

Blad, Hannah

From: Blad, Hannah
Sent: Thursday, October 10, 2019 1:25 PM
To: deb@mccwired.com; amy@mccwired.com; parks@monticelloin.gov
Cc: Ross, Anthony; SBranigin (SBranigin@indot.IN.gov); Miller, Shaun (INDOT); Costa, Chad; Quigg, Gary; Hawley, Jessica; Fleming, Steven; jerdman@indot.in.gov
Subject: FHWA Project: Des. No. 1702835; Gordon Road Intersection Improvement Project, White County, Indiana
Attachments: Gordon Road Intersection Project_Des1702835_Early Coordination_2019-10-10.pdf

Des. No.: 1702835

Project Description: Intersection improvement

Location: Intersection of US 421/SR 39 and Gordon Road, Union Township, Town of Monticello, Indiana

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project (Des. No. 1702835).

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic properties. The following agencies/individuals are being invited to become consulting parties:

State Historic Preservation Officer, Kankakee-Iroquois Regional Planning Commission, White County Commissioners, White County Highway Department, Town of Monticello Mayor, Town of Monticello Street Department, White County Historian, White County Historical Society, Indiana Landmarks – Northern Regional Office, Monticello Parks Department, Monticello Christian Church, Absentee Shawnee Tribe of Oklahoma, Eastern Shawnee Tribe of Oklahoma, Miami Tribe of Oklahoma, Peoria Tribe of Indians of Oklahoma, Pokagon Band of Potawatomi Indians

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with your comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If a hard copy of the materials is needed, please respond to this email with your request within seven (7) days.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comments. If we do not receive a response from an invited consulting party within the time allotted, the project will proceed consistent with the proposed design. **Therefore, if we do not receive a response within thirty (30) days, your agency or organization will not receive any further information on the project unless the scope of work changes.**

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Hannah Blad
Historian/Section 106 Specialist



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

September 6, 2019

This letter was sent to the listed parties.

RE: Gordon Road Intersection Project
Intersection of US 421/SR 39, Airport Road, and Gordon Road
Town of Monticello, White County, Indiana
Des. No. 1702835

Dear Consulting Party (see attached list),

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with an intersection project (Des. No. 1702835). Lochmueller Group is under contract with White County to advance the preliminary engineering and environmental documentation for the referenced project.

This letter is part of the early coordination phase of the environmental review process requesting comments associated with this project. We are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

The proposed undertaking is on US 421 at the intersection of Gordon Road in White County, Indiana. It is within Union Township, USGS Topographic Quadrangle, in Section 4, Township 26 North, Range 3 West.

The need for the project stems from the poor level of service at the current five road approaches converging at the intersection, the high amount of summer traffic attributed to Indiana Beach, Terrace Bay, Lake Freeman, and Lake Shader, and the lack of connectivity in pedestrian facilities in that area. The purpose of the project is to improve the safety of the intersection for vehicular traffic, as well as for pedestrians.

The project will involve the construction of a roundabout at the intersection of Gordon Road, US 421/SR 39, Airport Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Also, the project will build another portion of the Gordon Road multiuse trail that will extend from near the intersection of Ebony Drive and Gordon Road to Monticello City Park. The project will require the acquisition of approximately 4 acres of additional permanent right-of-way (ROW). Excavation will be required to replace the existing pavement which will be to a depth of approximately 2.5 feet. Additionally, storm sewer work may also be required. The excavation required for this work may go to a depth of up to 10 feet. Wayfinding signage will also be included with this project. Design is still in the early stages and more details will be made available as design progresses.

Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. In accordance with 36 CFR 800.2 (c), you are hereby requested to be a consulting party to participate in the Section 106 process. Entities that have been invited to participate in the Section 106 consultation process for this project are identified in the attached list. Per 36 CFR 800.3(f), we hereby request that the Indiana State Historic Preservation Officer (SHPO) notify this office if the SHPO staff is aware of any other parties that may be entitled to be consulting parties or should be contacted as potential consulting parties for the project.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, assess its effects and seek ways to avoid, minimize or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. At this time, no cultural resource investigations have occurred; however, the results of cultural resource identification and evaluation efforts, both above-ground and archaeological, will be forthcoming. Consulting parties will receive notification when these reports are completed.

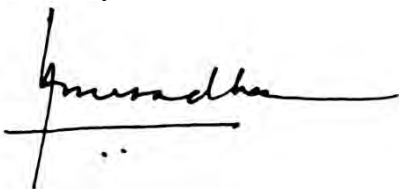
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party, or if you do not respond, you will not be included on the list of consulting parties for this project. If we do not receive your response in the time allotted, the project will proceed consistent with the proposed design and you will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Hannah Blad of Lochmueller Group at 317.334.6826 or hblad@lochgroup.com. All future responses regarding the proposed project should be forwarded to Lochmueller Group at the following address:

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, IN, 46268
hblad@lochgroup.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office

Environmental Services

Enclosures:

- USGS Topographic Map

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Kankakee-Iroquois Regional Planning Commission
- White County Commissioners
- White County Highway Department
- Town of Monticello Mayor
- Town of Monticello Street Department
- White County Historian
- White County Historical Society
- Indiana Landmarks - Northern Regional Office
- Monticello Parks Department
- Monticello Christian
- Absentee Shawnee Tribe of Oklahoma
- Eastern Shawnee Tribe of Oklahoma
- Miami Tribe of Oklahoma
- Peoria Tribe of Indians of Oklahoma
- Pokagon Band of Potawatomi Indians

Hannah Blad

From: Hannah Blad
Sent: Thursday, June 4, 2020 10:25 AM
To: deb@mccwired.com; amy@mccwired.com; parks@monticelloin.gov; BMCcord@dnr.IN.gov; jcarr (JCarr@dnr.IN.gov)
Cc: Ross, Anthony; Miller, Shaun (INDOT); SBranigin (SBranigin@indot.IN.gov); Chad Costa; Gary Quigg; Ruth Hook; Steven Fleming; mkyburz@whitecountyindiana.us; Erdmann, Jami S; PKorzeniewski (PKorzeniewski@indot.IN.gov)
Subject: FHWA Project: Des. No. 1702835; Gordon Road Intersection Improvement Project, White County, Indiana
Attachments: Gordon Road Intersection Project_Des1702835_CP Distribution Letter_2020-06-04.pdf

Des. No.: 1702835

Project Description: Intersection improvement

Location: Intersection of US 421/SR 39 and Gordon Road, Union Township, City of Monticello, Indiana

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project (Des. No. 1702835). The Section 106 Early Coordination Letter for this project was originally distributed on September 12, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Archaeology Report have been prepared and are ready for review and comment by consulting parties.

Please review this documentation located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Hannah Blad
Historian/Section 106 Specialist
Lochmueller Group

3502 Woodview Trace
Suite 150, Indianapolis, IN 46268
317.334.6826 (direct) | 574.248.2121 (mobile)
HBlad@lochgroup.com
<http://lochgroup.com>

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Hannah Blad

From: Korzeniewski, Patricia J <PKorzeniewski@indot.IN.gov>
Sent: Thursday, June 4, 2020 12:17 PM
To: Allen, Michelle (FHWA); thpo@estoo.net; Diane Hunter; lpappenfort@peoriatribe.com; Matthew.Bussler@pokagonband-nsn.gov; tonya@shawnee-tribe.com; snease@astribe.com
Cc: Ross, Anthony; Miller, Shaun (INDOT); Branigin, Susan; Korzeniewski, Patricia J; Hannah Blad
Subject: Clearance Notification FHWA Project: Des. No. 1702835; Gordon Road Intersection Improvement Project, White County, Indiana
Attachments: Gordon Road Intersection Project_Des1702835_CP Distribution Letter_2020-06-04.pdf

Des. No.: 1702835

Project Description: Intersection improvement

Location: Intersection of US 421/SR 39 and Gordon Road, Union Township, City of Monticello, White County, Indiana

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation, proposes to proceed with an intersection improvement project (Des. No. 1702835). The Section 106 Early Coordination Letter for this project was originally distributed on September 12, 2019.

As part of Section 106 of the National Historic Preservation Act, a Historic Property Report and Archaeology Report have been prepared and are ready for review and comment by consulting parties.

Please review the attached letter, which is also located in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE), and respond with any comments that you may have. If a hard copy of the materials is needed, please respond to this email with your request as soon as you can.

Consulting parties have thirty (30) calendar days from receipt of this information to review and provide comment. Tribal consulting parties may enter the process at any time and are encouraged to respond to this notification with any comments or concerns at their earliest convenience. Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Thank you in advance for your input,

Patricia Jo Korzeniewski
Archaeologist and Environmental Manager
INDOT, Cultural Resources Office
PKorzeniewski@indot.in.gov
(317) 233-2093

From: Hannah Blad <HBlad@lochgroup.com>
Sent: Thursday, June 4, 2020 10:25 AM
To: deb@mccwired.com; amy@mccwired.com; parks@monticelloin.gov; McCord, Beth K <BMccord@dnr.IN.gov>; Carr, John <JCarr@dnr.IN.gov>
Cc: Ross, Anthony <ARoss3@indot.IN.gov>; Miller, Shaun (INDOT) <smiller@indot.IN.gov>; Branigin, Susan <SBranigin@indot.IN.gov>; Chad Costa <CCosta@lochgroup.com>; Gary Quigg <gquigg@lochgroup.com>; Ruth Hook



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

June 4, 2020

This letter was sent to the listed parties.

RE: Des. No.: 1702835
DHPA Number: 24381
Gordon Road Intersection Project
Intersection of US 421/SR 39, Airport Road, and City Park Loop
City of Monticello, White County, Indiana

Dear Consulting Party,

White County, with funding from the Federal Highway Administration and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with an intersection project (Des. No. 1702835).

This letter is part of the Section 106 review process for this project. Section 106 of the National Historic Preservation Act requires federal agencies to take into account the effects of their undertakings on historic and archaeological properties. We are requesting comments from you regarding the possible effects of this project. Please use the above Des. Number and project description in your reply and your comments will be incorporated into the formal environmental study.

A Section 106 early coordination letter was distributed on September 12, 2019.

The proposed undertaking is on US 421 at the intersection of Gordon Road in White County, Indiana. It is within Union Township, USGS Topographic Quadrangle, in Section 4, Township 26 North, Range 3 West. The entire project is located within the city limits of Monticello.

The need for the project stems from the poor level of service at the current five road approaches converging at the intersection, the high amount of summer traffic attributed to Indiana Beach, Terrace Bay, Lake Freeman, and Lake Shader, and the lack of connectivity in pedestrian facilities in that area. The purpose of the project is to improve the safety of the intersection for vehicular traffic, as well as for pedestrians.

The proposed project will involve the construction of a roundabout at the intersection of US 421/SR39, Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multi-use trail east to Monticello City Park. The alignment of the trail will be shifted to accommodate the new roundabout. The project will also include updates to the curb and curbside drainage along the roadways and the addition of Americans with Disabilities Act (ADA) compliant curb

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ramps at the intersection. Excavation to a depth of approximately 2.5-feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection. This work will likely require excavation of up to 10-feet below current ground surface. Wayfinding signage will also be included with this project. This project will require the acquisition of approximately 4 additional acres of permanent right-of-way.

Lochmueller Group is under contract with White County to advance the preliminary design and environmental documentation for the referenced project. 106 Consulting, LLC has been subcontracted to complete the Section 106 archaeological documentation for the project.

In accordance with 36 CFR 800.2 (c), you were invited to become a consulting party as part of the Section 106 process, or you are hereby invited to become a consulting party as part of the Section 106 process. Entities that have previously accepted consulting party status--as well as additional entities that are currently being invited to become consulting parties--are identified in the attached list.

The Section 106 process involves efforts to identify historic properties potentially affected by the undertaking, to assess the undertaking's effects and to seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. For more information regarding the protection of historic resources, please see the Advisory Council on Historic Preservation's guide: *Protecting Historic Properties: A Citizen's Guide to Section 106 Review* available online at <https://www.achp.gov/sites/default/files/documents/2017-01/CitizenGuide.pdf>.

The Area of Potential Effects (APE) is the area in which the proposed project may cause alterations in the character or use of historic resources. The APE contains no resources listed in the National Register of Historic Places (National Register).

A historian who meets the Secretary of the Interior's Professional Qualification Standards identified and evaluated above-ground resources within the APE for potential eligibility for the National Register. As a result of the historic property identification and evaluation efforts, Monticello City Park (IHSSI #181-426-29118) and the Monticello Christian Church (Lochmueller #8) are recommended as eligible for listing in the National Register.

With regard to archaeological resources, an archaeologist who meets the Secretary of the Interior's Professional Qualification Standards identified no sites within the project area. As a result of these efforts, no sites were recommended as eligible for listing in the National Register and no further work or further work is recommended.

It should be noted that the Town of Monticello's Parks Department removed several bollards from the southwest quadrant of the park where it is anticipated the proposed roundabout will be constructed. The Parks Department is awaiting a decision on the alignment of this project before re-setting the bollards. Also, the Town of Monticello bought and demolished a c. 1950s ranch located at 100 Oak Drive. The town purchased the house/land in 2018 from the White County Economic Development Commission. Right-of-way will be acquired from this land parcel.

The Historic Property Report and Archaeology Report (Tribes only) are available for review in IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). You are invited to review these documents and to respond with comments on any historic resource impacts incurred as a result of this project so that an environmental report can be completed. We also welcome your related opinions and other input to be considered in the preparation of the environmental document. If you prefer a hard-copy of this material, please respond to this email with your request within seven (7) days.

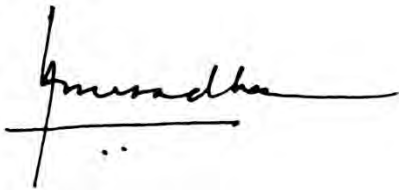
Please review the information and comment within thirty (30) calendar days of receipt. If you indicate that you do not desire to be a consulting party or if you have not previously accepted consulting party status and you do not respond to this letter, you will not be included on the list of consulting parties for this project and will not receive further information about the project unless the design changes.

For questions concerning specific project details, you may contact Hannah Blad of Lochmueller Group at 317.334.6826 or hblad@lochgroup.com. All future responses regarding the proposed project should be forwarded to Lochmueller Group at the following address:

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, IN, 46268
hblad@lochgroup.com

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

- APE Map

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Monticello Christian Church
- Monticello Parks Department
- Pokagon Band of Potawatomi Indians



Miami Tribe of Oklahoma

3410 P St. NW, Miami, OK 74354 • P.O. Box 1326, Miami, OK 74355
Ph: (918) 541-1300 • Fax: (918) 542-7260
www.miamination.com



Via email: smiller@indot.in.gov

June 4, 2020

Shaun Miller
Archaeological Team Lead, Cultural Resources Office
Indiana DOT
575 North Pennsylvania Street
Indianapolis, IN 46204

Re: Des. No. 1702835; Gordon Road Intersection Improvement Project, White County, Indiana –
Comments of the Miami Tribe of Oklahoma

Dear Mr. Miller:

Aya, kikwehsitoole – I show you respect. The Miami Tribe of Oklahoma, a federally recognized Indian tribe with a Constitution ratified in 1939 under the Oklahoma Indian Welfare Act of 1936, respectfully submits the following comments regarding Des. No. 1702835.

The Miami Tribe offers no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site. However, given the Miami Tribe's deep and enduring relationship to its historic lands and cultural property within present-day Indiana, if any human remains or Native American cultural items falling under the Native American Graves Protection and Repatriation Act (NAGPRA) or archaeological evidence is discovered during any phase of this project, the Miami Tribe requests immediate consultation with the entity of jurisdiction for the location of discovery. In such a case, please contact me at 918-541-8966 or by email at dhunter@miamination.com to initiate consultation.

The Miami Tribe accepts the invitation to serve as a consulting party to the proposed project. In my capacity as Tribal Historic Preservation Officer I am the point of contact for consultation.

Respectfully,

Diane Hunter
Tribal Historic Preservation Officer

Division of Historic Preservation & Archaeology · 402 W. Washington Street, W274 · Indianapolis, IN 46204-2739
Phone 317-232-1646 · Fax 317-232-0693 · dhpa@dnr.IN.gov · www.IN.gov/dnr/historic



June 29, 2020

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Federal Agency: Indiana Department of Transportation ("INDOT"),
on behalf of Federal Highway Administration, Indiana Division ("FHWA")

Re: Archaeological field reconnaissance short report (Bubb, 4/8/2020) and historic property report (Blad, 6/4/2020) for the proposed Gordon Road Intersection Project in the City of Monticello, White County, Indiana (Des. No. 1702835; DHPA No. 24381)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the "Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana," the staff of the Indiana State Historic Preservation Officer ("Indiana SHPO" or "INDNR-DHPA") has reviewed INDOT's June 4, 2020 letter and enclosed reports we received electronically on June 4.

Thank you for clarifying that the entire project area will be within the Monticello city limits.

Based on the information provided, and for the purposes of the Section 106 review of this project, we agree with the conclusions in the historic property report (Blad, 6/4/2020) that the Monticello City Park (IHSSI #181-426-29118) is eligible for inclusion in National Register of Historic Places ("NRHP") under Criterion A and that the Monticello Christian Church (Lochmueller #8) is eligible for inclusion in the NRHP under Criterion C. The proposed historic property boundaries for those two properties seem plausible.

Based upon the submitted archaeological short report (Bubb, 4/8/2020), and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area, and it is our opinion that no further archaeological investigations appear necessary at this project area.

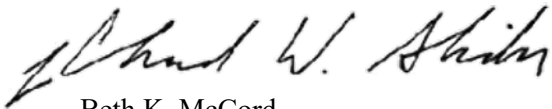
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

When plans for the project become available, please provide them to the Indiana SHPO staff and to the other consulting parties.

The Indiana SHPO staff's archaeological reviewer for this project is Beth K. McCord, and the structures reviewer is John Carr. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the Gordon Road Intersection Project in Monticello, White County (Des. No. 1702835), please continue to refer to DHPA No. 24381.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:JLC:jlc

emc: Anuradha Kumar, INDOT
Shaun Miller, INDOT
Anthony Ross, Ph.D., INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Chad Costa, Lochmueller Group
Hannah Blad, Lochmueller Group
Wade T. Tharp, INDNR-DHPA
John Carr, INDNR-DHPA



July 9, 2020

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: INDOT’s request for clarification of our opinion on the eligibility of Oak Park Subdivision
for the National Register of Historic Places (“NRHP”), in connection with the proposed
Gordon Road Intersection Project in the City of Monticello, White County, Indiana
(Des. No. 1702835; DHPA No. 24381)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “INDNR-DHPA”) has reviewed INDOT’s June 29, 2020 letter e-mail about the Oak Park Subdivision.

In our June 29, 2020, letter on this project, we said the following about above-ground properties identified and evaluated in the historic property report (“HPR”; Blad, 6/4/2020):

Based on the information provided, and for the purposes of the Section 106 review of this project, we agree with the conclusions in the historic property report (Blad, 6/4/2020) that the Monticello City Park (IHSSI #181-426-29118) is eligible for inclusion in National Register of Historic Places (“NRHP”) under Criterion A and that the Monticello Christian Church (Lochmueller #8) is eligible for inclusion in the NRHP under Criterion C. The proposed historic property boundaries for those two properties seem plausible.

We did not, however, expressly say that the Monticello City Park and the Monticello Christian Church are the *only* above-ground properties within the area of potential effects that are eligible for inclusion in the NRHP. Neither did we comment on the eligibility of Oak Park Subdivision, which the author of the HPR had evaluated as not being eligible for inclusion in the NRHP, after applying the guidance in the Multiple Property Documentation Form for “Residential Planning and Development in Indiana, 1940-1973.” We apologize for that oversight.

Subsequently, INDOT’s Cultural Resources Office staff asked us to clarify our position on the NRHP eligibility of Oak Park Subdivision, and we have taken a closer look at the HPR and other available information on the subdivision.

For the purposes of the Section 106 review of this project, we do *not* believe that Oak Park Subdivision is eligible for inclusion in the NRHP. Without a better post-war inventory of White County, we cannot be more certain than that.

We will share our thoughts about why we do not think Oak Park Subdivision is eligible. The subdivision has some of the characteristics of a custom development as defined by the Residential Planning and Development document: There was a modest attempt to incorporate views of the water asset of Tippecanoe River/Lake Freeman, the rural lane nature of the naturalistic drives is evident, and lots appear to retain mature trees. We differ in opinion from the HPR in terms of the automobile – suburb orientation of the area. Truly, a round trip to downtown Monticello would take five minutes in a car from the area. However, the layout of the houses and area are totally given to the automobile: There are no sidewalks, and, to our knowledge, there was no public transportation. In short, even the basic task of provisioning one's house in the subdivision in 1960, *without a car*, would have required carrying sacks of groceries on a sidewalk-less, high-speed, state route for a good bit of time. We agree with the basic conclusion of the HPR. The small scale of the project and nature of the housing do not rise to the level of making a locally significant example of the property type. The housing stock is modest in scale, and more significantly, appears to be only a few measures beyond being customized from standard tract house design sources. While certainly a desirable place to live, from a historical point of view, Oak Park Subdivision appears to be a partially-realized example of a custom suburb from its time period.

Our June 29 comments on the archaeological identification and evaluation efforts remain unchanged:

Based upon the submitted archaeological short report (Bubb, 4/8/2020), and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the NRHP within the proposed project area, and it is our opinion that no further archaeological investigations appear necessary at this project area.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to INDNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff's archaeological reviewer for this project is Beth K. McCord, and the structures reviewer is John Carr. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the Gordon Road Intersection Project in Monticello, White County (Des. No. 1702835), please continue to refer to DHPA No. 24381.

Very truly yours,



Beth K. McCord
Deputy State Historic Preservation Officer

BKM:PCD:JLC:jlc

emc: Anuradha Kumar, INDOT
Shaun Miller, INDOT
Anthony Ross, Ph.D., INDOT
Susan Branigin, INDOT
Shirley Clark, INDOT
Chad Costa, Lochmueller Group



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 234-5168

Eric Holcomb, Governor
Joe McGuinness, Commissioner

December 17, 2020

This letter was sent to the listed parties.

Re: Des. No.: 1702835
DHPA Number: 24381
Gordon Road Intersection Project
Intersection of US 421/SR 39, Airport Road, Gordon Road, and City Park Loop
City of Monticello, White County, Indiana

Dear Consulting Party,

Pursuant to Section 106 of the National Historic Preservation Act (1966) and 36 CFR Part 800 (Revised January 2001 and incorporating amendments effective August 5, 2004), federal agencies are required to take into account the effects of their undertakings on historic and archaeological properties. White County, with funding from the Federal Highway Administration (FHWA) and administrative oversight from the Indiana Department of Transportation (INDOT), proposes to proceed with an intersection project, Des. No. 1702835. Lochmueller Group is under contract with White County to advance the preliminary engineering and the environmental documentation for the referenced project. This letter intends to address the anticipated effects from the undertaking, ahead of FHWA's issuance of the formal effects finding and supporting 800.11(e) documentation. Input from participating consulting parties is being sought regarding this preliminary effects determination.

The proposed undertaking is on US 421 at the intersection of Gordon Road in White County, Indiana. It is within Union Township, Monticello South USGS Topographic Quadrangle, in Section 4, Township 26 North, Range 3 West. The entire project is located within the city limits of Monticello.

Proposed Project

The proposed project will involve the construction of a roundabout at the intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multi-use trail east to Monticello City Park and a sidewalk will extend from Monticello City park north along US 421/Main Street to the north entrance of the park. The alignment of the trail will be shifted to accommodate the new roundabout. The project will also include updates to the curb and curbside drainage along the roadways and the addition of Americans with Disabilities Act (ADA) compliant curb ramps at the intersection. At the intersection as well, lights will be added and the depth of excavation for those is anticipated to be 8 feet. Excavation to a depth of approximately 2.5-feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection which would involve excavation up to 10-feet below current ground surface. Wayfinding signage will also be included. This project will require the acquisition of approximately 1.61 acres of

permanent right-of-way (ROW) and 0.46 acre of temporary ROW. Please see Attachment D for Stage 2 Plans.

Historic Properties

A Historic Property Report (HPR) was completed for the project and submitted to the State Historic Preservation Officer (SHPO) and other consulting parties for review on June 4, 2020. No resources in the Area of Potential Effects (APE) are listed in the National Register of Historic Places (National Register). Two resources were recommended eligible for listing in the National Register: Monticello City Park (IHSSI #181-426-29118) and Monticello Christian Church (Lochmueller #8). Please see attachments A and B for maps and photographs of these resources.

Review of Consulting Party Comments Received after HPR

In a letter dated June 4, 2020, the Miami Tribe of Oklahoma responded to the HPR and offered no objection to the project. In addition, the SHPO responded to the HPR on June 29, 2020, in a letter concurring with the conclusions in the HPR including the eligibility of Monticello City Park and the Monticello Christian Church, noting that “[t]he proposed historic property boundaries for those two properties seem plausible.” In a follow up letter dated July 9, 2020 regarding the eligibility of the Oak Park Subdivision, the SHPO staff clarified the response from their June 29, 2020 letter, stating that they agreed that the subdivision was not eligible for listing in the National Register.

The owners of the historic properties (Monticello Parks Department and the Monticello Christian Church) did not respond to the consulting party invitation.

No other comments were received from consulting parties after the distribution of the HPR.

Effects to Historic Properties

According to CFR 800.5(a)(1), “an adverse effect is found when an undertaking may alter, directly or indirectly, any of the characteristics of a historic property that qualify the property for inclusion in the National Register in a manner that would diminish the integrity of the property’s location, design, setting, materials, workmanship, feeling, or association.” The following discusses potential effects to Monticello City Park (IHSSI #181-426-29118) and Monticello Christian Church (Lochmueller #8). Please see maps and photographs of these resources in Attachments A and B.

Monticello City Park (IHSSI #181-426-29118) – Monticello City Park is located in southern Monticello and has multiple structures and features associated with Works Progress Administration (WPA) construction. Between 1935 and 1938 workers for the WPA built several structures in Monticello City Park in the WPA Rustic architecture style. The Monticello City Park is eligible for listing in the National Register under Criterion A as a local example of a WPA-associated property and Criterion C for its architectural significance.

The proposed undertaking will encroach upon the recommended National Register boundary for the Monticello City Park. A portion of a roundabout will be constructed in the southwestern corner of the recommended National Register boundary. As a result of the construction of the roundabout, City Park Loop will be reconstructed and realigned within the recommended boundary as well. The new southern end of City Park Loop will meet the roundabout directly south of the recommended National Register boundary within city-owned ROW. From the roundabout, the realignment of City Park Loop will curve to the northeast and connect with the existing loop drive just southeast of the baseball diamond. In addition to the roundabout and road reconstruction, a sidewalk will be constructed between the south fence-line of the baseball diamond and the new alignment of City Park Loop and along the east side of S. Main Street (US

421/SR 39). This sidewalk will extend from a terminus just southeast of the baseball diamond to the north entrance of City Park Loop from S. Main Street. As a result of the new roundabout, twenty-seven concrete, pyramidal markers located along the perimeter of the park along S. Main Street will be relocated along the inside (park-side) edge of the new proposed sidewalk. The markers have been attributed to the WPA-era of the park anecdotally, since the bulk of park's improvements occurred during that period. However, no documentation has been found to establish their construction by the WPA or when the markers were added to the park boundary. Further, no information was obtained regarding whether or not the current location of the markers is where they were placed originally or has any historic importance. The markers have been replaced throughout the years by the parks department, when they have been damaged by vehicular traffic, using a mold designed to replicate the design. Recently, seventeen markers have been removed by the parks department in the location of the intersection with US 421/Main Street and Gordon Road, in anticipation of the project. The same markers will be placed in new locations near the entrance to the park as part of the project. The markers and greenspace in front of the park are the only features within the boundary for the park that will be impacted by the project. The baseball diamond will not be impacted by the project. See Attachment C, for the location of where the markers were previously located and where they will be placed as part of the project.

Also, within the recommended National Register boundary, four 40-foot-high-mast lights will be added adjacent to the roundabout. The project will have "No Adverse Effect" to this resource because the proposed changes will not alter the Monticello City Park in a manner that would diminish its historic integrity. Please see Attachment B, pages 6-8 for photographs of this property and Attachment C, pages 3, 4, 6, 8, 10, and 16 for plan sheets showing the proposed work around the property.

According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply. The undertaking will alter the existing setting within the property beyond its present condition, but it will not alter Monticello City Park in a manner that would diminish its historic integrity. The addition of a sidewalk along S. Main Street and City Park Loop, the realignment of a portion of City Park Loop, the construction of part of the roundabout, and the relocation of markers will not adversely alter the setting of the property.

Per 800.5(a)(2)(i), the undertaking will not result in the "Physical destruction of or damage to all or part of the property." Although alterations (the relocation of markers, the construction of a sidewalk, storm sewer, lighting, road, and roundabout) will occur within the recommended National Register boundary of the property along S. Main Street, the property will not be destroyed or damaged by this project.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will not cause "Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary's Standards for the Treatment of Historic Properties and/or other applicable guidelines." The addition of a sidewalk along S. Main Street and City Park Loop, the realignment of a portion of City Park Loop, the relocation of markers, the addition of high-mast lighting and the construction of part of the roundabout and storm sewer construction will alter the property within the recommended National Register boundary of the property along S. Main Street and City Park Loop. However, these alterations will not diminish the integrity of the property. It is anticipated that 0.68 acre (29,768 square feet) of permanent ROW will be acquired from this property for the construction of the sidewalk, roundabout, and City Park Loop.

Per 36 CFR 800.5(a)(2)(iii), the undertaking will not result in the, "Removal of the property from its historic location."

Per 36 CFR 800.5(a)2(iv), the undertaking will not result in a “Change of the character of the property’s uses or of physical features within the property setting that contribute to its historic significance.” Changes in the viewshed/streetscape, such as the addition of a sidewalk, lighting, and roundabout, the realignment of a portion of City Park Loop, and the relocation of markers, will not change the character of the property’s use or the physical features that contribute to its historic significance in this area such that the overall integrity of the property will be diminished. The existing viewshed/streetscape includes modern sidewalks, signage, curbing, ramps, pavement markings, and lighting which were installed outside the period of significance.

Per 36 CFR 800.5(a)2(v), the undertaking will not cause the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Changes in the viewshed/streetscape, such as the addition of a sidewalk along S. Main Street and City Park Loop, the realignment of a portion of City Park Loop, the relocation of markers, the addition of high-mast lighting, and the construction of part of the roundabout, will introduce visual elements to the property. The construction of the sidewalk will bring more pedestrian and bicycle traffic by the park. However, these elements will be very similar to existing elements within the existing viewshed, will not be highly visible, and will not diminish the integrity of the significant historic features of the property.

Per 36 CFR 800.5(a)2(vi), the undertaking will not result in the “Neglect of a property which causes its deterioration...”

Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” Ownership of the property will not change as a result of this project.

Monticello Christian Church (Lochmueller #8) – The Monticello Christian Church, located at 105 Gordon Road in Monticello, was constructed in 1968 in the Contemporary Revival style. Sparsely ornamented, the structure is dominated by three steeply pitched gable roofs and freestanding tower. The exterior is clad in brick veneer and there are concrete sills on all the lower windows. The central gable features a large, multi-light, rectangular stained-glass window on the north elevation, rising from the lower hipped roof to the gable roof peak. On the south elevation, the central gable has a tall window in the shape of a thin Latin cross. The sanctuary seats face the cross window. The reinforced concrete tower, constructed in a narrow, cruciform plan, rises in front of the central gable and ends high above the roof peak, where it is crowned by three metal Latin crosses. The Monticello Christian Church is eligible for listing in the National Register under Criterion C for its architectural significance.

The proposed undertaking will encroach upon the recommended National Register boundary of the Monticello Christian Church. The existing ROW along Gordon Road extends approximately 48 feet north and 40 feet south of the roadway centerline. The recommended National Register boundary ends at the ROW line which extends 17 feet from the edge of pavement. The construction of the roundabout and the realignment of the Gordon Road approach into the roundabout will take place within the recommended National Register boundary for the Monticello Christian Church. In total, 0.21 acre of permanent ROW and 0.46 acre of temporary ROW will be acquired from the Monticello Christian Church. Traffic on Airport Road will have a one-way signal during construction, and traffic heading north will have to travel west on Gordon Road to access downtown Monticello and SR 39. The temporary ROW is required as part of the Maintenance of Traffic (MOT) plan and provides the necessary clearance to provide a temporary access road between Gordon and Airport Road while the roundabout is constructed. Two of the three modern church signs located along Gordon Road, S. Main Street, and Airport Road will not be affected by this project. The sign located at the southwest corner of the intersection of US 421/Main Street and Gordon Road will be relocated as part of the project. This sign is a modern addition to the church property and does

not contribute to the architectural significance of the church. The church sign will be relocated within the temporary ROW. In addition, five trees adjacent to the sign will be removed as part of the project. Four out of the five trees have a diameter at breast height (DBH) of 10 inches or greater, while the fifth tree has a DBH of less than 10 inches. No other existing features within the National Register boundary will be impacted by the encroachment. In addition, three 40-foot high-mast lights will be added near the new roundabout within the recommended National Register boundary. The project will have “No Adverse Effect” to this resource because the proposed changes will not alter the Monticello Christian Church in a manner that would diminish its historic integrity. Please see Attachment B, pages 4 and 5 for photographs of this property, and Attachment C, pages 4, 6, 8, 10, 12, 14, and 16 for plan sheets showing the proposed work around this property.

According to 36 CFR 800.5(a)(1) the criteria of adverse effect do not apply. The undertaking will alter the existing setting of the resource beyond its present condition, but it will not alter the Monticello Christian Church in a manner that would diminish its historic integrity. The construction of small portions of the roundabout, and the Gordon Road approach into the roundabout, at the northeast corner of the recommended National Register boundary for the church will not adversely alter the setting of the property.

Per 800.5(a)(2)(i), the undertaking will not result in the “Physical destruction of or damage to all or part of the property.” Although alterations (high-mast lights, sign and tree removal, construction of a small portion of the roundabout and a small portion of the approach of Gordon Road) will occur within the recommended National Register boundary of the property along Gordon Road and US 421/S. Main Street, the property will not be destroyed or damaged by this project.

Per 36 CFR 800.5(a)(2)(ii), the undertaking will not cause “Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties and/or other applicable guidelines.” The addition of lights, the removal of the sign and trees, the construction of small portion of the roundabout and a small portion of the approach of Gordon Road will alter the property within the recommended National Register boundary along Gordon Road and US 421/S. Main Street. However, these alterations will not diminish the integrity of the property. These improvements will require 0.21 acres of permanent ROW and 0.46 acres of temporary ROW be acquired from the Monticello Christian Church.

Per 36 CFR 800.5(a)(2)(iii), the undertaking will not result in the “Removal of the property from its historic location.”

Per 36 CFR 800.5(a)(2)(iv), the undertaking will not result in a “Change of the character of the property’s uses or of physical features within the property setting that contribute to the historic significance.” Changes in the viewshed/streetscape, including the addition of lights, the removal of the sign and trees, the construction of small portions of the roundabout and the Gordon Road approach will not change the character of the property’s use or the physical features that contribute to its historic significance in this area such that the overall integrity of the property will be diminished. The existing viewshed/streetscape includes modern sidewalks, signage, curbing, ramps, pavement markings, and lighting which were installed outside the period of significance.

Per 36 CFR 800.5(a)(2)(v), the undertaking will not cause the “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features.” Changes in the viewshed/streetscape including the addition of lights, the removal of the sign and trees, the partial construction of a roundabout and approach of Gordon Road within the recommended National Register

boundary will introduce visual elements to the property. However, these elements will be very similar to existing elements, will not be highly visible, and will not diminish the integrity of the significant historic features of the property.

Per 36 CFR 800.5(a)2(vi), the undertaking will not cause the “Neglect of a property which causes its deterioration...”

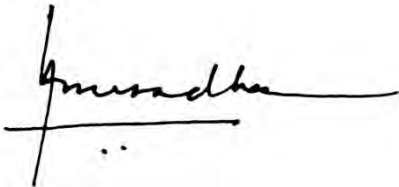
Per 36 CFR 800.5(a)2(vii), the undertaking will not cause the “Transfer, lease, or sale of property out of Federal ownership or control...” Ownership of the historic resource will not change as a result of this project.

This letter and attachments may be viewed electronically by accessing INDOT’s Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents/> (the Des. No. is the most efficient search term, once in IN SCOPE). Please use the project identification details provided in the subject heading to search for the documents. A hard copy of the materials will be provided to any invited consulting party who makes such a request within seven (7) days of receipt of this notification.

Please review the attached information and comment at your earliest convenience. In your comments, please indicate if you concur with the effects determinations in this letter. If you have any questions regarding this information, please contact Hannah Blad at 317.334.6826 or hblad@lochgroup.com. We appreciate any comments within 30 days of receipt of this information.

Tribal contacts may contact Shaun Miller at smiller@indot.in.gov or 317-233-6795 or Michelle Allen at FHWA at michelle.allen@dot.gov or 317-226-7344.

Sincerely,



Anuradha V. Kumar, Manager
Cultural Resources Office
Environmental Services

Enclosures:

- Attachment A – Maps
- Attachment B – Photographs in Areas of Concern
- Attachment C – Stage 2 Plans

Enclosures removed to
prevent duplication

Distribution List:

- State Historic Preservation Officer
- Miami Tribe of Oklahoma
- Pokagon Band of Potawatomi Indians



January 19, 2021

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Effects letter for the proposed Gordon Road Intersection Project in the City of Monticello, White
County, Indiana (Des. No. 1702835; DHPA No. 24381)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “DNR-DHPA”) has reviewed INDOT’s December 17, 2020 effect letter, which we received electronically on the same date.

We agree with the December 17 effects analysis letter that the Monticello City Park (IHSSI #181-426-29118) which is eligible for inclusion in National Register of Historic Places (“NRHP”) under Criterion A, and the Monticello Christian Church (Lochmueller #8) which is eligible for inclusion in the NRHP under Criterion C do not appear to be adversely affected by this project. We do not believe the characteristics that qualify the identified historic properties for inclusion in the NRHP will be diminished as a result of this project.

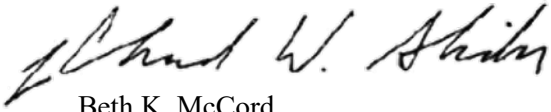
If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to DNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

Unless another consulting party expresses concern about an effect on a historic property by the comment deadline, it might now be appropriate to ask INDOT to make a finding.

The Indiana SHPO staff’s archaeological reviewer for this project is Beth K. McCord, and the structures reviewer is Chad Slider. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the Gordon Road Intersection Project in Monticello, White County (Des. No. 1702835), please continue to refer to DHPA No. 24381.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord". The signature is fluid and cursive, with the first name "Beth" being more prominent.

Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CWS:cws

emc: Karstin Carmany-George, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Anthony Ross, Ph.D., INDOT
Susan Branigin, INDOT
Chad Costa, Lochmueller Group
Hannah Blad, Lochmueller Group
Chad Slider, DNR-DHPA
Beth McCord, DNR-DHPA

Section 106 800.11(e)

Appendix E

**Historic Property Report Summary
Archaeology Report Summary**

Gordon Road Intersection Project:
Intersection of US 421/SR 39, Airport Road, Gordon Road, and City Park Loop
Town of Monticello, White County, Indiana
Des. No. 1702835

Historic Property Report
June 4, 2020



Prepared for:
White County Board of Commissioners
Federal Highway Administration



Hannah Blad
Hannah Blad

**Gordon Road Intersection Project:
Intersection of US 421/SR 39, Airport Road, Gordon Road, and City Park Loop
Town of Monticello, White County, Indiana
Des. No. 1702835
Historic Property Report**

I. MANAGEMENT SUMMARY

This report documents the identification and evaluation efforts for properties included in the Area of Potential Effects (APE) for the Gordon Road Intersection Project at the intersection of US 421/SR39, Airport Road, Gordon Road, and City Park Loop in the Town of Monticello. Above-ground resources located within the project APE were identified and evaluated in accordance with Section 106, National Historic Preservation Act (NHPA) of 1966, as amended, and the regulations implementing Section 106 (36 CFR Part 800).

As a result of the NHPA, as amended, and CFR Part 800, federal agencies are required to take into account the impact of federal undertakings upon historic properties in the area of the undertaking. Historic properties include buildings, structures, sites, objects, and/or districts that are eligible for or listed in the National Register of Historic Places (National Register). As this project is receiving funding from the Federal Highway Administration (FHWA), it is subject to a Section 106 review.

The APE contains no properties listed in the National Register.

The APE contains two properties that are recommended eligible for listing in the National Register:

- Monticello City Park; 906 City Park Loop, Monticello, IN
- Monticello Christian Church; 105 Gordon Road, Monticello, IN

II. INTRODUCTION/PROJECT DESCRIPTION

The White County Board of Commissioners and FHWA propose to proceed with an intersection improvement project. The proposed project is located within the southern portion of the City of Monticello in White County. The project will involve the construction of a roundabout at the intersection of US 421/SR 39, Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multiuse trail east to Monticello City Park. The alignment of the trail will be shifted north to accommodate the new roundabout. The project will require the acquisition of approximately 4 acres of additional permanent right-of-way (ROW).

The project area is located in an urban setting with residential development, commercial properties, recreational and religious facilities.

The APE is the “geographic area or area within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties, if any such properties exist. The area of

Archaeological Field Reconnaissance for the Proposed
Gordon Road and US 421/SR 39 Roundabout (Des. 1702835) in
the City of Monticello, Union Township, White County, Indiana

Prepared by:

Emily Culver, MA & Louis Bubb, MA

Submitted By:

Louis Bubb, MA
Principal Investigator
106 Consulting LLC
4425 Redmont Road
Deer Park, Ohio 45236-3138
(513) 620-6770
LouisBubb@Gmail.com

Prepared for:

Mr. Gary Quigg
Lochmueller Group, Inc.
3502 Woodview Trace #150
Indianapolis, IN 46268
(317) 222-3880

Lead Agency:

Indiana Department of Transportation

April 8, 2020

Louis Bubb, MA, Principal Investigator
Project #106C-0383

All fieldwork was conducted in accordance to Division of Historic Preservation and Archaeology (DHPA) Guidebook for Indiana Historic Sites and Structures Inventory (2019).

The majority of the project area was in lawn grass exhibiting 0 to 30 percent ground surface visibility (GSV) (see Attachment B: Figure 5; Attachment C: Photograph 1 through Photograph 3). Screened shovel test probes were excavated at a 15 m (49 ft) interval in these areas.

Describe Methods: A small portion of the proposed project area—at the location of a c. 1960 residence which had recently been removed for this project—exhibited 50 to 75 percent GSV (see Attachment C: Photograph 5). This area was surface collected at a 5 m (16 ft) interval in addition to the excavation of STPs. Intermixed soil horizons and imported non-local fill were noted at this location and no cultural materials were encountered

The southern portion of the proposed project area—which follows S.R. 39—contained steep cut slopes created by a relatively recent reconstruction of S.R. 39 (see Attachment C: Photograph 6 and Photograph 7). This area was visually inspected.

Attach photographs documenting disturbances below

Describe Disturbances: Existing roadway, utility corridors, ditches, slope

Comments:

Results

- ☐ Archaeological records check has determined that the project area does not have the potential to contain archaeological resources.
- ☐ Archaeological records check has determined that the project area has the potential to contain archaeological resources.
- ☒ Phase Ia reconnaissance has located no archaeological resources in the project area.
- ☐ Phase Ia reconnaissance has identified landforms conducive to buried archaeological deposits.

Actual Area Surveyed hectares: 07.9

acres: 19.6

Comments:

No archaeological sites were encountered within the proposed project area. Soils were largely consistent with the Darroch and Octagon series soils (see Soil Survey Staff 2019b), though several STPs did exhibit intermixed or stripped soil horizons, the result of disturbances associated with residential and commercial development and prior roadway construction (e.g., Attachment C: Photograph 4 and Photograph 5).

With no archaeological resources listed upon or eligible for inclusion to the National Register of Historic Places being located within or adjacent to the proposed project area, no further archaeological assessment seems warranted.

Project clearance is recommended.

In the unlikely event that unrecorded archaeological deposits are encountered, construction activities must cease in the vicinity and INDOT-CRO and DHPA contacted to determine the next appropriate actions. Similarly, if human remains are observed, any further disturbance must cease and INDOT-CRO and DHPA must be contacted.

Recommendation

- ☐ The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
- ☐ The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.

☒ The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.

☐ The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.

☐ The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

☒ Figure showing project location within Indiana.

☒ USGS topographic map showing the project area (*1:24,000 scale*).

☒ Aerial photograph showing the project area, land use and survey methods.

☒ Photographs of the project area.

☐ Project plans (*if available*)

Other Attachments:

References Cited:

Comments:

Curation

Curation Facility for Project Documentation:



March 31, 2021

Hannah Blad
Section 106/Historian
Lochmueller Group
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Federal Agency: Indiana Department of Transportation (“INDOT”),
on behalf of Federal Highway Administration, Indiana Division (“FHWA”)

Re: Indiana Department of Transportation’s finding of “no adverse effect” on behalf of the Federal Highway Administration Effects letter for the proposed Gordon Road Intersection Project in the City of Monticello, White County, Indiana (Des. No. 1702835; DHPA No. 24381)

Dear Ms. Blad:

Pursuant to Section 106 of the National Historic Preservation Act of 1966, as amended (54 U.S.C. § 306108), 36 C.F.R. Part 800, and the “Programmatic Agreement (PA) Among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program In the State of Indiana,” the staff of the Indiana State Historic Preservation Officer (“Indiana SHPO” or “DNR-DHPA”) has reviewed your review submittal form dated March 1, 2021 and INDOT’s finding and the 36 C.F.R. § 800.11(e) documentation all of which was received on March 1 for the above indicated project.

We concur with INDOT’s March 1, Section 106 finding, on behalf of FHWA, that the only historic properties eligible for inclusion in the National Register of Historic Places (“NRHP”) within the area of potential effects are the Monticello City Park (IHSSI #181-426-29118) located in southern Monticello, and the Monticello Christian Church (Lochmueller #8) located at 105 Gordon Road. We concur with FHWA’s individual findings of No Adverse Effect for the Monticello City Park and the Monticello Christian Church, and we also note that FHWA intends to issue de minimis findings for both properties in satisfaction of Section 4(f). Accordingly, we concur with INDOT’s March 1, 2021, Section 106 finding, on behalf of FHWA of “No Adverse Effect” for the Gordon Road Intersection Project.

If any prehistoric or historic archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29) requires that the discovery be reported to DNR-DHPA within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and Indiana Code 14-21-1-29 does not obviate the need to adhere to applicable federal statutes and regulations, including but not limited to 36 C.F.R. Part 800.

The Indiana SHPO staff's archaeological reviewer for this project is Beth K. McCord, and the structures reviewer is Chad Slider. However, if you have a question about the Section 106 process, please contact initially the INDOT Cultural Resources staff members who are assigned to this project.

In all future correspondence about the Gordon Road Intersection Project in Monticello, White County (Des. No. 1702835), please continue to refer to DHPA No. 24381.

Very truly yours,

A handwritten signature in black ink, appearing to read "Beth K. McCord".

Beth K. McCord
Deputy State Historic Preservation Officer

BKM:CWS:cws

emc: Karstin Carmany-George, FHWA
Anuradha Kumar, INDOT
Shaun Miller, INDOT
Anthony Ross, Ph.D., INDOT
Susan Branigin, INDOT
Chad Costa, Lochmueller Group
Hannah Blad, Lochmueller Group
Chad Slider, DNR-DHPA
Beth McCord, DNR-DHPA

Public Notice
Des. No. 1702835

White County is planning to undertake an intersection improvement project, funded in part by the Federal Highway Administration (FHWA). The project is located in the City of Monticello at the intersection of US 421/SR 39, Airport Road, Gordon Road, and City Park Loop.

Under the preferred alternative, the proposed project would involve the construction of a roundabout at the intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multi-use trail east to Monticello City Park and a sidewalk will extend from Monticello City park north along US 421/Main Street to the north entrance of the park. The alignment of the trail will be shifted to accommodate the new roundabout. The project will also include updates to the curb and curbside drainage along the roadways and the addition of Americans with Disabilities Act (ADA) compliant curb ramps at the intersection. At the intersection as well, approximately fourteen high mast lights will be added and the depth of excavation for those is anticipated to be 8 feet. Excavation to a depth of approximately 2.5-feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection which would involve excavation up to 10-feet below current ground surface. Wayfinding signage will also be included. This project will require the acquisition of approximately 1.61 acres of permanent right-of-way (ROW) and 0.46 acre of temporary ROW.

Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include the Monticello City Park and the Monticello Christian Church. The proposed action impacts properties listed in or eligible for the NRHP. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Adverse Effect" finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection at the Lochmueller Group office listed below. This documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>. Additionally, persons with limited Internet access may request project information be mailed. Please contact Hannah Blad at 574.334.5487 and hblad@lochgroup.com. This documentation serves as the basis for the "No Adverse Effect" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Hannah Blad, Lochmueller Group, 112 W. Jefferson Blvd., Suite 500, South Bend, IN 46601, hblad@lochgroup.com, 574.334.5487, no later than April 10, 2021.

In accordance with the "Americans with Disabilities Act", if you have a disability for which White County needs to provide accessibility to the document(s) such as interpreters or readers, please contact Katherine Tardiff, 574.583.4568, adacoordinator@monticelloin.gov.

PROOF OF PUBLICATION

STATE OF INDIANA)
COUNTY OF WHITE)

HERALD JOURNAL
114 S Main Street
Monticello, IN 47960

Lochmueller Group
502 Woodview Trace
Suite 150
Indianapolis, IN 46268

Legal# 21-064/HJ
Des No 1702835

Publisher's Fee: \$46.26

(ATTACH COPY OF AD)

The undersigned, Brenda Grandstaff, Legal Advertising Clerk of THE MONTICELLO HERALD JOURNAL, a weekly newspaper of general circulation, printed in the English language and published in Monticello, Indiana in said county; does hereby certify that the advertisement attached hereto is a true copy, which was duly published in said paper for:

1 week(s). The first publication was on
11th day of March 2021

In addition, this News paper has a website and this public notice was posted on the same day as it was published in the

Dated this 11th day of March 2021

Brenda Grandstaff

Brenda Grandstaff, Legal Advertising Clerk

PUBLIC NOTICE

Des. No. 1702835

White County is planning to undertake an intersection improvement project, funded in part by the Federal Highway Administration (FHWA). The project is located in the City of Monticello at the intersection of US 421/SR 39, Airport Road, Gordon Road, and City Park Loop.

Under the preferred alternative, the proposed project would involve the construction of a roundabout at the intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multi-use trail east to Monticello City Park and a sidewalk will extend from Monticello City park north along US 421/Main Street to the north entrance of the park. The alignment of the trail will be shifted to accommodate the new roundabout. The project will also include updates to the curb and curbside drainage along the roadways and the addition of Americans with Disabilities Act (ADA) compliant curb ramps at the intersection. At the intersection as well, approximately fourteen high mast lights will be added and the depth of excavation for those is anticipated to be 8 feet. Excavation to a depth of approximately 2.5-feet below current ground surface will be required for the roadway work. Additionally, this project will likely require updates to the storm sewer system at the intersection which would involve excavation up to 10-feet below current ground surface. Wayfinding signage will also be included. This project will require the acquisition of approximately 1.61 acres of permanent right-of-way (ROW) and 0.46 acre of temporary ROW.

Properties listed in or eligible for the National Register of Historic Places (NRHP) located within the Area of Potential Effects (APE) include the Monticello City Park and the Monticello Christian Church. The proposed action impacts properties listed in or eligible for the NRHP. The Indiana Department of Transportation (INDOT), on behalf of the FHWA, has issued a "No Adverse Effect" finding for the project because the project will not diminish the integrity of the characteristics that qualify the historic properties within the APE for inclusion in the NRHP. In accordance with the National Historic Preservation Act, the views of the public are being sought regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.4(d)(2), the documentation specified in 36 CFR 800.11(e) is available for inspection at the Lochmueller Group office listed below. This documentation can be viewed electronically by accessing INDOT's Section 106 document posting website IN SCOPE at <http://erms.indot.in.gov/Section106Documents>.

Additionally, persons with limited Internet access may request project information be mailed. Please contact Hannah Blad at 574.334.5487 and hblad@lochgroup.com. This documentation serves as the basis for the "No Adverse Effect" finding. The views of the public on this effect finding are being sought. Please reply with any comments to Hannah Blad, Lochmueller Group, 112 W. Jefferson Blvd., Suite 500, South Bend, IN 46601, hblad@lochgroup.com, 574.334.5487, no later than **APRIL 10, 2021**.

In accordance with the "Americans with Disabilities Act", if you have a disability for which White County needs to provide accessibility to the document(s) such as interpreters or readers, please contact Katherine Tardiff, 574.583.4568, adacoordinator@monticelloin.gov.

hspaxlp

Categorical Exclusion

Appendix E

**Red Flag Investigation
& Hazardous Materials**



Date: September 11, 2019

To: Site Assessment & Management
Environmental Policy Office - Environmental Services Division
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Angela Kattmann, LPG
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
akattmann@lochgroup.com

Re: RED FLAG INVESTIGATION
Des. No. 1702835, Local Project
Intersection Improvement Project
Gordon Road and US 421
White County, Indiana

PROJECT DESCRIPTION

Brief Description of Project: White County proposes to proceed with an intersection improvement project at the intersection of Gordon Road, US 421, and Airport Road in the Town of Monticello in White County, Indiana. The project will involve the construction of a roundabout at the intersection to assist in facilitating traffic at the intersection. The project will require the acquisition of approximately 4 acres of additional permanent right-of-way. Design is still in the early stages and more details will be made available as design progresses.

Bridge and/or Culvert Project: Yes ☐ No ☒ Structure # _____

If this is a bridge project, is the bridge Historical? Yes ☐ No ☐ , Select ☐ Non-Select ☐

(Note: If the project involves a historical bridge, please include the bridge information in the Recommendations Section of the report).

Proposed right of way: Temporary ☐ # Acres _____ Permanent ☒ # Acres 4.0 , Not Applicable ☐

Type of excavation: Excavation will be required to replace the existing pavement which will be to a depth of approximately 2.5 feet. Additionally, storm sewer work may also be required. The excavation required for this work may go to a depth of up to 10 feet.

Maintenance of traffic: It is anticipated that the project will require the closure of the intersection to through traffic. A detour with appropriate signage will be established through the duration of road closure.

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Work in waterway: Yes ☐ No ☒ Below ordinary high water mark: Yes ☐ No ☒

State Project: ☐ LPA: ☒

Any other factors influencing recommendations: N/A

INFRASTRUCTURE TABLE AND SUMMARY

Infrastructure Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Religious Facilities	3*	Recreational Facilities	3
Airports ¹	1	Pipelines	1
Cemeteries	1	Railroads	2
Hospitals	N/A	Trails	6
Schools	1	Managed Lands	1

¹In order to complete the required airport review, a review of public airports within 3.8 miles (20,000 feet) is required.

Explanation:

Religious Facilities: *Although not mapped on the GIS layer, three (3) religious facilities are located within the 0.5 mile search radius according to publicly available satellite imagery. The closest religious facility, Monticello Christian Church, is located within the project area. Coordination with Monticello Christian Church will occur.

Airports: Although not located within the 0.5 mile search radius, one (1) public airport, White County Airport, is located within 3.8 miles (20,000 feet) of the project area. The public airport is located 0.98 mile south of the project area; therefore, early coordination with INDOT Aviation will occur.

Cemeteries: One (1) cemetery is located within the 0.5 mile search radius. The cemetery, Rockrock II, is located 0.08 mile south of the project area. No impact is expected.

Schools: One (1) school is located within the 0.5 mile search radius. The school, Oaklawn Elementary School, is located 0.44 mile northeast of the project area. No impact is expected.

Recreational Facilities: Three (3) recreation facilities are located within the 0.5 mile search radius. The nearest recreation facility, City Park, is located adjacent to the project area. Coordination with Monticello Parks and Recreation will occur.

Pipelines: One (1) pipeline segment is located within the 0.5 mile search radius. The pipeline, a natural gas pipeline owned by Northern Indiana Public Service Company, is located 0.1 mile southeast of the project area. No impact is expected.

Railroads: Two (2) railroad segments are located within the 0.5 mile search radius. Both segments represent an unknown railroad and the nearest segment is located 0.21 mile northeast of the project area. No impact is expected.

Trails: Six (6) trails are located within the 0.5 mile search radius. The closest trail, White County/Monticello Trail, is within the project area. This trail is currently under development. Coordination with the White County Board of Commissioners will occur.

Managed Lands: One (1) managed land is located within the 0.5 mile search radius. The managed land, Monticello Altherr Park, is located 0.15 mile north of the project area. No impact is expected.

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WATER RESOURCES TABLE AND SUMMARY

Water Resources Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
NWI - Points	N/A	Canal Routes - Historic	N/A
Karst Springs	N/A	NWI - Wetlands	5
Canal Structures – Historic	N/A	Lakes	1
NPS NRI Listed	N/A	Floodplain - DFIRM	7
NWI-Lines	N/A	Cave Entrance Density	N/A
IDEM 303d Listed Streams and Lakes (Impaired)	4	Sinkhole Areas	N/A
Rivers and Streams	11	Sinking-Stream Basins	N/A

Explanation:

IDEM 303d Listed Streams and Lakes (Impaired): Four (4) impaired stream segments are located within the 0.5 mile search radius. The nearest stream segment, Tippecanoe River, is 0.07 mile southeast of the project area and is impaired for polychlorinated biphenyls (PCBs) in fish tissue. No impact is expected.

Rivers and Streams: Eleven (11) river and stream segments are located within the 0.5 mile search radius. The nearest stream segment, an unnamed tributary (UNT) to Tippecanoe River, is located 0.04 mile east of the project area. No impact is expected.

NWI – Wetlands: Five (5) NWI wetlands are located within the 0.5 mile search radius. The nearest wetland is located 0.06 mile northeast of the project area. No impact is expected.

Lakes: One (1) lake is located within the 0.5 mile search radius. The lake, Lake Freeman, is located 0.08 mile southeast of the project area. No impact is expected.

Floodplain – DFIRM: Seven (7) floodplain polygons are within the 0.5 mile search radius. The nearest floodplain is located 0.05 mile southeast of the project area. No impact is expected.

URBANIZED AREA BOUNDARY SUMMARY

Explanation: This project lies within the Monticello UAB; however, a Rule 13 Permit from IDEM has not been issued. No further coordination is necessary at this time.

MINING AND MINERAL EXPLORATION TABLE AND SUMMARY

Mining/Mineral Exploration			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Petroleum Wells	1	Mineral Resources	N/A
Mines – Surface	N/A	Mines – Underground	N/A

Explanation:

Petroleum Wells: One (1) well is located within the 0.5 mile search radius. The well is located 0.21 mile northeast of the project area. No impact is expected.

HAZARDOUS MATERIAL CONCERNS TABLE AND SUMMARY

Hazardous Material Concerns			
Indicate the number of items of concern found within the 0.5 mile search radius. If there are no items, please indicate N/A:			
Superfund	N/A	Manufactured Gas Plant Sites	N/A
RCRA Generator/ TSD	N/A	Open Dump Waste Sites	N/A
RCRA Corrective Action Sites	N/A	Restricted Waste Sites	N/A
State Cleanup Sites	3	Waste Transfer Stations	N/A
Septage Waste Sites	N/A	Tire Waste Sites	N/A
Underground Storage Tank (UST) Sites	4	Confined Feeding Operations (CFO)	N/A
Voluntary Remediation Program	N/A	Brownfields	N/A
Construction Demolition Waste	N/A	Institutional Controls	N/A
Solid Waste Landfill	N/A	NPDES Facilities	6
Infectious/Medical Waste Sites	N/A	NPDES Pipe Locations	6
Leaking Underground Storage (LUST) Sites	1	Notice of Contamination Sites	N/A

Explanation:

State Cleanup Sites: Three (3) state cleanup sites are located within the 0.5 mile search radius. The nearest site, INDOT Monticello Subdistrict (Agency Interest (AI) ID# 54162), is mapped incorrectly and is actually located 0.35 mile southeast of the project area. According to a review of the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC), the site has received a No Further Action (NFA) letter from IDEM on July 6, 2004. The letter stated that contamination is below acceptable levels. No impact is expected. Another site, Poncies Water Sports (AI ID #55012), is located 0.26 mile north of the project area. According to IDEM VFC, NFA Approval was received by IDEM in July 2019. However, a source of chlorinated solvent contamination was found south of this site and the plume extends south towards the project area. Coordination will be conducted with IDEM before further site activities occur.

Underground Storage Tank (UST) Sites: Four (4) UST sites are mapped within the 0.5 mile search radius. It should be noted at one (1) site, Monticello Wastewater Treatment Plant, is mapped twice. The nearest site, listed as Monticello Shell (AI ID# 54656). The icon for this site is mapped incorrectly; the site is actually within the project area at the northwest corner of Gordon Road and US 421/SR 39. The site is a currently active Sunoco gas station. According to a review of the IDEM VFC, the site is in compliance as of August 2018. No impact is expected.

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An Equal Opportunity Employer*

Leaking UST (LUST) Site: One (1) LUST site is located within the 0.5 mile search radius. The site, Poncies Water Sports (AI ID# 55012), is located 0.26 mile north of the project area. A NFA Request Report was submitted to IDEM in May 2019 and NFA approval was received in July 2019. No impact is expected.

National Pollution Discharge Elimination System (NPDES) Facilities: Six (6) NPDES facilities are located within the 0.5 mile search radius. It should be noted that one (1) site, Monticello Wastewater Treatment Plant, is mapped twice. The nearest facility; Dollar General Monticello, Indiana; is mapped incorrectly and is actually located adjacent to the project area to the west. The site is listed as a construction stormwater permit. No impact is expected.

NPDES Pipe Locations: Six (6) NPDES pipe locations are located within the 0.5 mile search radius. The nearest pipe location, at the Monticello Wastewater Treatment Plant, is located 0.45 mile northeast of the project area. No impact is expected.

ECOLOGICAL INFORMATION SUMMARY

The White County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, or rare (ETR) species and high quality natural communities is attached with ETR species highlighted. A preliminary review of the Indiana Natural Heritage Database by INDOT Environmental Services did not indicate the presence of ETR species within the 0.5 mile search radius.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation for INDOT Projects".

An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the Rusty Patched Bumble Bee, in or within 0.5 mile of the project area. No impact is expected.

RECOMMENDATIONS SECTION

Include recommendations from each section. If there are no recommendations, please indicate N/A:

INFRASTRUCTURE:

Religious Facilities: Monticello Christian Church is within the project area. Coordination with Monticello Christian Church will occur.

Airports: The White County Airport is located 0.98 mile south of the project area. Early coordination with INDOT Aviation will occur.

Recreational Facilities: City Park is located adjacent to the project area. Coordination with Monticello Parks and Recreation will occur.

Trails: White County/Monticello Trail, is within the project area. This trail is currently under development. Coordination with the White County Board of Commissioners will occur.

WATER RESOURCES: N/A

URBANIZED AREA BOUNDARY: N/A

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS:

State Cleanup Sites: Poncies Water Sports (AI ID #55012) is located 0.26 mile north of the project area. According to IDEM VFC, NFA Approval was received by IDEM in July 2019. However, a source of chlorinated solvent contamination was found south of this site and the plume extends south towards the project area. Coordination will be conducted with IDEM before further site activities occur.

ECOLOGICAL INFORMATION: Coordination with USFWS and IDNR will occur. The range-wide programmatic consultation for the Indiana Bat and Northern Long-eared Bat will be completed according to the most recent "Using the USFWS's IPaC System for Listed Bat Consultation INDOT Projects".

Nicole Fohey-
Breting

Digitally signed by
Nicole Fohey-Breting
Date: 2019.09.12
15:46:12 -04'00'

INDOT Environmental Services concurrence: _____ (Signature)

Prepared by:



Angela Kattmann, LPG
Environmental Geologist
Lochmueller Group

Graphics:

A map for each report section with a 0.5 mile search radius buffer around all project area(s) showing all items identified as possible items of concern is attached. If there is not a section map included, please change the YES to N/A:

SITE LOCATION: YES

INFRASTRUCTURE: YES

WATER RESOURCES: YES

URBANIZED AREA BOUNDARY: YES

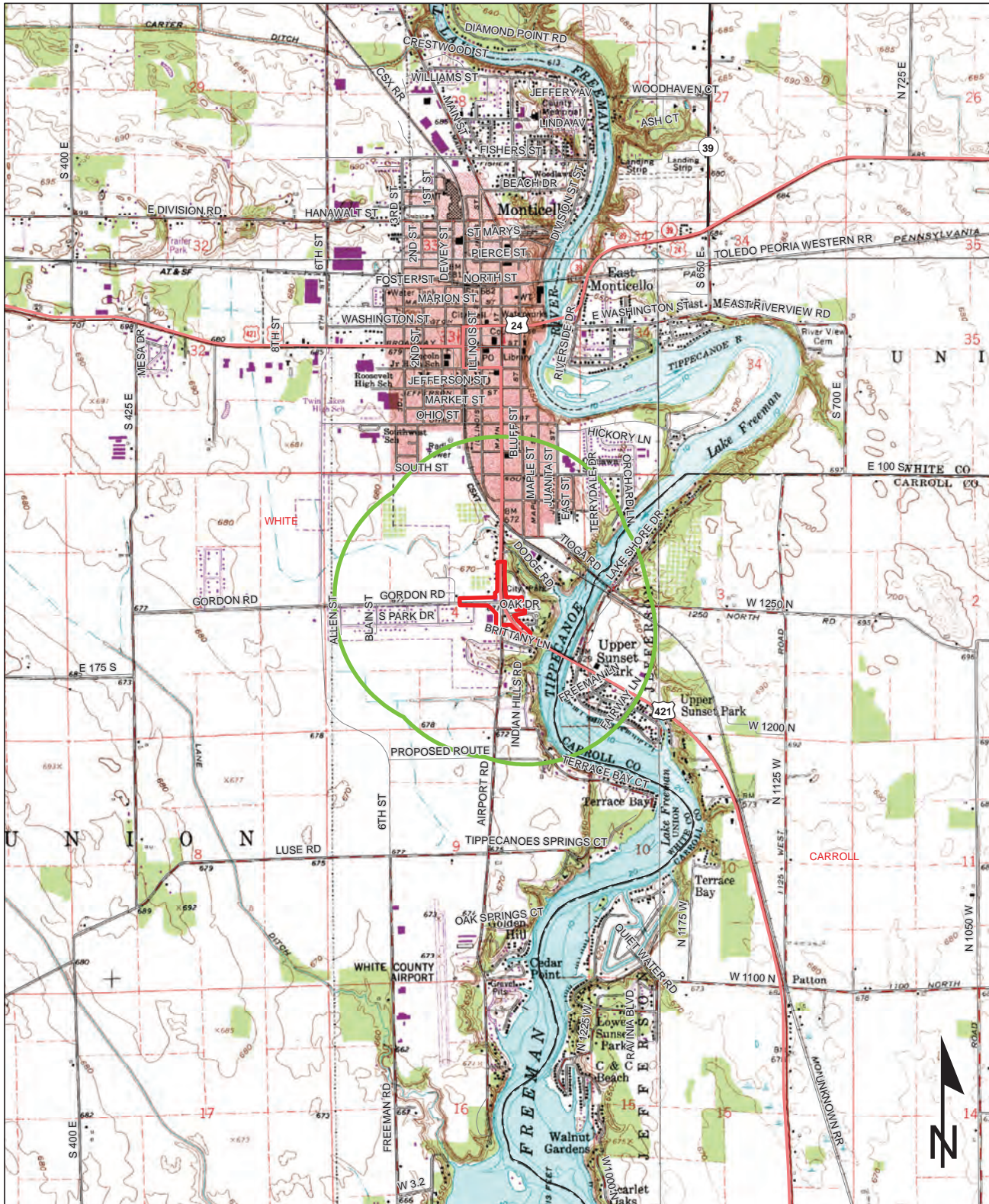
MINING/MINERAL EXPLORATION: YES

HAZMAT CONCERNS: YES

Additional Attachments:

WHITE COUNTY ETR LIST

Red Flag Investigation - Site Location
Gordon Road at US 421/SR 39
Des. No. 1702835, Intersection Improvement
White County, Indiana



Sources: 0.5 0.25 0 0.5 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

**MONTICELLO SOUTH
 QUADRANGLE
 INDIANA 7.5 MINUTE SERIES
 (TOPOGRAPHIC)**

Red Flag Investigation - Infrastructure

Gordon Road at US 421/SR 39

Des. No. 1702835, Intersection Improvement

White County, Indiana



Sources: 0.15 0.075 0 0.15 Miles
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83
 This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Toll
	Hospital		Trails		Interstate
	School		Managed Lands		State Route
			County Boundary		US Route
					Local Road

Red Flag Investigation - Water Resources

Gordon Road at US 421/SR 39

Des. No. 1702835, Intersection Improvement

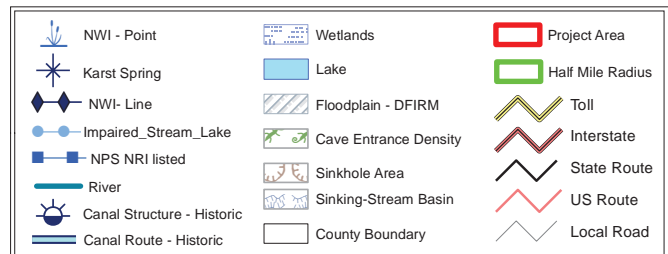
White County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

0.15 0.075 0 0.15 Miles



Red Flag Investigation - Urbanized Area Boundary

Gordon Road at US 421/SR 39

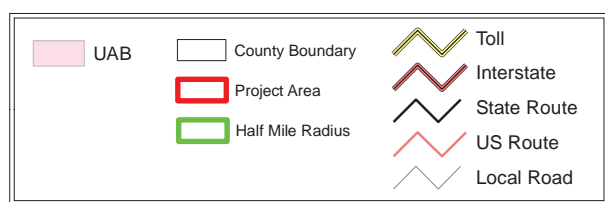
Des. No. 1702835, Intersection Improvement

White County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Red Flag Investigation - Mining/Mineral Resources

Gordon Road at US 421/SR 39

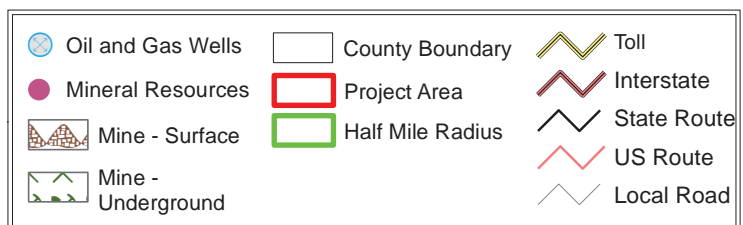
Des. No. 1702835, Intersection Improvement

White County, Indiana



Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

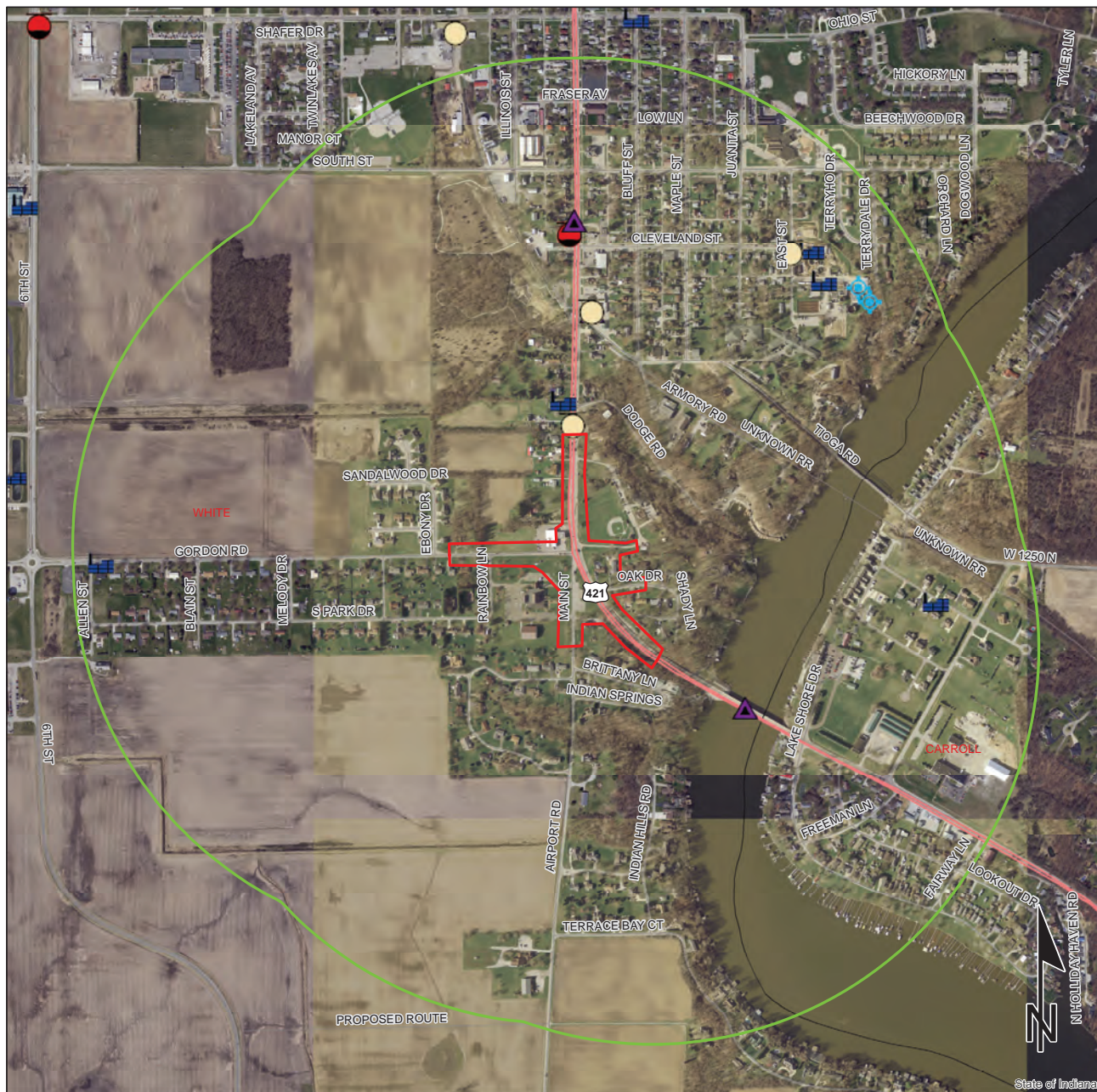


Red Flag Investigation - Hazardous Material Concerns

Gordon Road at US 421/SR 39

Des. No. 1702835, Intersection Improvement

White County, Indiana



	Brownfield		RCRA Generator/TSD		Institutional Controls
	RCRA Corrective Action Sites		Restricted Waste Site		County Boundary
	Confined Feeding Operation		Septage Waste Site		Project Area
	Notice Of Contamination		Solid Waste Landfill		Half Mile Radius
	Construction/Demolition Site		State Cleanup Site		Toll
	Infectious/Medical Waste Site		Superfund		Interstate
	Leaking Underground Storage Tank		Tire Waste Site		State Route
	Manufactured Gas Plant		Underground Storage Tank		US Route
	NPDES Facilities		Voluntary Remediation Program		Local Road
	NPDES Pipe Locations		Waste Transfer Station		
	Open Dump Waste Site				

0.15 0.075 0 0.15
Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Des. No. 1702835

Appendix E: Red Flag Investigation & Hazardous Materials

Sources:
Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

E12

Indiana County Endangered, Threatened and Rare Species List

County: White

Species Name	Common Name	FED	STATE	GRANK	SRANK
Crustacean: Ostracoda					
ÄÄ !"# \$%&'&%(%&& ý*	Jeannel's Cave Ostracod		SE	G2	S1
Mollusk: Bivalvia (Mussels)					
+, -.# / &*%'ÄÖ %. *%	Eastern Fanshell Pearlymussel	LE	SE	G1Q	S1
1-#2)%Ä3%'0#.!)#Ä%' .%&%&%&%	Northern Riffleshell	LE	SE	G2T2	S1
1-#2)%Ä3%'0. *4! 0. %	Snuffbox	LE	SE	G3	S1
5!Ä\$#&%&'Ä!2. #0 &"% %	Longsolid	C	SE	G3	SX
6%3-Ä*)*Ä'7%Ä\$*#)%	Wavyrayed Lampmussel		SSC	G5	S3
8 2#9%. *%'Ä!2. #0!&" %	Round Hickorynut	C	SE	G4	S1
Ä) 0:#2%Ä!Ä\$ - . , !Ä	Sheepnose	LE	SE	G3	S1
Ä)! .#2 3 %\$)%&%	Clubshell	LE	SE	G1G2	S1
Ä) !.#2 3%'\$#. "%0!3	Ohio Pigtoe		SSC	G4	S2
Ä) !.#2 3%' - . , %3'" "%0!3	Pyramid Pigtoe		SE	G2G3	SX
Ä0,\$:#2. %&\$:!Ä'7%Ä\$*#)% . *Ä	Kidneyshell		SSC	G4G5	S2
; !%".!)%'\$,) * &". *\$%'\$)* &". *\$ %	Rabbitsfoot	LT	SE	G3G4T3	S1
<*3-Ä#&%*%'Ä'%32*!% %	Salamander Mussel	C	SSC	G3	S2
=#>#)%Ä3%') *9*"!Ä	Purple Lilliput	C	SSC	G3Q	S2
?*)#Ä%7%2%&%) *Ä	Rayed Bean	LE	SE	G2	S1
Insect: Lepidoptera (Butterflies & Moths)					
@#/#. %Ä) & ' & 2. %Ä &Ä*Ä	The Nebraska Silver Bordered Fritillary		SE	G5T3T4	S1
1!:- . , Ä'2*3%\$!)%	Two-spotted Skipper		ST	G4	S2
6 0: ' !. , ""\$ ' 73#Ä!Ä	Smoky-eyed Brown		ST	G5T3T4	S1S2
Insect: Odonata (Dragonflies & Damselflies)					
1&%))!&3%" ""9% %&ÄÄ	Turquoise Bluet		SR	G5	S3
Fish					
10: #Ä0#3%'0*-- \$%&#	Tippecanoe Darter	C	SSC	G3G4	S3
Ä . \$*&% 9*Ä	Gilt Darter		SE	G4	S1
Amphibian					
6*0:#2%0 Ä'-*.* &Ä	Northern Leopard Frog		SSC	G5	S2
Reptile					
+) 33 , Ä/ !00%0%	Spotted Turtle	C	SE	G5	S2
13 , "#** %'2)%&"* & *	Blanding's Turtle	C	SE	G4	S2
B*&#Ä0 .&#&'Ä!2. !2 .! 3' Ä!2. !2 .! 3	Eastern Mud Turtle		SE	G5T5	S2
= ..%- & '#.&%0%#.&%0%	Ornate Box Turtle		SE	G5T5	S1
Bird					
C33 #".%& !Ä': &Ä#D*	Henslow's Sparrow		SE	G4	S3B
@%.0. %3*%)#&%!"%Ä"	Upland Sandpiper		SE	G5	S3B
+ :)*"#&*%Ä'&*	Black Tern		SE	G4G5	S1B
+ *. \$!Ä':!"Ä#&*!Ä	Northern Harrier		SE	G5	S2

Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Indiana County Endangered, Threatened and Rare Species List

County: White

Species Name	Common Name	FED	STATE	GRANK	SRANK
+*Ä0#0:#.!Ä'-%)!Ä0.*Ä	Marsh Wren		SE	G5	S3B
+*Ä0#0:#.!Ä'-)%0 &Ä*Ä	Sedge Wren		SE	G5	S3B
E%)*% 0!Ä') !\$#\$ -:%)!Ä	Bald Eagle		SSC	G5	S2
6%&!A')!"# 9*\$%&!A Ä	Loggerhead Shrike		SE	G4	S3B
F, \$0*\$#. %>,\$0*\$#. %>	Black-crowned Night-heron		SE	G5	S1B
Mammal					
G #3,Ä'2!.Ä%. *!Ä	Plains Pocket Gopher		SSC	G5	S2
<- .3#-:*)!Ä'7.%&A)*&*	Franklin's Ground Squirrel		SE	G5	S2
=%>*"" %'0%>!Ä	American Badger		SSC	G5	S2
Vascular Plant					
@ .2 .*A\$%&%" &A*A Ä	American Barberry		SE	G3	S1
@ ÄÄ, %'2!))**	Kitten Tails		SE	G3	S1
+ %3%ÄÄ*%PÄ0%0%	Wild Hyacinth		SE	G5?Q	S1
+ %. >\$#&#"" %	Prairie Gray Sedge		ST	G5	S1
+ %. >' 2!.& %	Ebony Sedge		SR	G5	S2
+ %. >'Ä0.%3*& %	Straw Sedge		ST	G5	S2
+ *.Ä*!3':.))*	Hill's Thistle		SE	G3	S1
+ .%0% !Ä'- ""*\$))%0%	Scarlet Hawthorn		ST	G5	S2
1) #\$: %*AD#)7*"	Wolf Spikerush		SR	G3G5	S2
1!. , 2*%7. \$.%0%	Forked Aster		SR	G3	S2
G &0*%&%'-!2 .!) &0%	Downy Gentian		ST	G4G5	S2
H)%3- , !3')*& %.	American Cow-wheat		SR	G5	S2
H)%&0:*!3'9*./ *\$&\$!3	Virginia Bunchflower		SE	G5	S1
8 �: .%'-. &&*Ä	Small Sundrops		SR	G5	S2
8 ., !#-Ä*Ä'. %\$ 3#Ä%	Black-fruit Mountain-ricegrass		SR	G5	S2
Ä%&\$!3') *2 . / *	Leiberg's Witchgrass		ST	G4	S2
Ä)%0%&0: .%'! !\$#-:% %	Prairie White-fringed Orchid	LT	SE	G2G3	S1
Ä#), 0% &*%&!00%)))**	Prairie Parsley		SE	G5	S1
Ä. &%&0: Ä'%Ä- .%	Rough Rattlesnake-root		SR	G4?	S2
<\$!0))%. *%'-%.9!)%'9%.J'%!Ä0.)*Ä	Southern Skullcap		WL	G4T4?	S2
?*#)%'- ""07""%	Prairie Violet		ST	G5	S2
High Quality Natural Community					
Ä. %*.* 'K"".,K3 Ä*\$	Dry-mesic Prairie		SG	G3	S2
Ä. %*.* 'K'3 Ä*\$	Mesic Prairie		SG	G2	S2
Ä. %*.* 'K'Ä%&""3 Ä*\$	Mesic Sand Prairie		SG	GNR	SNR
Ä. %*.* 'K'Ä%&""D 0	Wet Sand Prairie		SG	G3	S3
Ä. %*.* 'K'Ä%&""D 0K3 Ä*\$	Wet-mesic Sand Prairie		SG	G1?	S2
<%9%&&%'K'Ä%&"".,	Dry Sand Savanna		SG	G2?	S2

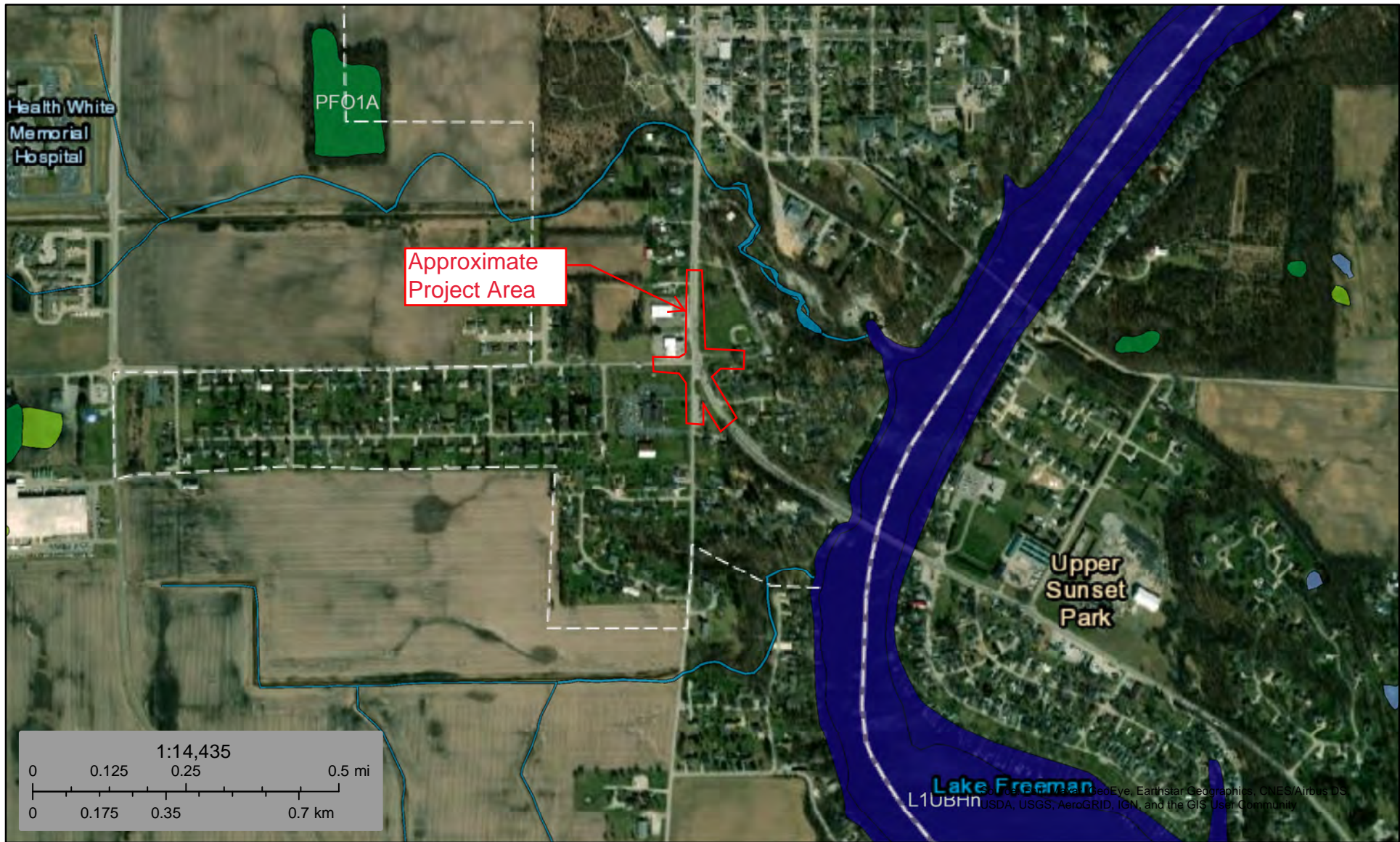
Indiana Natural Heritage Data Center
Division of Nature Preserves
Indiana Department of Natural Resources
This data is not the result of comprehensive county surveys.

Fed: LE = Endangered; LT = Threatened; C = candidate; PDL = proposed for delisting
State: SE = state endangered; ST = state threatened; SR = state rare; SSC = state species of special concern; SX = state extirpated; SG = state significant; WL = watch list
GRANK: Global Heritage Rank: G1 = critically imperiled globally; G2 = imperiled globally; G3 = rare or uncommon globally; G4 = widespread and abundant globally but with long term concerns; G5 = widespread and abundant globally; G? = unranked; GX = extinct; Q = uncertain rank; T = taxonomic subunit rank
SRANK: State Heritage Rank: S1 = critically imperiled in state; S2 = imperiled in state; S3 = rare or uncommon in state; G4 = widespread and abundant in state but with long term concern; SG = state significant; SH = historical in state; SX = state extirpated; B = breeding status; S? = unranked; SNR = unranked; SNA = nonbreeding status unranked

Categorical Exclusion

Appendix F

Water Resources



February 17, 2021

Wetlands

- Estuarine and Marine Deepwater
- Estuarine and Marine Wetland

- Freshwater Emergent Wetland
- Freshwater Forested/Shrub Wetland
- Freshwater Pond
- Lake
- Other
- Riverine

This map is for general reference only. The US Fish and Wildlife Service is not responsible for the accuracy or currentness of the base data shown on this map. All wetlands related data should be used in accordance with the layer metadata found on the Wetlands Mapper web site.

National Flood Hazard Layer FIRMette



86°46'2"W 40°44'1"N



Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
OTHER FEATURES		Levee, Dike, or Floodwall
		Cross Sections with 1% Annual Chance Water Surface Elevation
MAP PANELS		Coastal Transect
		Base Flood Elevation Line (BFE)
OTHER FEATURES		Limit of Study
		Jurisdiction Boundary
OTHER FEATURES		Coastal Transect Baseline
		Profile Baseline
OTHER FEATURES		Hydrographic Feature
		Digital Data Available
MAP PANELS		No Digital Data Available
		Unmapped



The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 2/17/2021 at 5:08 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

Categorical Exclusion

Appendix G

Public Involvement

March 8, 2019

SAMPLE NOS LETTER

NOTICE OF SURVEY

Dear Property Owner:

Banning will be conducting a survey on or near your property on Gordon Road or US421/SR39 as part of a White County Project (Des.No. 1702835) over the next several months. You've possibly already seen our crews working within or near the road right of way lines. The survey will possibly require our personnel to enter your property to conduct the survey. Access by our surveyors is permitted by law per Indiana Code IC 25-21.5-9-7. If we need to make special arrangements for access to your property, please contact me for such arrangements. Stakes may be placed on your property in order to conduct the survey.

Banning Engineering's employees will be in marked trucks and wearing safety vests with Banning Engineering's logo on the back. If you have sold this property, or it is occupied by someone else, please let us know the name and address of the new owner or current occupant so that we can contact them about this survey. If you have questions or concerns, please contact Banning Engineering or Lochmueller Group to discuss.

Sincerely,

A handwritten signature in blue ink, appearing to read "Brian L. Haggard".

Brian L. Haggard, PS
Vice President
Banning Engineering
317-707-3719
bhaggard@banning-eng.com

A handwritten signature in blue ink, appearing to read "Steve Fleming".

Steve Fleming, PE
Vice President of Transportation
Lochmueller Group
317-334-6829
SFleming@lochgroup.com

Categorical Exclusion

Appendix H

Air Quality

Indiana Department of Transportation (IND T)
State Preservation and Local Initiated Projects FY 2020 - 2024

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
Indiana Department of Transportation	39804 / 1383600	A 03	US 24	Road Rehabilitation (3 R/4R Standards)	From 0.71 mi W of US 421/SR 43 to 0.68 mi E or US 421/SR 43	LaPorte	1.349	NHPP	\$4,141,017.00	Road ROW	RW	\$800,000.00	\$200,000.00	\$1,000,000.00				
Comments:Please amend FY20 RW into the STIP. No MPO.																		
Indiana Department of Transportation	40114 / 1383716	Init.	US 421	Bridge Deck Overlay	Bridge over Honey Creek, 2.53 mi N of US 24	LaPorte	0	NHPP		Bridge Construction	CN	\$779,041.60	\$194,760.40	\$973,802.00				
										Bridge Consulting	PE	\$56,000.00	\$14,000.00	\$70,000.00				
Indiana Department of Transportation	40114 / 1383716	M 08	US 421	Bridge Deck Overlay	Bridge over Honey Creek, 2.53 mi N of US 24	LaPorte	0	NHPP	\$917,410.00	Bridge Construction	CN	\$44,827.20	\$11,206.80	(\$861,376.00)	\$917,410.00			
Comments:No MPO. Modify CN from FY20 to FY21. Was not awardable on the 4/1/20 (FY20) letting moved to 7/8/20 (FY21) for re-let. This contract has 3 total DES#'s.																		
Monticello	40179 / 1600938	Init.	ST 1013	Bike/Pedestrian Facilities	S Main Street	LaPorte	.319	STPBG		Local Funds	CN	\$0.00	\$84,095.97		\$84,095.97			
										Local Transportation Alternatives	CN	\$336,383.90	\$0.00		\$336,383.90			
Indiana Department of Transportation	40527 / 1701544	Init.	I 65	Small Structure Pipe Lining	Over Unnamed Ditch/Creek; 0.5 9 mi N of County Line	Crawfordsville	0	NHPP		Bridge Construction	CN	\$1,338,898.50	\$148,766.50	\$1,487,665.00				
Indiana Department of Transportation	40957 / 1800447	Init.	I 65	Small Structure Pipe Lining	2.84 mi S of SR 18	Crawfordsville	0	NHPP		Bridge Construction	CN	\$1,272,335.40	\$141,370.60		\$1,413,706.00			
										Bridge ROW	RW	\$23,400.00	\$2,600.00	\$26,000.00				
White County	41168 / 1600927	Init.	IR 1001	Bridge Rehabilitation Or Repair	Bridge 95 over Little Monon Creek on CR 300 E appr 0.35 mi S of Monon Rd	LaPorte	.135	STPBG		Group IV Program	RW	\$60,000.00	\$0.00	\$60,000.00				
										Local Funds	RW	\$0.00	\$15,000.00	\$15,000.00				
										Local Funds	CN	\$0.00	\$251,000.00			\$251,000.00		
										Local Bridge Program	CN	\$1,004,000.00	\$0.00			\$1,004,000.00		
White County	41168 / 1600927	M 09	IR 1001	Bridge Rehabilitation Or Repair	Bridge 95 over Little Monon Creek on CR 300 E appr 0.35 mi S of Monon Rd	LaPorte	.135	STBG	\$1,282,180.00	Local Funds	RW	\$0.00	\$0.00	(\$5,436.00)	\$5,436.00			
										Local Bridge Program	RW	\$0.00	\$0.00	(\$21,744.00)	\$21,744.00			
Comments:No MPO RW funds moving to FY21 in the amount of \$27,280.00. Funding program changing from Group 4 to Local Bridge																		
White County	41180 / 1702835	Init.	ST 1009	Intersection Improvement, Roundabout	Intersection of Gordon Road and US 421/S.R. 39 in Monticello, White County, IN	LaPorte	.07	STPBG		Group IV Program	RW	\$32,000.00	\$0.00	\$32,000.00				
										Group IV Program	CN	\$2,222,016.00	\$0.00				\$2,222,016.00	

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

SPONSOR	CONTR ACT # / LEAD DES	STIP NAME	ROUTE	WORK TYPE	LOCATION	DISTRICT	MILES	FEDERAL CATEGORY	Estimated Cost left to Complete Project*	PROGRAM	PHASE	FEDERAL	MATCH	2020	2021	2022	2023	2024
White County	41180 / 1702835	Init.	ST 1009	Intersection Improvement, Roundabout	Intersection of Gordon Road and US 421/S.R. 39 in Monticello, White County, IN	LaPorte	.07	STPBG		Local Funds	RW	\$0.00	\$8,000.00	\$8,000.00				
										Local Funds	CN	\$0.00	\$555,504.00				\$555,504.00	
Indiana Department of Transportation	41198 / 1800777	Init.	SR 16	Bridge Painting	@.-TIPPECANOE-E XING, 0.02m i W of SR 39 E JCT	LaPorte	0	STPBG		Bridge Construction	CN	\$2,183,411.20	\$545,852.80	\$40,000.00	\$2,689,264.00			
Indiana Department of Transportation	41198 / 1800777	A 03	SR 16	Bridge Painting	@.-TIPPECANOE-E XING, 0.02m i W of SR 39 E JCT	LaPorte	0	STBG	\$366,464.00	Bridge Consulting	PE	\$344,521.60	\$86,130.40	\$411,152.00	\$19,500.00			
Comments:Please amend FY20																		
Indiana Department of Transportation	41425 / 1703007	Init.	SR 39	Bridge Replacement, Concrete	Sr 39@.-Harp Ditch #1, 01.82 N SR 16	LaPorte	0	STPBG		Bridge Construction	CN	\$620,256.00	\$155,064.00			\$20,000.00	\$755,320.00	
										Bridge Consulting	PE	\$63,360.00	\$15,840.00	\$79,200.00				
										Bridge ROW	RW	\$24,000.00	\$6,000.00			\$30,000.00		
White County	42062 / 1802935	A 04	IR 5220	Bridge Deck Replacement	Lowes Road Bridge over Lake Shafer	LaPorte	.152	STBG	\$2,266,000.00	Local Funds	CN	\$0.00	\$390,600.00					\$390,600.00
										Local Bridge Program	CN	\$1,562,400.00	\$0.00					\$1,562,400.00
Comments:No MPO add CN funds in FY24																		
White County	42062 / 1802935	A 07	IR 5220	Bridge Deck Replacement	Lowes Road Bridge over Lake Shafer	LaPorte	.152	STBG	\$2,266,000.00	Local Funds	PE	\$0.00	\$62,600.00	\$62,600.00				
										Local Bridge Program	PE	\$250,400.00	\$0.00	\$250,400.00				
Comments:Add PE Phase for \$250,400.00 FY20, No MPO																		
White County	42062 / 1802935	A 10	IR 5220	Bridge Deck Replacement	Lowes Road Bridge over Lake Shafer	LaPorte	.152	STBG	\$2,266,000.00	Local Funds	PE	\$0.00	\$62,600.00	\$62,600.00				
										Local Bridge Program	PE	\$250,400.00	\$0.00	\$250,400.00				
Comments:No MPO- Add PE FY20 Federal 250,400.00 and local \$62,600.00																		
White County	42062 / 1802935	M 04	IR 5220	Bridge Deck Replacement	Lowes Road Bridge over Lake Shafer	LaPorte	.152	STBG	\$2,237,100.00	Local Funds	PE	\$0.00	-\$5,780.00	(\$5,780.00)				
										Local Bridge Program	PE	-\$23,120.00	\$0.00	(\$23,120.00)				
Comments:No MPO-Decreasing PE funds in FY20 in the amount of \$23,120.00 federal and \$5,780.00 local.																		
Indiana Department of Transportation	42222 / 1901360	A 01	US 24	HMA Overlay, Preventive Maintenance	0.68mi E of US 421/SR 43 to 2. 65mi W of SR 39 (CR 300E)	LaPorte	2.61	NHPP	\$1,470,894.00	Bridge Construction	CN	\$140,546.40	\$35,136.60			\$175,683.00		
										Bridge Consulting	PE	\$105,700.00	\$26,425.00	\$132,125.00				

*Estimated Costs left to Complete Project column is for costs that may extend beyond the four years of a STIP. This column is not fiscally constrained and is for information purposes.

Categorical Exclusion

Appendix I

Other Information

Land and Water Conservation Fund (LWCF) County Property List for Indiana (Last Updated July 2020)

ProjectNumber	SubProjectCode	County	Property
1800574	1800574	White	Altherr Park
1800605	1800605	White	Altherr Park
1800633	1800633	White	Monon Park

*Park names may have changed. If acquisition of publically owned land or impacts to publically owned land is anticipated, coordination with IDNR, Division of Outdoor Recreation, should occur.



MONTICELLO PARKS & RECREATION

225 N. Main St.
Monticello, IN 47960
(574) 583-3983



October 6, 2020

Chris Kunkel
Lochmueller Group, Inc.
3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268

Re: Section 4(f) Coordination
Des. No. 1702835
Gordon Road and S. Main Street (US 421/SR 39)
Intersection Improvement Project
City of Monticello, White County, Indiana

Dear Mr. Kunkel:

I understand that the intersection improvement project at Gordon Road, S. Main St. (US 421/SR 39), Airport Road, and City Park Loop will affect Monticello City Park, a park owned and maintained by the Monticello City Parks Department. It is my understanding that in order to improve the intersection by constructing a roundabout and construct a sidewalk, approximately 0.68 acre of permanent right-of-way will be acquired from Monticello City Park land.

Information regarding the proposed project and aerial photographs illustrating the proposed acquisition were provided to us by Lochmueller Group on January 30, 2020. Based on a review of this information, Monticello City Parks Department understands that this acquisition is necessary and in the interest of the public. As the official with jurisdiction over Monticello City Park, I agree that this constitutes a *de minimis* Section 4(f) impact, as described in the Federal Highway Administration's *Section 4(f) Policy Paper (dated July 20, 2012)*, for the following reasons:

- Ownership and management of the park property will be retained by the Monticello City Parks Department.
- Upon completion of the project, the park property will be returned to a condition that is at least as good as that which existed prior to the project.

In the end, the proposed project will improve the traffic flow at the intersection and provide pedestrian access to the park property. Therefore, I agree the project will not adversely affect the recreational activities, features, and attributes that qualify Monticello City Park for protection under Section 4(f) of the US Department of Transportation Act.

Respectfully,

Mr. Mitchell E. Billue, Park Superintendent
Monticello City Parks Department

PUBLIC NOTICE

The White County Board of Commissioners is planning to undertake an intersection improvement project funded in part by the Federal Highway Administration (FHWA). The project is located at the intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County. The project will involve the construction of a roundabout at this five-legged intersection with added pedestrian facilities that connect to existing sidewalks and recreational properties.

The project will require the acquisition of approximately 0.68 acre of property belonging to the City of Monticello Parks Department. The Monticello City Park resides on this property and included with this property are recreational facilities that are open to the public. The designed action will not adversely impact the activities, features, and attributes that qualify the property for protection under Section 4(f) of the Department of Transportation Act of 1966 and in accordance with SAFETEA-LU Section 6009 (a). As such, it is the intent of the FHWA to issue a finding of de minimis. In accordance with SAFETEA-LU Section 6009 (a), the views of the public are being sought regarding the effect of the proposed project on the public recreational area. Please respond with any comments no later than December 4, 2020 to the contact information below.

Chris Kunkel,
Environmental Specialist
Lochmueller Group
3502 Woodview Trace, Ste 150
Indianapolis, IN 46268
ckunkel@lochgroup.com

hspaxlp

PROOF OF PUBLICATION

STATE OF INDIANA)
COUNTY OF WHITE)

HERALD JOURNAL
114 S Main Street
Monticello, IN 47960

Chris Kunkle
Lochmueller Group
3502 Woodview Trace, Suite 150
Monticello, IN 47960

Legal# 20-236/HJ DES# 1702835

Publisher's Fee: \$20.39

(ATTACH COPY OF AD)

The undersigned, Brenda Grandstaff, Legal Advertising Clerk of THE MONTICELLO HERALD JOURNAL, a weekly newspaper of general circulation, printed in the English language and published in Monticello, Indiana in said county; does hereby certify that the advertisement attached hereto is a true copy, which was duly published in said paper for:

1 week(s). The first publication was on 4th
November 2020

In addition, this News paper has a website and this public notice was posted on the same day as it was published in the newspaper.

Dated this 4th day of November 2020

Brenda Grandstaff

Brenda Grandstaff, Legal Advertising Clerk

Des. No. 1702835: Gordon Road Intersection Improvement Project
EJ Analysis

April 5, 2021

The Federal Highway Administration (FHWA) and the White County Board of Commissioners propose to proceed with an intersection improvement project in Monticello, White County, Indiana.

Project Location

The proposed project would involve the construction of a roundabout at the five-legged intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop in the City of Monticello, White County. Specifically, the project is located in Section 4, Township 26 North, Range 3 West of Union Township as depicted on the Monticello South U.S. Geological Survey 1:24,000 scale quadrangle. Adjacent land use consists of residential development, commercial properties, as well as recreational and religious facilities.

Purpose and Need

The need for this project stems from the poor level of service (LOS) for the five-legged intersection. There are six LOS levels ranging from LOS A (free flow) to LOS F (oversaturated). LOS C is considered acceptable. Currently, according to a traffic study performed by Lochmueller Group in January 2020, the LOS for the intersection overall is LOS C during both AM and PM peak hours. All approaches exhibit LOS C or better with the exception of the eastbound approach (Gordon Road) which exhibits LOS D during the AM peak hours and LOS E at PM peak hours. The design year (2042) LOS for the current signalized intersection is forecasted to be LOS F, which as stated above is oversaturated and considered unacceptable. A secondary need of the project is the lack of connectivity in pedestrian facilities in the area. The purpose of the proposed project is to improve the LOS of each of the approaches to a minimum of LOS C and improve the connectivity for pedestrian facilities.

Project Description (Preferred Alternative)

The proposed project will involve the construction of a roundabout at the intersection of S. Main Street (US 421/SR 39), Airport Road, Gordon Road, and City Park Loop to assist in facilitating traffic flow at the intersection. Additionally, the project will extend the existing Gordon Road multi-use trail east to Monticello City Park and a sidewalk will extend from Monticello City park north along US 421/Main Street to the north entrance of the park. The alignment of the trail will be shifted to accommodate the new roundabout. The project will also include updates to the curb and curbside drainage along the roadways and the addition of Americans with Disabilities Act (ADA) compliant curb ramps and lights at the intersection. Additionally, this project will likely require updates to the storm sewer system at the intersection. Wayfinding signage will also be included. The total length, including all work along the approaches, is approximately 7,392 feet (1.4 miles).

Environmental Justice Analysis

An Environmental Justice (EJ) analysis is required for any project requiring two or more relocations or more than 0.5 acre of new permanent right-of-way. Because the project is expected to require more than 0.5 acre of new permanent right-of-way (approximately 1.61 acres), an EJ analysis was conducted.

Potential EJ impacts are detected by locating minority populations and low-income populations in and near the project area, calculating their percentage in the area relative to a reference population to determine if, in fact, populations of EJ concern do exist, and determining whether there will be disproportionate adverse

impacts to them. The reference population may be a county, city, or town and is called the community of comparison (COC). For this project the COC is White County, Indiana. The community that overlaps the project limits is called the affected community (AC). For this project there are two AC's. AC 1 is Census Tract 9585 and AC 2 is Census Tract 9586.

An AC has a population of concern for EJ if the population is more than 50% low-income or minority or if the low-income population or minority population is greater than 125% of the population in the COC.

	COC White County, Indiana	AC 1 Census Tract 9585	AC 2 Census Tract 9586
LOW-INCOME POPULATION			
Total Population for Whom Poverty Status is Determined	23,801	3,588	4,805
Total Population Below Poverty Level	2,198	440	546
Percent Low-Income	9.2%	12.3%	11.4%
125 Percent of COC	11.5%		
AC Percent Low-Income Greater Than 125 Percent of COC?		Yes	No
AC Percent Low-Income Greater Than 50 Percent?		No	No
Population of EJ Concern?		Yes	No
MINORITY POPULATION			
Total Population	24,149	3,691	4,976
Minority Population	2,577	250	754
Percent Minority	10.7%	6.8%	15.2%
125 Percent of COC	13.3%		
AC Percent Minority Greater Than 125 Percent of COC?		No	Yes
AC Percent Minority Greater Than 50 Percent?		No	No
Population of EJ Concern?		No	Yes

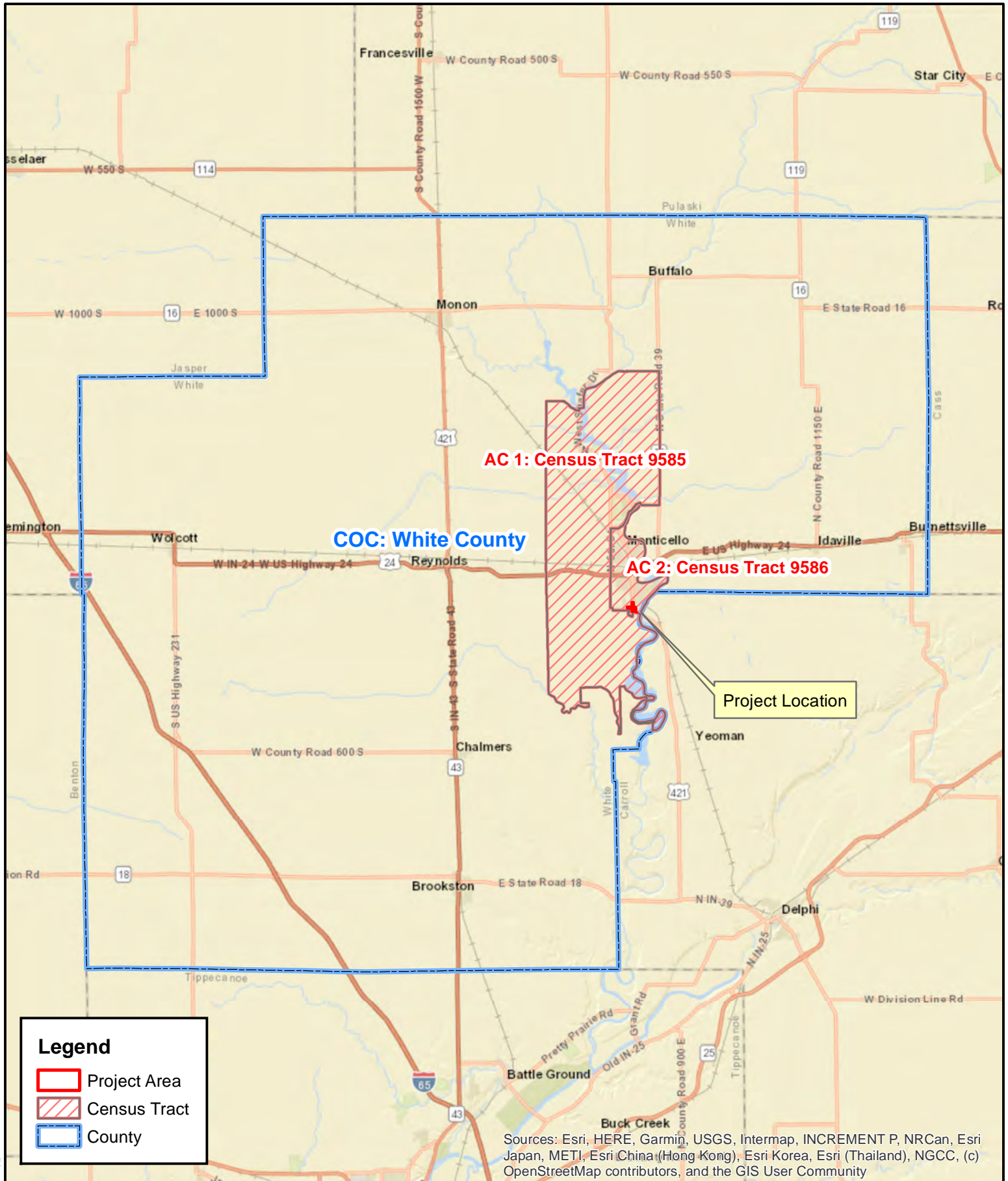
A review of American Community Survey five-year estimates data (2015-2019) was completed on February 18, 2021. The data was obtained from the U.S. Census Bureau's webpage (<https://data.census.gov/cedsci/>).

A review of the data revealed that the AC 2 did not contain a minority population greater than or equal to 50%; however, it does contain a population greater than 125% of the COC minority population. Therefore, minority populations of EJ concern are present within the project area. The data for low-income populations determined the AC 1 was not greater than or equal to 50%; however, it was greater than 125% of the COC. Therefore, low-income populations of EJ concern are present within the project area.

The proposed project is expected to require the acquisition of approximately 1.61 acres of permanent ROW and 0.46 acre of temporary ROW. Land use within the proposed permanent ROW consists of residential development, commercial properties, as well as recreational and religious facilities. No relocations are anticipated.

Overall, the negative impacts to property owners within the project area will be minimal and consist primarily of short-term construction impacts and the loss of a strip of ROW. No relocations are anticipated.

The ROW to be acquired will not substantially diminish the existing use of it by the affected property owners. The maintenance of traffic (MOT) for this project will occur in phases to ensure motorists have access through the intersection throughout construction. Access will be maintained for property owners during construction of the project. Impacts from the project to any EJ community in this area will be beneficial due to the improved traffic flow at the intersection and the improved pedestrian connectivity in Monticello. It is expected the project will not have a disproportionately high and adverse environmental or health impact to low-income or minority populations of EJ concern when compared to non EJ populations.



LOCHMUELLER GROUP

3502 Woodview Trace, Suite 150
 Indianapolis, IN 46268
 Phone: (317) 222-3880
 Fax: (317) 222-3881

EJ Analysis Map

Des. No. 1702835

0 2.5 5
 Miles

North Arrow

County: White
 Township: Union
 State: Indiana

Gordon Road Intersection Improvement
 Gordon Road and US 421/SR 39
 Created: 2/18/2021, C. Kunkel

HISPANIC OR LATINO ORIGIN BY RACE



Note: This is a modified view of the original table produced by the U.S. Census Bureau. This download or printed version may have missing information from the original table.

	White County, Indiana	Census Tract 9585, White County, Indiana	Census Tract 9586, White County, Indiana
Label	Estimate	Estimate	Estimate
▼ Total:	24,149	3,691	4,976
▼ Not Hispanic or Latino:	22,128	3,573	4,350
White alone	21,572	3,441	4,222
Black or African American alone	114	32	5
American Indian and Alaska Native alone	80	80	0
Asian alone	55	20	0
Native Hawaiian and Other Pacific Islander alone	0	0	0
Some other race alone	0	0	0
▼ Two or more races:	307	0	123
Two races including Some other race	1	0	0
Two races excluding Some other race, and three or more races	306	0	123
▼ Hispanic or Latino:	2,021	118	626
White alone	383	53	129
Black or African American alone	9	9	0
American Indian and Alaska Native alone	4	0	0
Asian alone	0	0	0
Native Hawaiian and Other Pacific Islander alone	10	0	0
Some other race alone	1,518	16	477
▼ Two or more races:	97	40	20
Two races including Some other race	69	40	20
Two races excluding Some other race, and three or more races	28	0	0

Table Notes

HISPANIC OR LATINO ORIGIN BY RACE

Survey/Program:
American Community Survey
Universe:
Total population
Year:
2019
Estimates:
5-Year
Table ID:
B03002

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

Source: U.S. Census Bureau, 2015-2019 American Community Survey 5-Year Estimates

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

The 2015-2019 American Community Survey (ACS) data generally reflect the September 2018 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas. In certain instances, the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineation lists due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Explanation of Symbols:

- An "*" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
- An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution, or the margin of error associated with a median was larger than the median itself.
- An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.
- An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.
- An "***" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
- An "*****" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
- An "N" entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
- An "(X)" means that the estimate is not applicable or not available.

Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Note: This is a modified view of the original table produced by the U.S. Census Bureau. This download or printed version may have missing information from the original table.

	White County, Indiana	Census Tract 9585, White County, Indiana	Census Tract 9586, White County, Indiana
Label	Estimate	Estimate	Estimate
▼ Total:	23,801	3,588	4,805
▼ Income in the past 12 months below poverty level:	2,198	440	546
▼ Male:	824	194	156
Under 5 years	94	19	23
5 years	26	11	15
6 to 11 years	83	10	17
12 to 14 years	47	6	0
15 years	2	0	0
16 and 17 years	38	6	0
18 to 24 years	126	25	54
25 to 34 years	67	0	4
35 to 44 years	55	28	0
45 to 54 years	95	14	43
55 to 64 years	113	36	0
65 to 74 years	60	39	0
75 years and over	18	0	0
▼ Female:	1,374	246	390
Under 5 years	97	18	43
5 years	14	0	0
6 to 11 years	120	7	13
12 to 14 years	71	13	0
15 years	43	0	0
16 and 17 years	0	0	0
18 to 24 years	210	9	133
25 to 34 years	250	51	110
35 to 44 years	128	8	11
45 to 54 years	105	38	23
55 to 64 years	127	34	0
65 to 74 years	119	68	15
75 years and over	90	0	42
▼ Income in the past 12 months at or above poverty level:	21,603	3,148	4,259
▼ Male:	11,020	1,586	2,078
Under 5 years	659	58	170
5 years	143	17	49
6 to 11 years	907	14	294
12 to 14 years	328	30	104
15 years	132	9	61
16 and 17 years	332	74	26

Table Notes

POVERTY STATUS IN THE PAST 12 MONTHS BY SEX BY AGE

Survey/Program:
American Community Survey

Universe:
Population for whom poverty status is determined

Year:
2019

Estimates:
5-Year

Table ID:
B17001

Although the American Community Survey (ACS) produces population, demographic and housing unit estimates, it is the Census Bureau's Population Estimates Program that produces and disseminates the official estimates of the population for the nation, states, counties, cities, and towns and estimates of housing units for states and counties.

Source: U.S. Census Bureau, 2015-2019 American Community Survey 5-Year Estimates

Data are based on a sample and are subject to sampling variability. The degree of uncertainty for an estimate arising from sampling variability is represented through the use of a margin of error. The value shown here is the 90 percent margin of error. The margin of error can be interpreted roughly as providing a 90 percent probability that the interval defined by the estimate minus the margin of error and the estimate plus the margin of error (the lower and upper confidence bounds) contains the true value. In addition to sampling variability, the ACS estimates are subject to nonsampling error (for a discussion of nonsampling variability, see ACS Technical Documentation). The effect of nonsampling error is not represented in these tables.

The 2015-2019 American Community Survey (ACS) data generally reflect the September 2018 Office of Management and Budget (OMB) delineations of metropolitan and micropolitan statistical areas. In certain instances, the names, codes, and boundaries of the principal cities shown in ACS tables may differ from the OMB delineation lists due to differences in the effective dates of the geographic entities.

Estimates of urban and rural populations, housing units, and characteristics reflect boundaries of urban areas defined based on Census 2010 data. As a result, data for urban and rural areas from the ACS do not necessarily reflect the results of ongoing urbanization.

Explanation of Symbols:

- An "***" entry in the margin of error column indicates that either no sample observations or too few sample observations were available to compute a standard error and thus the margin of error. A statistical test is not appropriate.
- An "-" entry in the estimate column indicates that either no sample observations or too few sample observations were available to compute an estimate, or a ratio of medians cannot be calculated because one or both of the median estimates falls in the lowest interval or upper interval of an open-ended distribution, or the margin of error associated with a median was larger than the median itself.
- An "-" following a median estimate means the median falls in the lowest interval of an open-ended distribution.
- An "+" following a median estimate means the median falls in the upper interval of an open-ended distribution.
- An "****" entry in the margin of error column indicates that the median falls in the lowest interval or upper interval of an open-ended distribution. A statistical test is not appropriate.
- An "*****" entry in the margin of error column indicates that the estimate is controlled. A statistical test for sampling variability is not appropriate.
- An "N" entry in the estimate and margin of error columns indicates that data for this geographic area cannot be displayed because the number of sample cases is too small.
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Supporting documentation on code lists, subject definitions, data accuracy, and statistical testing can be found on the American Community Survey website in the Technical Documentation section.

Sample size and data quality measures (including coverage rates, allocation rates, and response rates) can be found on the American Community Survey website in the Methodology section.

Chris Kunkel

From: Fair, Terri <TFair@indot.IN.gov>
Sent: Monday, April 5, 2021 11:22 AM
To: Chris Kunkel
Cc: Bales, Ronald; Miller, Brandon; Ruth Hook
Subject: FW: Gordon Road Intersection Improvement - Des. No. 1702835 - Environmental Justice Analysis

INDOT-Environmental Services Division (ESD) has reviewed the project information along with the Environmental Justice (EJ) Analysis for the above referenced project. With the information provided, the project may require minimal right-of-way, require no relocations, and would not disrupt community cohesion or create a physical barrier. With the information provided, INDOT-ESD would not consider the impacts associated with this project as causing a disproportionately high and adverse effect on minority and/or low income populations of EJ concern relative to non EJ populations in accordance with the provisions of Executive Order 12898 and FHWA Order 6640.23a. No further EJ Analysis is required.

From: Chris Kunkel <CKunkel@lochgroup.com>
Sent: Monday, February 22, 2021 3:03 PM
To: Bales, Ronald <rbales@indot.IN.gov>
Cc: Ruth Hook <RHook@lochgroup.com>
Subject: Gordon Road Intersection Improvement - Des. No. 1702835 - Environmental Justice Analysis

**** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ****

Good afternoon, Ron,

Please find attached the EJ Analysis for this intersection improvement project in White County, Indiana. The project area included EJ populations of concern for both poverty and minorities. Please review this EJ Analysis at your earliest convenience.

Should you have any questions or comments, please let us know.

Thank you and have a great day!

Chris Kunkel
Environmental Specialist II
Lochmueller Group

3502 Woodview Trace, Suite 150, Indianapolis, IN 46268
317.334.6818 (direct) | 317.677.5132 (mobile)
CKunkel@lochgroup.com
<http://lochgroup.com>

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MEMO

To: [REDACTED]

From: Cheryl L Sharp, PE, PTOE

Date: January 28, 2020

Subject: Gordon Road Roundabout
Lochmueller Group Project 218-0026-OHY

Lochmueller Group has conducted a roundabout analysis to determine the optimal roundabout intersection configuration for the intersection of Gordon Road and US 421/ SR 39 in Monticello, Indiana. The intersection is located south of Indiana Beach and Lake Shafer, which are popular summer vacation destinations. The purpose of this study was to evaluate the optimal roundabout configuration to improve intersection operability as well as to evaluate the associated traffic and safety implications of the proposed roundabout. The existing intersection configuration is depicted in **Figure 1**.



Figure 1: Intersection of Gordon Road and US 421/ SR 39

Existing Conditions

Currently, the intersection is a 5 leg signalized intersection. For the purpose of the analysis, US 421 / SR 39 will be considered the southbound and northbound approaches. The eastbound approach is considered Gordon Road and the westbound approach is City Park Loop, which is a one way exit out of the park adjacent to the intersection. South Airport Road is the southwest leg of the intersection and will be considered the northeastbound approach.

3502 Woodview Trace, Suite 150
Indianapolis, Indiana 46268
PHONE: 317.222.3880 • TOLL FREE: 888.830.6977

The northbound, northeastbound, westbound, and eastbound approaches are each single lane. The southbound approach is two lanes with one thru-lane and one thru/right-lane with a restricted right-turn on red. The westbound approach also has a restricted right-turn on red. Each of the roads at the intersection of Gordon Rd and US 421 / SR 39 are functionally classified as minor arterials.

The City of Monticello, Indiana experiences seasonal variations in the traffic volumes due to the nearby Indiana Beach and Lake Shafer. Indiana Beach is a popular boardwalk resort and amusement park on Lake Shafer which draws large crowds during the summer months. This causes traffic volumes in the summer to typically be higher than the remainder of the year. Therefore, traffic counts were performed in both July 2019 and October 2019 to quantify existing peak and non-peak conditions. Existing signal timings were obtained from the Local Public Agency.

To quantify existing traffic volumes, turning movement counts were collected at the study intersection during the weekday morning and evening peak periods. It was determined that the peak hours of traffic flow are 7:30 to 8:30 a.m. and 4:30 to 5:30 p.m. This study focused on morning and afternoon peak period conditions on a typical weekday since these represent the most critical times of day for traffic operations and roadway capacity. Based on 2018 INDOT data from their Traffic Count Database System (TCDS), the annual average daily traffic (AADT) along US 421 / SR 39 is 6,897 vehicles per day (vpd) and the AADT along Gordon Road is 4,484 vpd. S Airport Road has a lower AADT at 1,766 vpd.

The existing multi-modal facilities are limited. There are no sidewalks or existing bicycle facilities in the study intersection. Monticello is not currently served by public transportation.

Growth Rate

Regional Growth

INDOT historic traffic data was reviewed from the Traffic Count Database System (TCDS) and used to determine background growth rates for the study area. A 3% growth rate was determined for the area after reviewing the historic traffic data on the TCDS system. It should be noted that the growth rate was only used for the thru movements along US 421/SR 15. Additionally, the growth rate was only applied to the October 2019 counts, as the July counts included the additional seasonal traffic volumes.

Seasonal Factor

Due to nearby Indiana Beach and Lake Shafer, the intersection experiences a degree of seasonal traffic. To account for the expected change in traffic due to the seasonal nature of the surrounding land uses, traffic counts were performed during both peak seasonal conditions and non-peak typical conditions in July 2019 and October 2019 respectively. The difference between the July and October traffic volumes were determined to represent the additional seasonal traffic volumes due to Indiana Beach and Lake Shafer. The difference in existing traffic counts from peak season and non-peak season were then layered on top of the grown October 2019 traffic volumes to better represent average conditions at the intersection of US 421/SR 15 and Gordon Road. Figure 2 shows the existing lane configurations and traffic controls, existing traffic volumes, and 2042 baseline traffic volumes based on a 3% growth rate for the north- and southbound thru movements.

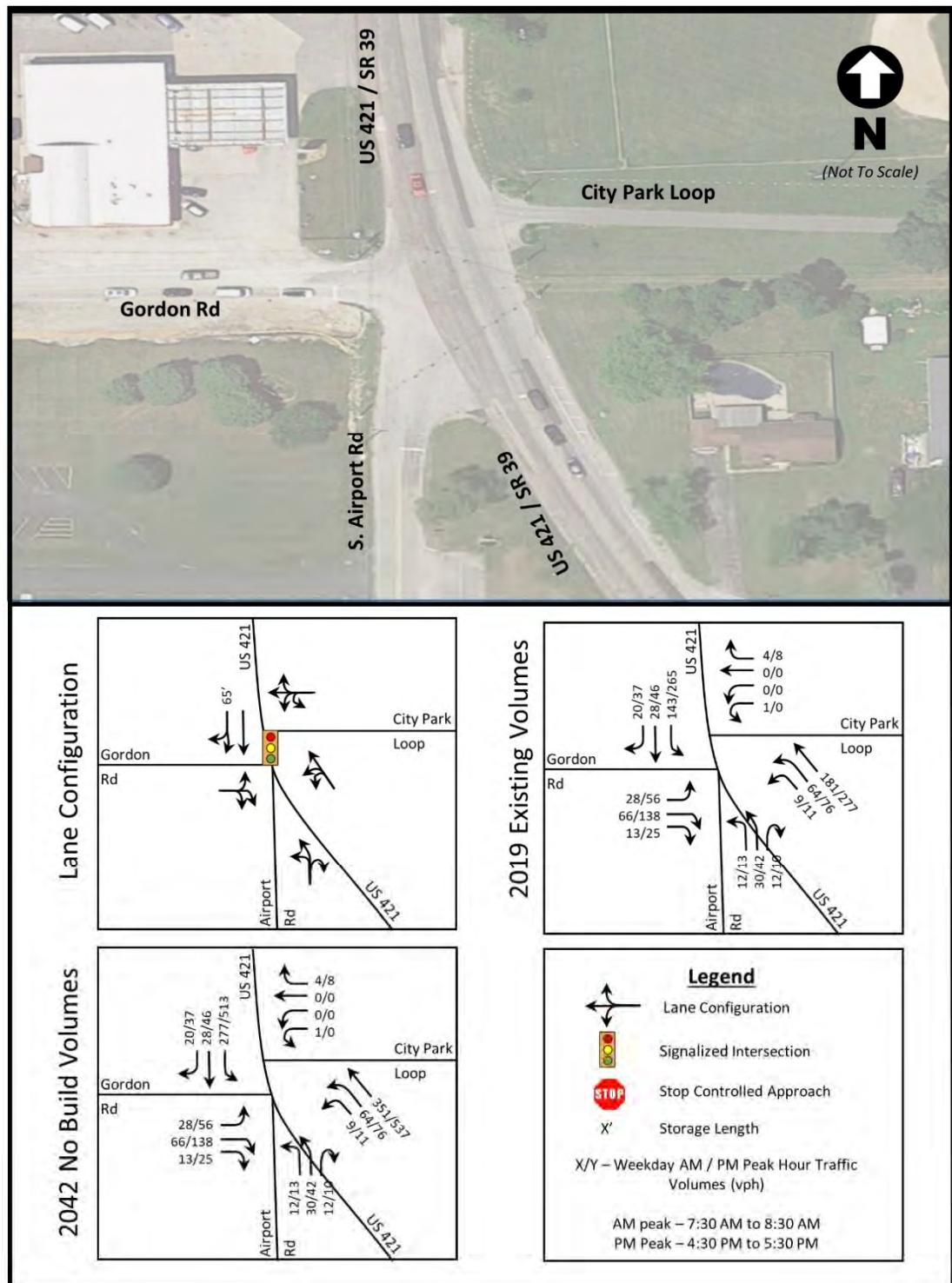


Figure 2. Gordon Road Lane Configuration and Traffic Volumes

Traffic Analysis

The analysis was done for the existing weekday AM and PM conditions. Three scenarios were analyzed including:

- 2019 Baseline Conditions (Existing Volumes, Lane Configurations and Signal Timings)
- 2042 Design Year Baseline (Grown Volumes, Existing Lane Configurations and Signal Timings)
- 2022 Design Year (Grown Volumes, Improved Roundabout Configuration)
- 2032 Design Year (Grown Volumes, Improved Roundabout Configuration)
- 2042 Design Year (Grown Volumes, Improved Roundabout Configuration)

Synchro 10 was used for the existing and Design Year baseline conditions that analyze a traffic signal. Sidra Intersection 8 software was used to evaluate the proposed roundabout for the Design Year improved conditions.

2019 Baseline Traffic Operations

Traffic operating conditions at the study intersections were evaluated using Synchro 10 traffic modeling software, which is based upon the methodologies outlined in the “Highway Capacity Manual” (HCM) published by the Transportation Research Board.

The performance of a transportation system is quantified by Levels of Service (LOS), which are measures of traffic flow that consider factors such as speed, delay, interruptions, safety, and driver comfort and convenience. There are six levels of service ranging from LOS A (“free flow”) to LOS F (“oversaturated”). LOS C is commonly used for design purposes and represents a roadway with volumes utilizing 70 to 80 percent of its capacity. LOS D is typically considered acceptable for peak period conditions in urban and suburban areas.

Levels of service for intersections are determined based on the average delay experienced by motorists. For signalized intersections, delay is estimated for each movement and then aggregated by approach and the intersection as a whole; at intersections with partial (side-street) stop control, delay is calculated for minor movements only since motorists on the main road are not required to stop.

The thresholds for each level of service vary based upon the type of control to reflect different driver expectations. Signalized intersections are designed to carry higher traffic volumes, and therefore motorists accept heavier delays as compared to unsignalized intersections. **Table 1** summarizes the criterion for both signalized and unsignalized intersections, as defined by the HCM and Section 4.5.7 of the National Cooperative Highway Research Program (NCHRP) Roundabout Informational Guide. **Table 2** summarizes the 2019 baseline operating conditions.

Table 1: Intersection Level of Service Thresholds

Level of Service	Control Delay per Vehicle (sec/veh)	
	Signalized	Unsignalized
A	≤ 10	0-10
B	> 10-20	> 10-15
C	> 20-35	> 15-25
D	> 35-55	> 25-35
E	> 55-80	> 35-50
F	> 80	> 50

Table 2. 2019 Baseline Operating Conditions

Intersection & Movements	LOS (Delay, sec) [Queue Length*, feet] <v/c ratio>	
	AM Peak Hour	PM Peak Hour
US 421/SR 15 & Gordon Rd (signalized)		
Eastbound Approach	D (48.1) [#128] <0.69>	E (71.1) [#291] <0.90>
Westbound Approach	A (0.0) [0] <0.02>	A (0.1) [0] <0.02>
Northbound Approach	C (25.4) [178] <0.65>	C (30.5) [#357] <0.77>
Northeastbound Approach	A (0.7) [0] <0.14>	A (1.5) [0] <0.22>
Southbound Approach	B (16.9) [127] <0.33>	B (18.4) [253] <0.49>
Overall Intersection	C (24.7)	C (32.4)

* 95th Percentile Queue

Delay presented in seconds per vehicle

95th percentile volume exceeds capacity, queue may be longer. Queue shown is maximum after two cycles

As shown, all of the approaches within the study area function with acceptable levels of service during all peak periods for the 2019 existing volumes. However, the eastbound approach during the PM peak hour is nearing a failing level of service and has a 12-car queue. This is likely due to the large volume of right-turns and the inability to turn right on red at this approach.

2042 Baseline Traffic Operations

The proposed roundabout needs to perform well throughout the future design period, which is assumed to be 2042. If the alternative concepts perform adequately during the future design period (2042), it can be concluded they will perform adequately under existing, 2022, and 2032 conditions as well. Therefore, the no-build analysis was only completed for the 2042 design year.

The 2042 design year baseline analysis was evaluated using the same methodologies detailed in the 2019 baseline conditions. The existing lane configurations and signal timings were used for the 2042 design year baseline analysis. The 2042 baseline volumes are shown in **Figure 2. Table 3** summarizes the operating conditions for the design year no-build analysis.

Table 3. 2042 Design Year Baseline Operating Conditions

Intersection & Movements	LOS (Delay, sec) [Queue Length*, feet] <v/c ratio>	
	AM Peak Hour	PM Peak Hour
US 421/SR 15 & Gordon Rd (signalized)		
Eastbound Approach	D (48.1) [#128] <0.69>	E (71.1) [#291] <0.90>
Westbound Approach	A (0.0) [0] <0.02>	A (0.1) [0] <0.03>
Northbound Approach	F (152.7) [#407] <1.25>	F (699.6) [#729] <2.49>
Northeastbound Approach	A (0.7) [0] <0.14>	A (1.5) [0] <0.22>
Southbound Approach	C (20.2) [213] <0.54>	C (29.2) [#548] <0.81>
Overall Intersection	F (89.3)	F (320.1)

* 95th Percentile Queue

Delay presented in seconds per vehicle

95th percentile volume exceeds capacity, queue may be longer. Queue shown is maximum after two cycles

As shown, the intersection has a failing level of service for both the AM and PM 2042 design year baseline conditions without any changes to the existing road geometry. The increased traffic volumes, specifically the northbound thru movement, increases the volume to capacity ratio of that approach to 2.49 during PM conditions. This ratio is 149% over capacity. To mitigate these failing levels of service, a single-lane roundabout was analyzed for this same intersection.

Forecasted Traffic Conditions

The forecasted conditions analyze the efficiencies of modifying the existing intersection to a single-lane roundabout. The same methodology applied to the volumes for the baseline conditions was again applied to the 2022, 2032, and 2042 design year volumes in an effort to determine the adequacy of the roundabout to accommodate traffic generated by the expected growth. The same volumes for the 2042 no-build condition were used for the 2042 build condition as the increase in traffic growth is not related to the implementation of the roundabout. The traffic volumes were forecasted at a 3% growth rate for the north- and southbound thru movements and are shown in **Figure 3**.

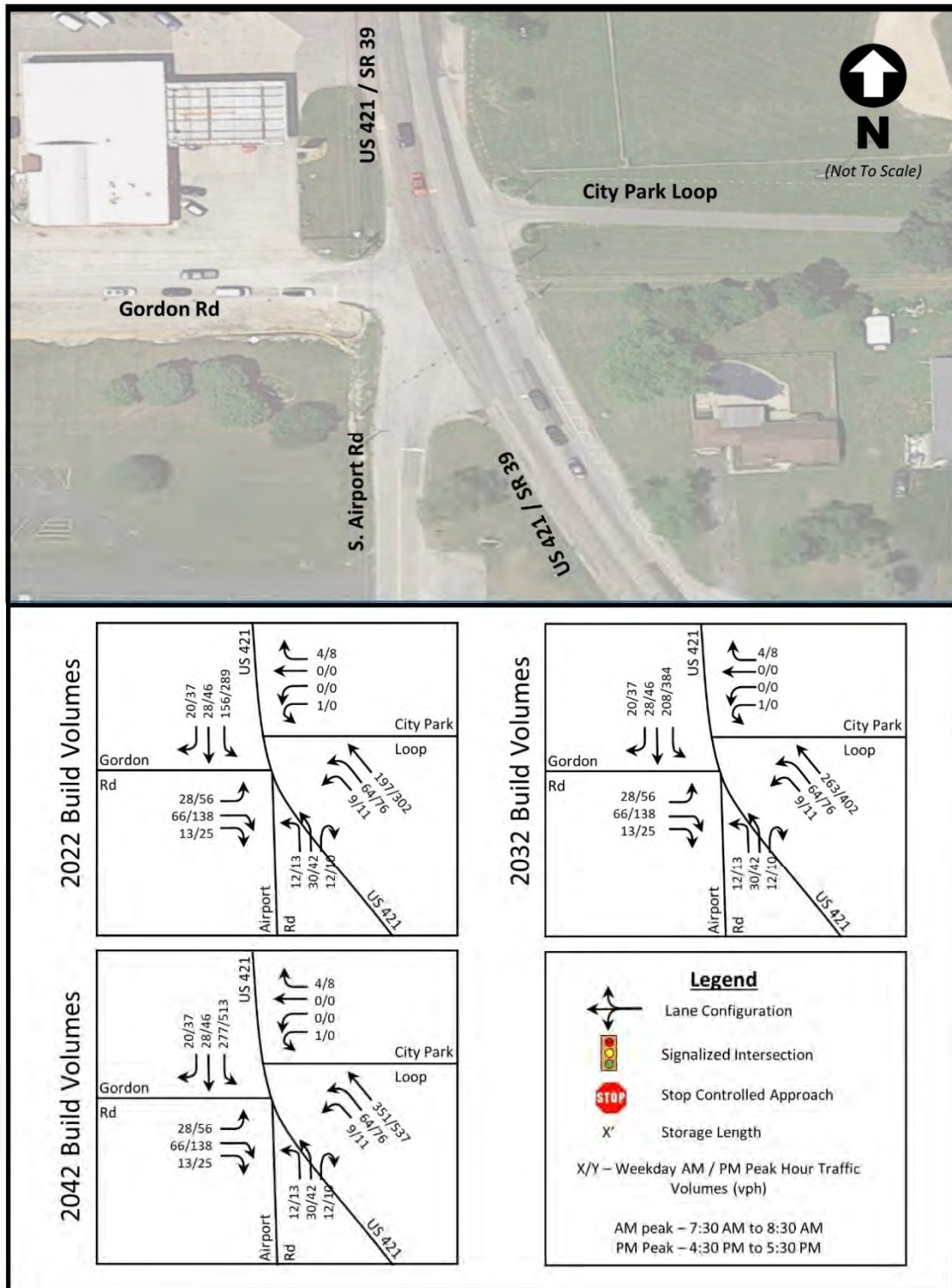


Figure 3. Gordon Road Forecasted Traffic Volumes

2022 Forecasted Traffic Operations

The design year build analysis was evaluated using Sidra Intersection 8, which is based upon the methodologies outlined in the "Highway Capacity Manual" (HCM) 6th Edition published by the Transportation Research Board.

The existing 2019 volumes were grown at 3% for each respective design year. The 2022 forecasted build volumes are shown in **Figure 3**. The level of service performance requirements for unsignalized intersection were used for the analysis of the proposed single-lane roundabout. **Table 4** details the operating conditions for the 2022 build out of the proposed roundabout.

Table 4. 2022 Design Year Operating Conditions

Intersection & Movements	LOS (Delay, sec) [Queue Length*, feet] <v/c ratio>	
	AM Peak Hour	PM Peak Hour
US 421/SR 15 & Gordon Rd (signalized)		
Eastbound Approach	A (4.8) [15] <0.13>	A (6.8) [31] <0.26>
Westbound Approach	A (4.4) [2] <0.02>	A (5.1) [2] <0.02>
Northbound Approach	A (6.0) [42] <0.31>	A (7.0) [62] <0.40>
Northeastbound Approach	A (4.2) [6] <0.06>	A (5.5) [9] <0.09>
Southbound Approach	A (4.8) [23] <0.19>	A (6.6) [54] <0.36>
Overall Intersection	A (5.3)	A (6.7)

* 95th Percentile Queue

Delay presented in seconds per vehicle

As shown, the impact of a single-lane roundabout would significantly improve the intersection's operations. This implementation of a roundabout resolves large queues caused by restricted right on red turns and accommodates large volumes of thru traffic. Each movement operates favorably at a LOS A. This is optimal and an improvement from the existing conditions.

2032 Forecasted Operating Conditions

The same methodologies used for the 2022 forecasted analysis were used for the 2032 forecasted analysis. The 2032 forecasted build volumes are shown in **Figure 3**. **Table 5** details the operating conditions for the 2032 build out of the proposed single-lane roundabout.

Table 5. 2032 Design Year Operating Conditions

Intersection & Movements	LOS (Delay, sec) [Queue Length*, feet] <v/c ratio>	
	AM Peak Hour	PM Peak Hour
US 421/SR 15 & Gordon Rd (signalized)		
Eastbound Approach	A (5.1) [15] <0.14>	A (7.9) [34] <0.30>
Westbound Approach	A (4.9) [2] <0.02>	A (5.8) [2] <0.03>
Northbound Approach	A (6.9) [58] <0.38>	A (8.5) [90] <0.50>
Northeastbound Approach	A (4.4) [7] <0.06>	A (6.3) [10] <0.10>
Southbound Approach	A (5.3) [30] <0.24>	A (7.8) [77] <0.46>
Overall Intersection	A (6.0)	A (8.0)

* 95th Percentile Queue

Delay presented in seconds per vehicle

Similar to the 2022 forecasted operations, the impact of a single-lane roundabout in the 2032 analysis would maintain the improvements made to the intersection's operations. Each movement operates favorably at a LOS A and has surplus capacity. The low queue lengths and delays are maintained in the 2032 condition as well.

2042 Forecasted Operating Conditions

The same methodologies used for the 2022 forecasted analysis were used for the 2042 forecasted analysis. The 2042 forecasted build volumes are shown in **Figure 3**. **Table 6** details the operating conditions for the 2042 build out of the proposed single-lane roundabout.

Table 6. 2042 Design Year Operating Conditions

Intersection & Movements	LOS (Delay, sec) [Queue Length*, feet] <v/c ratio>	
	AM Peak Hour	PM Peak Hour
US 421/SR 15 & Gordon Rd (signalized)		
Eastbound Approach	A (5.6) [16] <0.15>	A (9.7) [41] <0.35>
Westbound Approach	A (5.6) [2] <0.03>	A (6.9) [3] <0.03>
Northbound Approach	A (8.3) [84] <0.48>	B (11.3) [146] <0.64>
Northeastbound Approach	A (4.8) [7] <0.07>	A (7.5) [12] <0.12>
Southbound Approach	A (6.0) [41] <0.30>	B (10.1) [120] <0.58>
Overall Intersection	A (7.1)	B (10.4)

* 95th Percentile Queue

Delay presented in seconds per vehicle

As shown, even in the 2042 design year build out, the impact of a single-lane roundabout would significantly improve the intersection's operations. The level of service is an A for the AM conditions, and a B for the PM conditions, as compared to the failing levels of service for both conditions in the 2042 baseline conditions. This implementation of a single-lane roundabout resolves large queues caused by restricted right on red turns and accommodates large volumes of thru traffic. The largest queue length occurs in the northbound approach and is approximately 6 cars in the PM peak hour. This queue length is an 80% improvement from the 2042 baseline conditions. The intersection delay is improved by 92% and

97% for the AM and PM peak hours, respectively compared to the 2042 baseline conditions. Each approach operates with favorable conditions, therefore a single-lane roundabout with five approaches is adequate for the expected volume at Gordon Road and US 421/ SR 39.

Safety Analysis

A single-lane roundabout has 8 conflict points, as compared to the 32 conflict points present at a conventional intersection. This reduction in conflict points may improve the safety performance of the intersection. The CMF for the conversion of a two-way stop-controlled intersection into a single-lane roundabout is 0.751, which indicates a single-lane roundabout would be expected to improve the existing safety performance of the intersection by up to 25%. The installation of a roundabout will reduce speeds and a reduction in speed will be necessary for vehicles to traverse the roundabout.

Findings and Conclusions

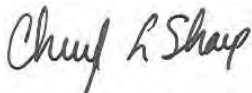
Based on the preceding study, the following may be concluded with regards to the proposed roundabout at US 421/SR 15 and Gordon Road:

- 2042 design year baseline conditions reach failing levels of service largely due to the expected growth in north- and southbound thru movements. Additionally, the restricted right-turn on red for the eastbound approaches results in large queues. Based on these factors, it can be concluded that the current intersection geometry does not adequately meet the needs of the expected future traffic volume at Gordon Road and US 421/ SR 39.
- The traffic impacts of the proposed roundabout significantly improve design-year operations. Based on the results, a single-lane roundabout is sufficient for the intersection of Gordon Road and US 421/ SR 39 for 2022, 2032, and 2042 forecasted conditions.
- The implementation of a roundabout is expected to improve the existing safety performance of the intersection by up to 25%.
- Based on the expected improved intersection operations and safety performance, a single-lane roundabout is recommended for the intersection of Gordon Road and US 421/ SR 39.

We appreciate your review of the above traffic impact study and look forward to receiving your comments. If you have any questions or comments, please do not hesitate to contact our office at your convenience.

Sincerely,

Lochmueller Group



Cheryl L. Sharp, P.E., PTOE
Project Manager



WHITE COUNTY BOARD OF COMMISSIONERS

Commissioners:

David Diener

Steve Burton

James Davis

May 27, 2021

Chris Kunkel
Lochmueller Group, Inc.
112 W. Jefferson Blvd
Suite 500
South Bend, Indiana 46601

Re: Section 4(f) Coordination
Des. No. 1702835
Gordon Road Intersection Improvement – River Park Square
Monticello, White County, Indiana

Dear Mr. Kunkel:

I understand the Gordon Road Intersection Improvement project may affect the White County/Monticello Trail, a multi-use path owned and maintained by our department. The project will not require the acquisition any right-of-way from the property, but construction activities at the intersection of Gordon Road, Main Street (US 421/SR 39), Airport Road, and City Park Loop will require the temporary closure of this portion of the path. The preferred alternative of this project will provide multi-use access to the Monticello City Park once construction is complete. Currently, the White County/Monticello Trail does not extend to the park.

As the official with jurisdiction (OWJ) over White County/Monticello Trail, I agree that the proposed project provides an enhancement to White County/Monticello Trail, and therefore would qualify for a Section 4(f) exception, as defined in 23 CFR 774.13(g). This exception applies for transportation enhancement projects and mitigation activities where:

1. The use of the Section 4(f) property is solely for the purpose of preserving or enhancing an activity, feature, or attribute that qualifies the property for protection, and;
2. The OWJ agrees in writing to the previous condition.

Once constructed, the Gordon Road Intersection Improvement Project will provide an enhancement to White County/Monticello Trail through improved multi-use access to Monticello City Park. Therefore, I agree the project will not adversely affect the recreational activities, features, and attributes that qualify White County/Monticello Trail for protection under Section 4(f) of the U.S. Department of Transportation Act.

Respectfully,

A handwritten signature in black ink, appearing to read "D. Diener", written in a cursive style.

Mr. David Diener
President, District 3 Commissioner
White County Board of Commissioners